

18.120.010 INTRODUCTION

2018-01-02

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010.A. BACKGROUND

I. PURPOSE

Chapter 18.120 is directed at implementing the following Olympia Comprehensive Plan policies:

- PL1.5. Require new development to meet appropriate minimum standards, such as landscaping and design guidelines, stormwater and other engineering standards, and buildings codes, and address risks, such as geologically hazardous areas; and require existing development to be gradually improved to such standards.
- PL1.12. Encourage major commercial projects to include display windows, small shops with separate entrances, and plazas with seating and other well-landscaped gathering spaces.
- PL3.1 Protect and evaluate historic and archaeological sites.
- PL3.2 Preserve those elements of the community which are unique to Olympia or which exemplify its heritage.
- PL3.5 Encourage development that is compatible with historic buildings and neighborhood character, and that includes complementary design elements such as mass, scale, materials, setting, and setbacks.
- PL4.2. Facilitate the preservation of historic neighborhood identity and important historic resources.
- PL6.1 Establish and periodically update a design review process and design criteria consistent with the goals and policies in the Comprehensive Plan for:
 - Commercial and mixed use development adjacent to freeways and public streets.
 - Other highly-visible, non-residential development, such as the Port of Olympia, campus developments, and master planned developments.
 - Multifamily residential development and manufactured housing parks.
 - Detached homes on smaller lots (less than 5,000 square feet) and in older neighborhoods (pre-1940).

- Properties listed on a Historic Register or located within a designated historic district.
- PL6.2. The design review process should recognize differences in the city with the objective of maintaining or improving the character and livability of each area or neighborhood.
- PL6.3. Require commercial and residential buildings to face the street or a courtyard or other common area.
- PL6.4. Require multi-family housing to incorporate architectural forms and features common to nearby housing to include porches, balconies, bay windows and similar details to have entries oriented to streets or a courtyard, and include accessible open space and to be reduced in size near lower density residential districts.
- PL6.5. Ensure that parking areas do not dominate street frontages or interrupt pedestrian routes, and that they are screened from single-family housing.
- PL6.5. Prohibit fences and walls that inhibit walking or isolate neighborhoods from streets, except to reduce noise, provide buffers, or create private rear yards.
- PL6.9. Require that buildings complement and enhance their surroundings, appeal to and support pedestrian activities, and facilitate transit use.

010.B. APPLICABILITY AND PROCEDURES

I. PROJECT REVIEW PROCEDURES AND GEOGRAPHIC APPLICABILITY

The applicability and review procedures for these guidelines shall be as described in OMC Chapter 18.72. The guidelines shall apply to all properties within the area shown in Figure 18.120.010.C.1.a.

2. GUIDELINES NOT APPLICABLE TO PROJECTS IN PUBLIC RIGHTS OF WAY

Chapter 18.120 does not apply to development in the public right of way.

010.C. DOWNTOWN DESIGN DISTRICTS

I. BACKGROUND AND INTENT

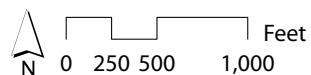
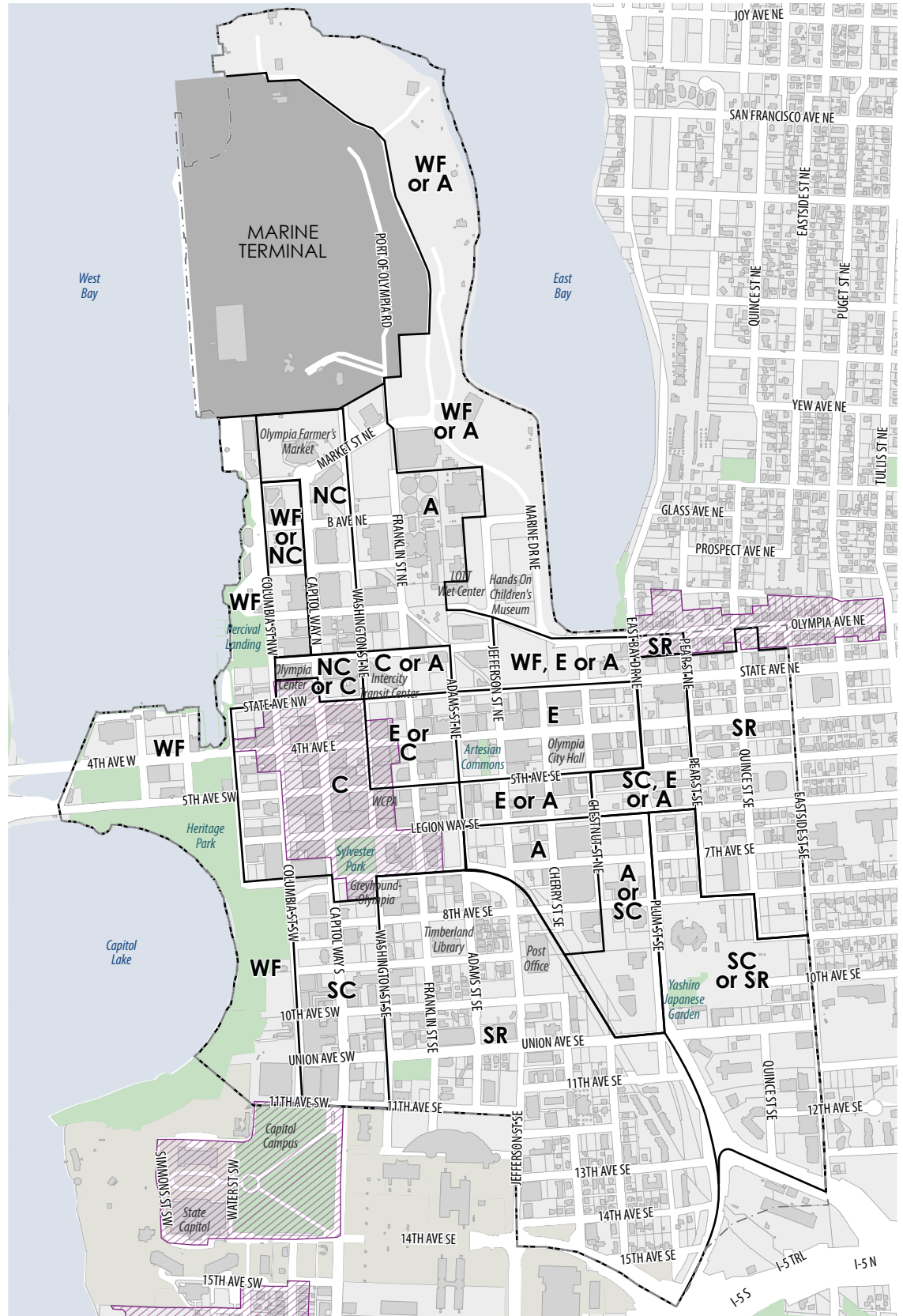
In order to achieve greater variety of uses and design character and greater design unity within specific districts and along streetscapes within the Downtown, Olympia's Downtown Strategy identifies 5 character areas to be created or enhanced. To implement this vision, these Guidelines establish 7 Downtown Design Districts (Districts) that generally correspond to the original character areas but are further divided to provide more specificity with respect to localized conditions. These Districts are identified in Figure 18.120.010.C.1.a.

Downtown Design Districts (DDD)

 **HISTORIC DISTRICT**

DESIGN DISTRICTS

- C: Core
- A: Art/Tech
- E: Entertainment
- WF: Waterfront
- NC: North Capitol Way
- SC: South Capitol Way
- SR: South residential neighborhood



Downtown Design Districts (DDD)



Figure 18.120.010.C.1.a Downtown Design Districts (Districts) (will be updated.)

2. DESCRIPTION OF DOWNTOWN DESIGN DISTRICTS

a. Core

- i. General objectives and description: Enhance and build on early 20th century architecture of the historic retail core. The historic architectural styles and influences are varied, including Neoclassical, Art Moderne, Chicago Style, Romanesque, Victorian, Mission Revival, and Art Deco. Another underlying design goal is to activate streetscapes—such as with storefront building design, wide sidewalks, and multimodal access. Within the historic retail core, design elements should complement the surrounding historic architecture with sensitivity to neighboring buildings’ proportions and rhythms; “classic main street” furnishings, finishes, and styling; and materials drawing inspiration from surrounding historic materials like sandstone, brick, terracotta, and granite.
- ii. Design vision elements:
 - Early 20th century architecture in the historic core in a variety of styles is the design foundation
 - Elements such as storefronts and wide sidewalks activate the streetscape
 - Classic main street furnishings
 - New buildings should complement the District’s character through design features and qualities such as materials, scale, façade composition, proportion, façade composition, etc. but not try to replicate historical styles.

b. Art/Tech

- i. General objectives and description: Utility is a design driver for the informal Artisan/Tech District, suggesting opportunities for adaptive reuse of existing buildings, and low-cost street improvements that promote new pedestrian and bike uses. The incorporation of “warm industrial” materials—weathered wood, metal, etc.—and design elements—garage doors or large entryways that engage the street, parklets, etc.—are examples of how architecture and streetscape designs might complement the area’s do-it-yourself character
- ii. Design vision elements
 - Utilitarian, workshop character
 - Warm industrial materials – weathered wood, metal, etc.
 - Diverse, eclectic, water and energy efficient architecture
 - Adaptive reuse of existing buildings
 - Low cost street improvements that promote new pedestrian and bike uses

- Outdoor work activities and storage of materials may be appropriate.

c. Entertainment

- i. General objectives and description: Art and creative expression defines this hub of dining, nightlife, and the performing arts. Design elements here may reflect aspects of the Core the Art/Tech Districts, but a focus on art, theatricality, imagination, and glittery night lighting will distinguish this area. Opportunities may include an art-themed corridor, permanent public art features, temporary art installations, performances, and creative detailing that perhaps features a hint of glitz. Buildings and streetscapes should support entertainment needs, such as wide sidewalks and pedestrian spaces, consistent and inviting lighting, and clear wayfinding.
- ii. Design vision elements
 - An artsy, creative imaginative character
 - Substantial night lighting, perhaps with theatrical elements
 - Buildings and streetscapes should support entertainment needs (e.g., wider sidewalks, consistent lighting, clear wayfinding, etc.
 - Facades may have some similar elements as Core and Art/Tech Districts.

d. Waterfront

- i. General objectives and description: Better connecting Downtown to its waterfront location—visually, physically, and symbolically—is a primary goal of this District. Design elements found at Percival Landing—furnishings, light fixtures, marine hardware, wood pilings and boardwalks, rope, etc.—may be incorporated into streetscape improvements and new developments to help draw the aesthetics of the waterfront deeper into Downtown. Designs and materials may reflect additional waterfront themes of shoreline ecology, Northwest Native American history, historic industrial uses, sailing and boating, fishing and aquaculture, and commercial trading at the Port.
- ii. Design vision elements:
 - Visual, physical, and symbolic connections to the water
 - Incorporate design features found at Percival Landing
 - Reflect themes of shoreline ecology, local maritime activities, NW Native American history, historic industrial uses, sailing & boating, fishing and aquaculture and commercial trading

e. North Capitol Way

- i. General objectives and description: One of the Olympia Downtown Strategy's important urban design concepts is improving the visual connections and pedestrian experience along Capitol Way between the Capitol Campus and Farmers Market. In the North Capitol Way Downtown Design District, this means providing a streetscape that is, as much as possible, consistent with other segments of Capitol Way and directing new development toward featuring attractive, inviting building fronts. Because this Capitol Way segment features a variety of commercial and residential uses, ground floor non-residential uses are not always envisioned, but all buildings should present a welcoming face to the street.
- ii. Design vision elements
 - A mix of uses
 - Front facades that invite the pedestrian to walk along Capitol Way
 - A variety of architectural and landscaping treatments

f. South Capitol Way

- i. General objectives and description: This District is currently characterized by older hotels, small commercial buildings, parking lots and a few banks and anonymous office buildings. Given the current building stock and open land, substantial redevelopment is envisioned. The primary public objective for new development is to add human activity and upgrade the pedestrian between Downtown Olympia and the Campus, especially to attract workers and visitors from the Campus. Therefore the desired redevelopment would feature substantial mixed-use or commercial buildings with active ground floor uses (retail shops, restaurants and cafes, etc.) and pedestrian oriented front facades. Because of the potential for office complexes on segments of Plum Street SE, this District designation is an option there.
- ii. Design vision elements
 - Substantial, urban scaled buildings with refined facades that compliment Campus and Downtown Core buildings.
 - Relatively uniform streetscape lighting, paving and landscaping to create a strong visual connection between the Campus and Downtown Core.
 - Active ground floor uses and inviting ground floor facades with weather protection.

g. South Residential Neighborhood

- i. General objectives and description: This District is envisioned to be urban residential neighborhood with a mix of housing and building types. Significant landscape plantings (in streetscapes, medians, and on private property), traditional tree-lined streets, and green stormwater infrastructure can help define a residential neighborhood character. Because there is also office buildings and state-owned properties, office development is also expected where zoning allows. Substantial infill multi-family residential development is envisioned and the State owns some parcels so new office buildings may be developed in the District. Even though larger new buildings are anticipated, it is important to help ensure compatibility between old and new structures and retain a residential character so that the neighborhood remains desirable as a place to live.
- ii. Design vision elements
 - Substantial urban neighborhood
 - Organic mix of housing with architectural variety
 - Significant landscape elements; some residential in character and some enhancing new office buildings in a campus-like setting.
 - Compatibility between old and new structures

18.120.020 SITE PLANNING

2018-02-15

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020.H. SITE PLANNING FOR SECURITY

020.A. INTRODUCTION

Site Planning addresses the pedestrian visual qualities of Downtown streetscapes and developments that are affected by considerations such as building orientation and location, service areas, and pedestrian and vehicular access. It also addresses site security and residential privacy and open space.

020.B. STREET FRONTS

I. BACKGROUND AND INTENT

This section focuses on the relationship between the street and site with the intent to:

- Assist in the implementation of City and regional transportation plans by establishing a pedestrian street overlay district that results in a network of active, aesthetically pleasing, and interesting streets that link the Downtown retail core, the Capitol Campus, the waterfront, and residential neighborhoods.
- Increase walkability in Downtown.
- Increase architectural and historic continuity between the retail core and the remainder of downtown.
- Create a high quality, compact pedestrian-oriented environment so that people will be encouraged to work, shop, play, and live.
- Encourage around-the-clock activity to occur.

- Provide a comfortable sense of enclosure along the street by providing strong, continuous edges which clearly define public open spaces and rights-of-way.
- Contribute to the streetscape by incorporating human scaled elements into building design.
- Provide direct visual contact between activities occurring inside buildings and the street environment.
- Incorporate artistic elements and public art into the streetscape and buildings.
- Result in urban building patterns and curtail the construction of suburban building patterns.
- Establish attractive “Downtown Entry Streets” that enhance the visual appearance of entrances and prominent corridors into and around the Downtown.
- Acknowledge the need to accommodate arrival by vehicle.
- Support increased arrival by transit, carpool, bike, and on foot.
- Create incentives to circulate within downtown by non-auto modes (on foot, by shuttle or bus, or bicycle).
- Encourage employee Transportation Demand Management Program success.

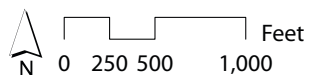
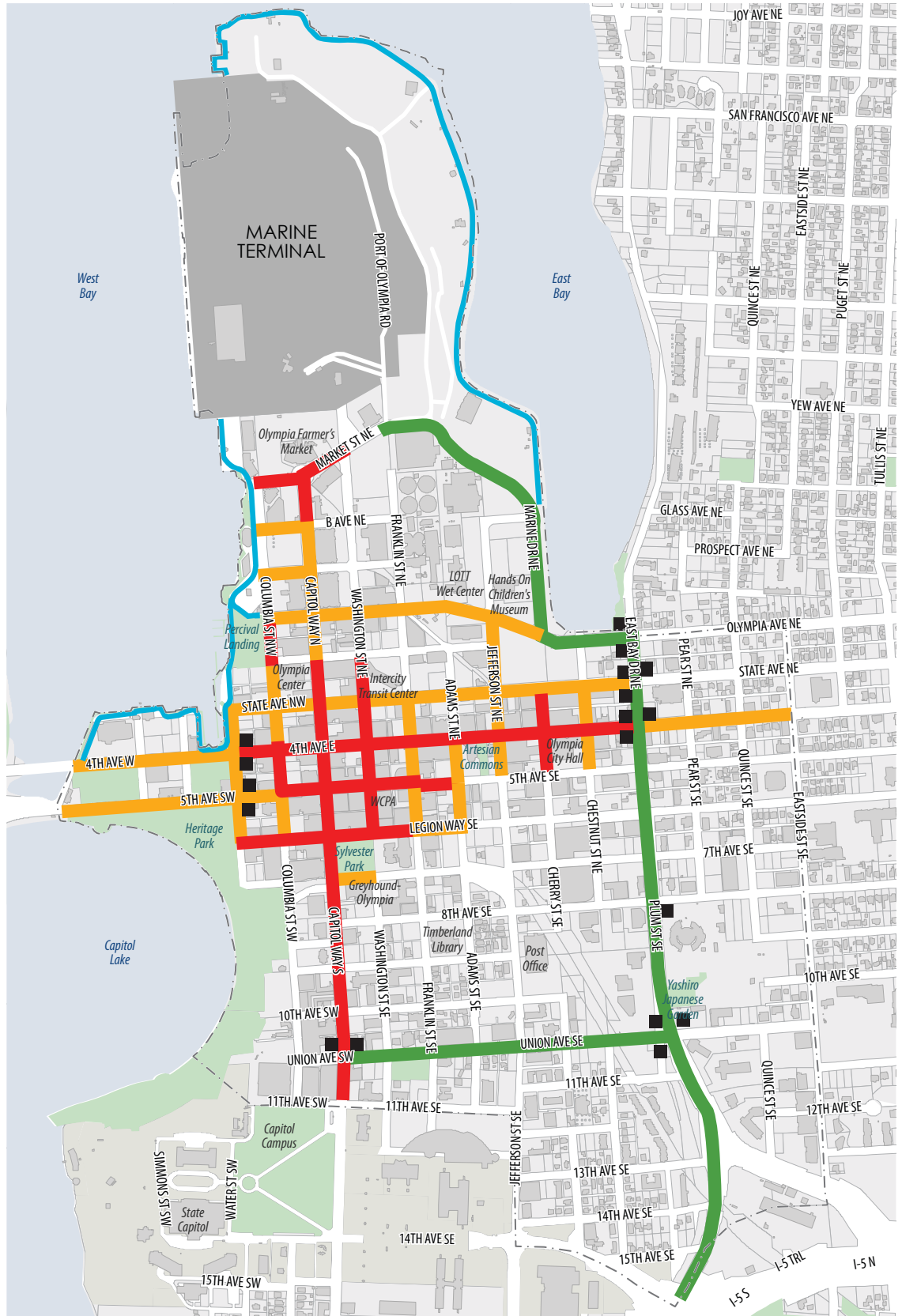
2. APPLICABILITY AND STREET TYPE DESIGNATIONS

Street Front requirements apply to facades of buildings facing Type A and B Pedestrian Oriented Streets, Type C Streets, Waterfront Sites and Downtown Entry Streets as indicated in Figure 18.120.020.B.2.a.

Specially Designated Streets

STREETS DESIGNATIONS

- "A" Pedestrian-oriented Streets
- "B" Pedestrian-oriented Streets
- Waterfront Sites
- Downtown Entry Streets
- (unmarked streets) "C" Streets
- High Visibility Street Corners



Specially Designated Streets

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- █ Downtown Entry Streets
- (unmarked streets) "C" Streets
- High Visibility Street Corners



Figure 18.120.020.B.2.a. Specially designated streets and waterfront sites.

Type A Pedestrian Oriented Streets are the most important downtown pedestrian routes with the greatest volume and concentration of pedestrian activities and attractions. The intent of guidelines in Section 3.a below is to ensure these streets maintain that level of pedestrian traffic and amenity. The guidelines for Type A streets emphasize a high level of façade transparency (amount of clear ground floor window area), wide required sidewalks, non-residential uses on the ground floor, restrictions on vehicle access and parking and maintenance of a prominent street wall (That is, there is a relatively continuous building edge along or near the back of the sidewalk).



Figure 18.120.020.B.2.a A Type A Pedestrian Oriented Street

Type B Pedestrian Oriented Streets are also important pedestrian connections but with somewhat less pedestrian traffic, amenities and attractions. They are also less central to Downtown's Core. The intent of guidelines in Section 3.b below is to ensure Type B streets are attractive pedestrian connections with sufficient interest at street level to encourage pedestrian traffic. Guidelines for Type B streets require less transparency, and access limitations than those for Type A streets and include no ground floor use requirements.



Figure 18.120.020.B.2.b. A Type B Pedestrian Oriented Street

Type C streets are those that are not Type A, Type B or Downtown Entry Streets or a Waterfront Site. The intent of guidelines in Section 3.c below is to ensure Type C streets are functional and attractive and can accommodate a wide variety of uses. Guidelines for Type C Streets require minimal sidewalks, a defined edge at the back of the sidewalk and efficient pedestrian access to building entries.



Figure 18.120.020.B.2.c. A Type C Street

Downtown Entry Streets are arterials with relatively high vehicle traffic volumes that bring travelers into the Downtown. Because they are highly visible, the intent of guidelines for Downtown Entry Streets in Section 3.f emphasize creating an attractive street front even if the uses may be auto oriented.



Figure 18.120.020.B.2.d. A Downtown Entry Street

Waterfront Sites are development sites directly facing the shoreline and guidelines for these sites in Section 3.e emphasize supporting an especially attractive and active pedestrian esplanade.



Figure 18.120.020.B.2.e. A Waterfront Site

Section 020.B also includes guidelines for street fronts with ground floor residential uses in Section 3.d. They apply throughout the Downtown and are intended to increase privacy for the resident, create an attractive streetscape and provide a graceful transition between public and private realms.

3. REQUIREMENTS FOR TYPE A PEDESTRIAN ORIENTED STREETS.

Building facades facing Type A Pedestrian Oriented Streets must:

- a. Feature “pedestrian oriented facades” as described in Section 18.120.040.F.
- b. Feature non-residential ground floor uses. Hotel and multifamily residential lobbies and entries are allowed.
- c. Feature ground floor setbacks at least 12’ from the face of curb. Upper stories may extend to the property/right-of-way line. The intent of this provision is to provide a clear and continuous walkable space sufficient to accommodate pedestrian movement and volumes. Buildings on properties with street frontage less than 100 feet, measured horizontally along the right of way, do not need to to meet this requirement.
- d. Provide street wall definition. The ground floor of buildings must extend to the property/right-of-way unless the setback from the curb required in “c” above is applicable. (I.e.: Align building along the ROW unless the resulting sidewalk area is less than 12 feet wide from back of curb to the building façade.)

Exception: Up to 80% of a building front (measured parallel to the right-of-way line) may be set back up to 60’ from the right-of-way provided the setback is occupied by pedestrian oriented open space as described in Section 18.120.030.C and the facades facing the open space meet the Pedestrian Facades and Weather Protection requirements in 18.120.040.F.

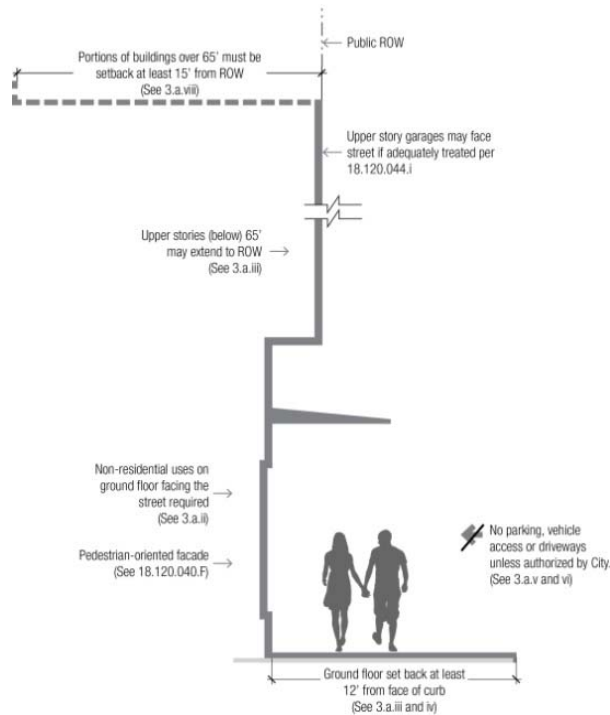


Figure 18.120.020.3.a. Summary of requirements for building facades facing a Type A Pedestrian Oriented Street. (This diagram is for illustrative purposes only. See Section 18.120.020.3 for stated requirements.)

- e. Not be occupied by onsite vehicle parking. Surface vehicle parking directly fronting a Type A Pedestrian Oriented Street is prohibited. Ground floor structured parking directly adjacent to type A Pedestrian Oriented Streets is prohibited. The City may allow upper story parking facing the street if the façade is suitably treated to appear as an occupied building or incorporates articulation treatments that break up the massing of the garage and add visual interest as described in 18.120.040.d.2.a.
- f. Not be occupied by vehicle access (driveways). All vehicle parking and access shall be from another street or alley unless the City determines that there is no other way to provide safe vehicle access. If the property is on the corner of two Type A Pedestrian Oriented Streets or fronts on a designated arterial and a Type A Pedestrian Oriented Street, the City will determine which street fronts may feature vehicle access (a driveway).
- g. Upper story setbacks for building elements or stories over 65' above grade shall be set back at least 15' from the public right-of-way. Exception: architecturally treated parapets and roof garden railings may extend up to 5 feet above the 65' façade face limit.)

4. TYPE B PEDESTRIAN ORIENTED STREET REQUIREMENTS.

Building facades facing Type B streets must conform to the requirements a through e, below, or if there are ground floor residential units, meet the requirements for ground floor residential units in 3.d. below. As an alternative the building façade will be acceptable if it meets the requirements for Type A Pedestrian Oriented Street provisions.

- a. Feature pedestrian oriented facades as described in Section 18.120.040.F.

Exceptions: In lieu of providing a pedestrian oriented façade on a Type B Street, the City may allow project applications that provide a pleasing pedestrian environment that does not meet all requirements in 18.120.040.F. In such cases, the applicant must demonstrate that the project includes a unique or creative solution that is superior for the pedestrian environment. Examples of such solutions include: special landscaping such as a garden, a trellised area with seating, a plaza, or substantial artwork.



Figure 18.120.020.B.3.b The Hands-On Children's Museum is an example of a very pedestrian friendly site that employs a unique solution rather than strict conformance to Pedestrian Oriented Street requirements.

- b. Feature ground floor setbacks at least 12' from the face of curb. Upper stories may extend to the property/right-of-way line.
- c. Street wall definition. The ground floor of buildings must extend to the property/right-of-way boundary or 12' from the face of curb, whichever is further from the roadway centerline. Exception: A building front may be set back up to 60' from the right-of-way provided the setback is occupied by pedestrian oriented open space as described in Section 18.120.030.C and the facade facing the open meet the Pedestrian Facades and Weather Protection requirements in 18.120.040.F.

- d. Not be occupied by vehicle parking or vehicle access (driveways). All vehicle parking and access shall be from another street or alley unless the City determines that there is no other way to provide safe vehicle access. If the building site fronts on two or more Pedestrian Oriented Streets or on a Pedestrian Oriented Street and a designated arterial, the City will determine what street front may feature vehicle access (driveway). Surface vehicle parking directly fronting a Type B Pedestrian Oriented Street is prohibited.
- e. If the building façade features ground floor residential units, adhere to Section 18.120.020.3.d. Street front requirements for buildings featuring ground floor residential units. In such cases the guidelines in this section B.4 do not apply.

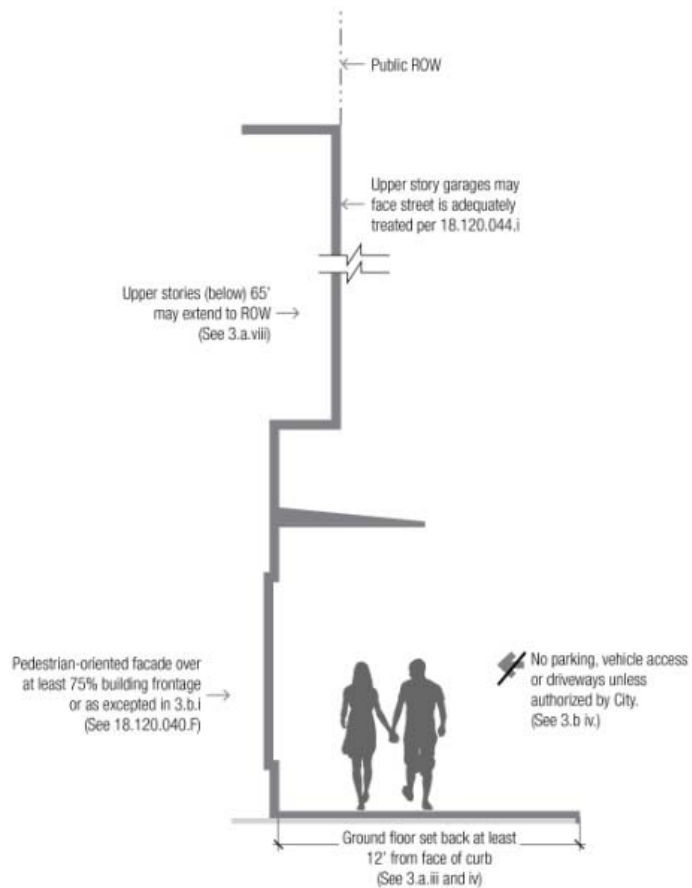


Figure 18.120.020.B.3.c. Summary of requirements for building facades facing a Type B Pedestrian Oriented Street. (This diagram is for illustrative purposes only. See Section 18.120.020.4 for stated requirements.)

5. TYPE C STREET REQUIREMENTS.

Building facades facing Type C streets must conform to the requirements a through e, below, or if there are ground floor residential units, the requirements for ground floor residential units in Section 6 below. As an alternative the building façade will be acceptable if it meets the requirements for Type A or Type B Pedestrian Oriented Street provisions. Physically define the street edge with building(s), landscaping, or other features as approved by the City.

Exceptions: Developments in the South Residential Neighborhood District may feature a front yard along the street edge that includes lawn or other landscaping, a pedestrian oriented open space or other treatment as approved by the City. No definition between the street and the site is needed if the front yard is a public pedestrian oriented open space such as a plaza, courtyard café seating area, or similar feature.

- a. **Set back development.** Set back development to provide sufficient room for a sidewalk at least 10 feet wide or sidewalk plus planting strip (total 10' wide) if there is not space in the public right-of-way to provide for the required walking area plus landscaping corridor.

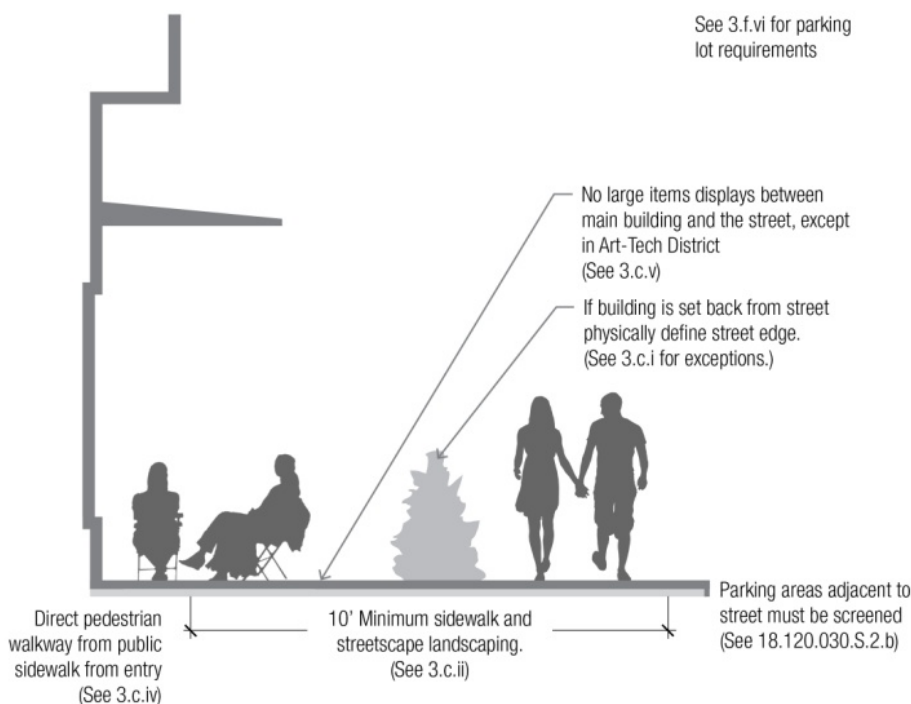


Figure 18.120.020.B.3.d. Requirements for building facades adjacent to Type C Streets. (This diagram is for illustrative purposes only. See Section 18-120-020.5 for stated requirements.)

- b. **Parking.** Parking areas adjacent to the street must be screened according to Section 18.120.030..2.b.

- c. Sidewalk access. Except for properties fronting directly on the shoreline in the Waterfront Design District, building entries must have direct access to the public sidewalk. Such entries must face the street to the extent possible. Where entries are located on the side of the building, they must be visible from the street and connected by a pedestrian pathway.
- d. Display items. No large item display areas are permitted between the main building and the street front (e.g. auto sales in the front yard area). Sidewalks shall not be enclosed as building space for retailing. Small, temporary displays for items such as groceries, hardware, nursery stock, books, etc., may be allowed provided the display does not unduly impede pedestrian sidewalk traffic. Large items, such as potting soil and compost bags, are not allowed. Outdoor seating is allowed subject to the City's street use regulations.

Exceptions to Section 3.d: The provisions of this Section 3.d. prohibiting large display items do not apply to properties in the Art-Tech Design District or Port of Olympia properties north of Market Street.

6. STREET FRONT REQUIREMENTS FOR BUILDINGS FEATURING GROUND FLOOR RESIDENTIAL UNITS.

The following applies to all ground-related units such as townhouses and zero-lot-line residences. Where the requirements of this Section 6 for ground floor residential uses, conflict with the guidelines in Sections 3, 4 5 or 8 (guidelines for designated street types) the guidelines of this Section 6 shall apply unless the City determines otherwise. (Ground floor residential uses are not allowed on Type A streets or Waterfront Sites.)



Figure 18.120.020.B.3.e Desirable ground related residential example with raised unit, landscaping and small porches to enhance privacy while providing a welcoming streetscape. Units front on the street. Garages are off the alley.

- a. Street access. Townhouses and other ground-related residences fronting a street must all have individual ground-related entries accessible from the street.
- b. Privacy. Provide for the internal privacy for people living in the ground related units through all of the measures below.

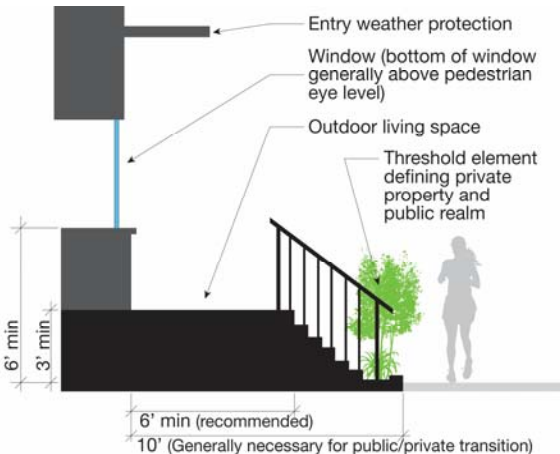


Figure 18.120.020.B.3.f Dimensional relationships to maintain privacy and provide a successful transition between public and private realms

- i. Set back ground level residences at least 10 feet from a public right-of-way or elevate the ground floor unit at least 3' above the sidewalk grade (preferably both). If the building front is less than 10' from the public ROW, the bottom of the ground floor windows facing the street must be at least 6' above sidewalk grade.
- ii. Provide a physical “threshold” feature such as a hedge, retaining wall, rockery, stair, gate, railing, or a combination of such elements on private property that defines and bridges the boundary between public right-of-way and private entry, porch, yard or patio. Thresholds may screen but must not block views to and from the street. Retaining walls must not be taller than 30". If additional height is required to accommodate grade conditions, then terraces edged by stepped retaining walls, each no more than 30" high are acceptable. Fences and screens must not be higher than 4' above the sidewalk.
- iii. Provide an outdoor space at least 6 feet in depth and 6 feet wide (36 SF minimum) in the front yard such as a porch, patio, deck, or similar space. Where feasible, this space must be at the same level as the interior of the unit. A portion of this space may be landscaped and a portion of it may count as part of the entry space required below.
- iv. Design the front door and entry area to enhance privacy. Locate and size windows and transparent doors so that pedestrians on the sidewalk cannot see directly into the lower ½ of the ground floor.

- v. Provide a covered area, porch or protected entry space, or other architectural weather protection at least 6 feet wide by 4 feet deep, that provides cover for a person entering the unit and a transitional space between outside and inside the dwelling.



Figure 18.120.020.B.3.g. Ground floor residential units such as these often lack privacy and the livability of outdoor spaces is limited. Locating the ground floor is at grade or below the sidewalk with insufficient setback results in an uninviting space even with the planter in place. This condition must be avoided.

- c. Garage configuration. Where the primary pedestrian access is off the same facade as vehicular access, driveway widths shall be no more than 12' wide. Entries into the garage shall be set back at least 5' from the front of the street facing building wall. All interior vehicle storage areas (garages shall have a door. Where more than one vehicle storage space is required, developments shall incorporate single-width "tandem" parking configurations for all ground related units to minimize the impact of garage doors on the streetscapes.

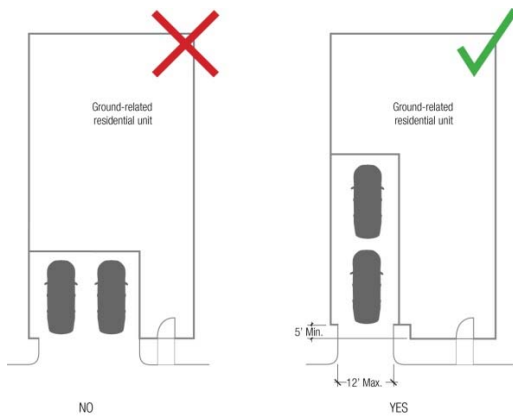


Figure 18.120.020.B.3.h. Driveways serving ground floor residential units shall be no more than 12' wide. Tandem parking is allowed. (This diagram is for illustrative purposes only. See Guideline 18-120-020.3.d.viii for stated requirements.)

7. ORIENTATION OF DEVELOPMENT ON WATERFRONT DESIGN DISTRICT SITES FRONTING DIRECTLY ON THE SHORELINE.

Projects proposed for sites fronting directly on the shoreline and not separated by a street and identified in Figure 18.120.020.B.2.f shall be configured to comply with the following. See also the regulations in the City' Shoreline Master Program:

- a. Shoreline facing facades. Building facades facing the shoreline shall feature a pedestrian oriented façade as described in 18.120.040.F.
- b. Façade perpendicular to the shoreline. Building facades roughly perpendicular to the shoreline shall comply with provisions for Type B Pedestrian Oriented Streets unless the City determines that there is a compelling reason to the contrary due to unusual use or site conditions.

8. ORIENTATION OF DEVELOPMENT ON DOWNTOWN ENTRY STREETS.

Projects proposed for sites fronting designated Downtown Entry Streets" as indicated on Figure 18.120.020.B.2.f. shall adhere to the following: As an alternative if the applicant chooses, projects may conform to requirements for either A or B Pedestrian Oriented Streets in lieu of Downtown Entry Street requirements.

- a. Non-residential buildings. Commercial and other non-residential building facades facing a Downtown Entry Street and located within 15 feet of the right of way must conform to the following:
 - i. Front facades facing an Entry Street must feature at least 50% transparency (clear window area) on the ground floor façade between 2 and 8 feet above grade.
 - ii. Front facades facing an Entry Street must feature a front entrance visible from the public street and a direct walkway between the public sidewalk and the main entrance.
 - iii. Front facades facing an Entry Street must feature weather protection at least over the front entry per 18.120.040.E.2.b. Additionally, the front façade must feature weather protection at least 5' deep over 75% of all portions of the façade adjacent to a pedestrian path or walk.
 - iv. No fences or screens (except for parking lot screening) taller than 4 feet or retaining walls higher than 30" are allowed within 15 feet of the right of way.

- v. Physically define the street edge with building(s), landscaping, or other features as approved by the City. Exception: No definition between the street and the site is needed if the front yard is a public open space such as a plaza, courtyard café seating area, or similar feature.

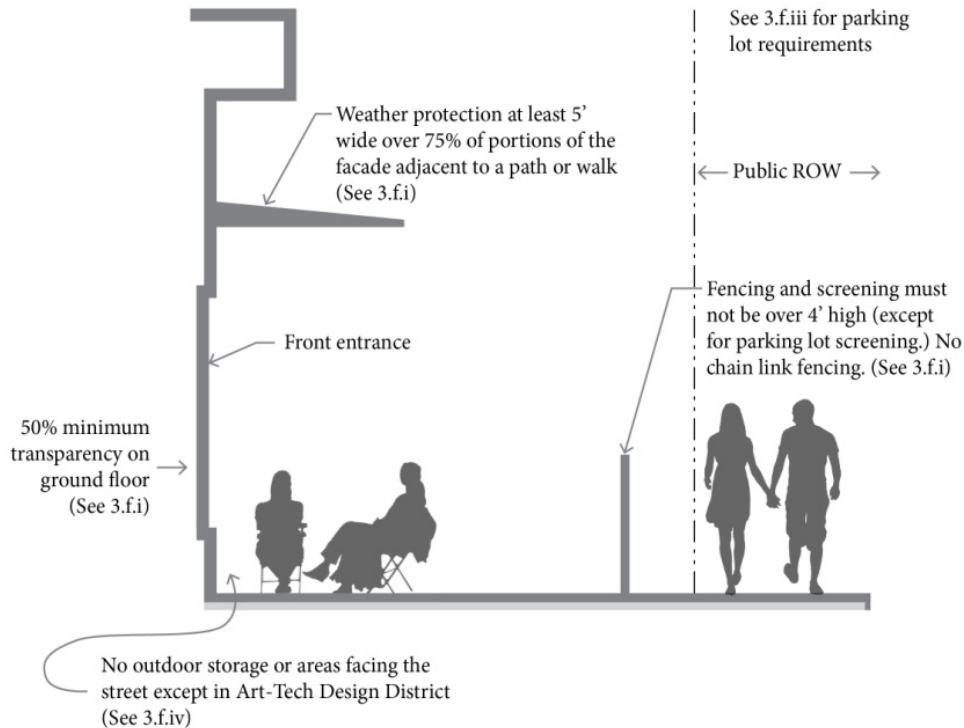


Figure 18.120.020.B.3.i. Requirements for non-residential buildings within 15' of an Entry Street ROW. (This diagram is for illustrative purposes only. See Section 18.120.020.3.f for stated requirements.)

- b. **Residential buildings.** Residential buildings located within 30 feet of the right of way must conform to the following:
 - i. Front facades facing an Entry Street must feature at least 15% transparency on the ground floor.
 - ii. Front facades facing an Entry Street must feature a covered porch, stoop, or patio for individual ground floor unit entries, or a clearly recognizable, covered shared entry per 18.120.040.E.2.b.
 - iii. No fences over 4 feet or retaining walls taller than 30" are allowed within 15 feet of the Downtown Entry Street ROW, except for parking lot screening.
 - iv. Garages or storage space are prohibited along the ground floor front façade facing the Downtown Entry Street.

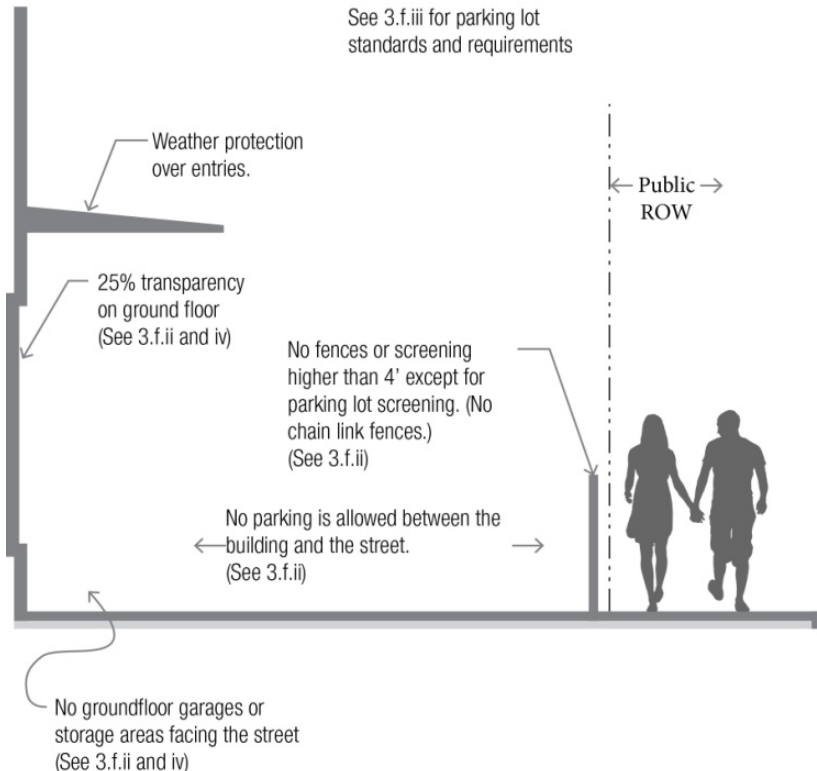


Figure 18.120.020.B.3.j. Requirements for residential buildings within 30' of an Entry Street ROW. (This diagram is for illustrative purposes only. See Section 18.120.020.8.b for stated requirements.)

c. Location of Parking All surface parking must be located behind, underneath, or to the side of the ground floor use facing the street (i.e., no onsite parking is allowed between the main building and the Downtown Entry Street). Parking is limited to 65 feet, as measured along the Downtown Entry Street frontage; or if the lot is wider than 130', parking may occupy up to 50% of the width of the lot facing the street. All surface parking areas that are located along the street must be screened per 18.36.180. Structured parking facing a Downtown Entry Street must be treated per Section 18.120.040.i.

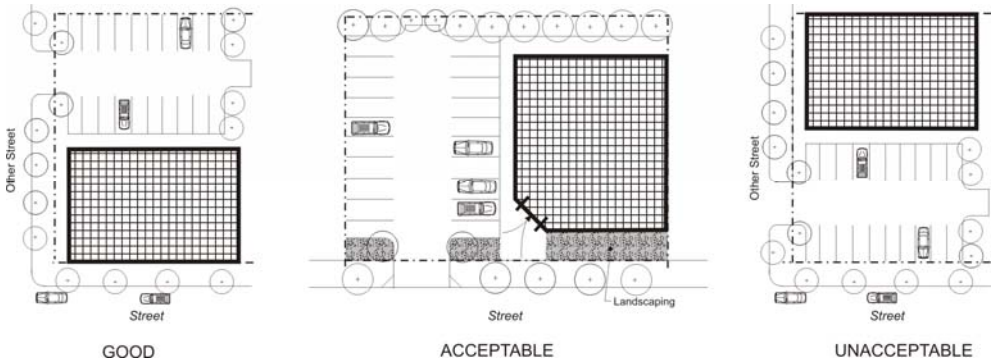


Figure 18.120.020.B.3.k. Requirements for surface parking location. (This diagram is for illustrative purposes only. See Section 18.120.020.3.f for stated requirements.)

- d. Outdoor Display Areas and Fences. Areas for outdoor displays or sales of large items are prohibited, except for small items that can be moved by hand. Storage or displays of bagged or bulk materials are prohibited. Chain link fences are prohibited in front yards.

020.C. PEDESTRIAN CIRCULATION (PLANNING)

I. INTENT

- To improve the pedestrian environment by making it easier, safer, and more comfortable to walk between businesses and residences, on street sidewalks, to transit stops, and through parking areas.
- To provide pedestrian facilities such as sidewalks, crosswalks, and bus shelters connecting to all modes of transportation.
- To provide convenient pedestrian circulation connecting all on-site activities to adjacent pedestrian routes and streets.
- To provide access to transit and services.

Note: See section 18.120.020.C for design requirements for pedestrian pathways and walkways. See especially 18.120.020.C.2.d for pathways along a building front that is adjacent to off-street parking.

2. REQUIREMENTS

- a. Pathways. In residential developments with multiple ground related units or entries, pathways between dwelling units and the street are required.



Figure 18.120.020.C.2.a. A good example of pedestrian connections between units. The pathway leads to the street. Note the positive security measures including bollard lighting, open landscaping with no hiding spaces, passive surveillance from windows and sense of enclosure that implies ownership of the space.

The pedestrian circulation system shall connect all main (primary) entrances on the site. For townhouses or other residential units fronting the street, the sidewalk may be used to meet this standard. For multifamily developments, pedestrian connections to other areas of the site, such as parking areas, recreational areas, common outdoor areas, and any pedestrian amenities shall be required.

- b. External stairways. External stairways or elevated walkways which provide pedestrian access to dwelling units located above the ground floor are prohibited. The City may allow exceptions for external stairways or walkways located in or facing interior courtyard areas provided they do not compromise visual access from dwelling units into the courtyard. This is not intended to prohibit skywalks or pedestrian bridges between buildings.



Figure 18.120.020.C.2.b. Elevated external walkways such as this are not allowed

- c. Required pathways. For developments between Jefferson Street NE, Thurston Avenue NE, Olympia Avenue NE, Chestnut Street NE and State Avenue NE, provide a publicly accessible walkway running north/south from State Avenue NE and Olympia Avenue NE with dimensions and design standards as required by the City.

020.D. VEHICLE ACCESS AND CIRCULATION

I. INTENT

- To provide for more efficient circulation and to ease congestion.
- To enhance the visual character of interior access roads.
- To minimize conflicts with pedestrian circulation and activity.
- To enhance the safety and function of public streets.
- To support transit services.

2. REQUIREMENTS

- a. Vehicle access. Generally, vehicle access shall be from the street with the least traffic and not from Pedestrian Oriented Streets where it can be avoided. See Section 18.120.020 Street Fronts. The City in consultation with the City's Public Works Department will determine where vehicle access is permitted.

020.E. SITE PLANNING OF LARGE LOTS AND FULL BLOCK SITES

I. INTENT

- To create integrated development plans and phasing strategies.
- To reduce negative impacts to adjacent properties.
- To enhance pedestrian and vehicular circulation.
- To provide usable open space.
- To create focal points for pedestrian activity for developments.
- To enhance the visual character of the community.

2. REQUIREMENTS

- a. Unified site plans. Development at sites with two or more buildings shall demonstrate that the project is based on a unified site plan that meets the following criteria:
 - i. Incorporate pedestrian pathways or open space with landscaping as a unifying element.
 - ii. Provide for safe, efficient internal vehicular circulation that does not isolate the buildings.
 - iii. Integrate any required open space as a central or unifying element.
 - iv. Take advantage of special on-site or nearby features.

Figure 18.120.020.E.2.a Two examples of site plans



illustrating the provisions of Guideline 18.120.020.E.2.a. Note the incorporation of open space as an organizing feature in the right example.

- v. To achieve direct, safe and comfortable pedestrian connections, building entrances must not be focused around a central parking area but be connected by a pathway system and/or open space(s), unless the City determines this infeasible or undesirable.
- vi. A development may provide a major public entry serving several shops rather than providing a separate storefront entry for each shop. If the development employs the combined-entry option, then the entry must be prominent and articulated with special features. See Figure 18.120.020.E.2.b, below.
- vii. Provide publicly accessible pedestrian connections between public streets where the City determines that such connections would not adversely impact the development.
- viii. Provide solid waste facilities and service areas that are accessible to the agency or company providing the service. See Figure 18.120.020.E.2.a, above.



Figure 18.120.020.E.2.b Prominent entries meeting the intent of Guideline 18.120.020.E.2.a.vi. Note the recessed entries, transparency, prominent canopies, landscaping, lighting and expression of the entry in the façade's upper stories.

020.F. SOLID WASTE STORAGE AND REMOVAL FACILITIES, SERVICE AREAS AND MECHANICAL EQUIPMENT

I. INTENT

- To minimize adverse visual, olfactory, or auditory impacts of mechanical equipment, utility cabinets and service areas at ground and roof levels.
- To provide access for service vehicles to remove waste and maintain service elements.
- To provide adequate, durable, well-maintained, and accessible service and equipment areas.
- To protect residential uses and adjacent properties from impacts due to location and utilization of service areas.

2. REQUIREMENTS FOR MECHANICAL EQUIPMENT AND SERVICE AREAS OTHER THAN SOLID WASTE STORAGE AND REMOVAL FACILITIES.

a. Location of Service Areas, Utilities, and Mechanical Equipment.

- i. Service areas mechanical equipment areas shall be located to avoid negative visual, auditory (noise), olfactory, or physical impacts on the street environment and adjacent residentially zoned properties. The City may require evidence that such elements will not significantly impact neighboring properties or public areas. (For example, the City may require noise damping specifications for fans near residential zones.) Service areas shall be sited for alley access, if available.
- ii. In the Southeast Neighborhood District, exterior loading areas for commercial uses shall not be located within 20 feet of a residentially zoned property, unless the City finds such a restriction does not allow feasible development or adequate service vehicle access. In such cases, the areas and drives will be separated from the residential lot by a masonry wall at least 8 feet high.
- iii. Except for development in the Art/Tech District, service areas must not be visible from the sidewalk and adjacent properties. Where the City finds that the only option for locating a service area is either visible from a public right-of-way or space or from an adjacent property, the area must be screened with either landscape or structural screening measures provided in Section 18.120.020.F.b.
- iv. Ground-mounted mechanical equipment must be located and screened to minimize visual and noise impacts to pedestrians on streets and adjoining properties.

- v. Roof-mounted mechanical equipment must be located and screened so the equipment is not visible from the ground level of adjacent streets and properties within 20 feet of the structure. Match the color of roof mounted equipment with the exposed color of the roof to minimize visual impacts when equipment is visible from higher elevations nearby.

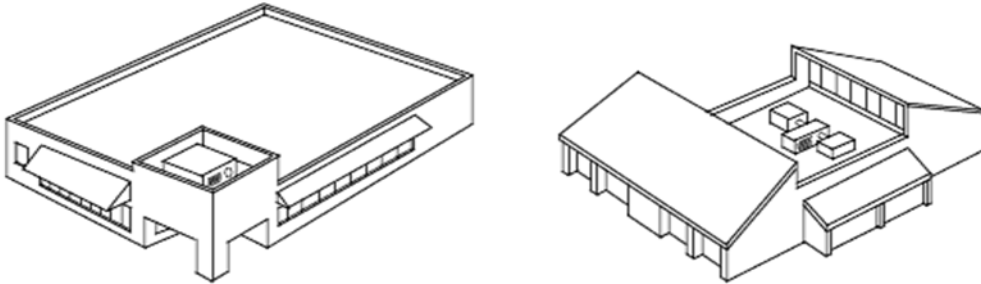


Figure 18.120.020.F.2.a. Examples of how to screen roof-mounted mechanical equipment.

- vi. Locate and screen utility meters, electrical conduit, and other service and utilities apparatus so they are not visible from adjacent properties and streets.
- vii. Other provisions of this Section 020.F notwithstanding, service areas used by residents shall be located and configured to avoid entrapment areas and other conditions where personal security is a concern. The City may require pedestrian-scaled lighting or other measures to enhance security.
- viii. While exterior service areas must be screened, screening requirements may be reduced by the City at access points for service areas inside buildings.
- ix. In addition to the required screening, art work such as paint schemes or coverings that help to blend the equipment into the background may also be utilized.
- x. Locate and/or shield noise producing mechanical equipment such as fans, heat pumps, etc. to meet State law provisions referenced in OMC 18.40.080.B.
- xi. All service connections and on-site utilities including wires and pipes must be located underground unless there is a compelling public purpose to the contrary. Meters may be attached to buildings. Project proponents are required to coordinate with the local electric utility provider to locate electrical service facilities in the least obtrusive way.
- xii. Show the locations of all mechanical equipment and utility vaults on both site plan and landscape plans early in the process.



Figure 18.120.020.F.2.b. Place utility meters in less visible locations. Note that this example is acceptable on a service alley but not near a street or residential walkway.

b. Screening of mechanical equipment and service areas. Where screening of service areas is required because the City determines that the provisions of 18.120.020.F.2.a cannot reasonably be met, adhere to the following:

- i. A structural enclosure shall be constructed of masonry, heavy-gauge metal, or decay-resistant composite wood. The walls must be sufficient to provide full screening from the affected roadway, property or use. The enclosure may use overlapping walls to screen dumpsters and other materials. Gates shall be made of heavy-gauge, site-obscuring material. The materials, colors and design of the enclosure shall be compatible with those of the principal structure.
- ii. The service area shall be paved and run-off managed according to City requirements.

3. REQUIREMENTS FOR SOLID WASTE STORAGE AND REMOVAL FACILITIES.

- a. Location. Solid waste storage and removal facilities (such dumpsters and trash containers, compactors, and access for trash removal vehicles) shall be located to avoid negative visual, auditory (noise), olfactory, or physical impacts on the street environment and adjacent residentially zoned properties. The City may require evidence that such elements will not significantly impact neighboring properties or public areas.
- b. Security. Other provisions of this Section 020.F notwithstanding, waste storage areas used by residents shall be located and configured to avoid entrapment areas and other conditions where personal security is a concern. The City may require pedestrian-scaled lighting or other measures to enhance security.

- c. Access for service vehicles. All developments must provide adequate access to solid waste and other service providers to remove solid waste and perform maintenance. Access must account for the type of vehicles and equipment used to perform the service. Project proponents are required to coordinate with the City's Public Works Department Solid Waste Division to identify adequate service access for waste disposal and other services to the proposed development and adjacent properties.

020.G. MULTIFAMILY OPEN SPACE

I. INTENT

- To promote the health and well-being of downtown residents.
- To create usable space that is suitable for leisure or recreational activities for residents.
- To create open space that contributes to Downtown's residential setting.

2. REQUIREMENTS

- a. Amount of required residential open space. All new multifamily buildings must provide on-site open space of at least 100 square feet per dwelling unit or 10% of residential unit floor area, excluding hallways and common areas. This provision does not apply to remodeling of existing residential units or additions to existing residential structures.

Acceptable types of open space include:

- i. Common open space. "Common outdoor open space" includes landscaped courtyards or decks, entrance plazas, gardens with pathways, children's play areas, pools, water features, accessible areas used for storm water retention or other multipurpose recreational and/or green spaces. Special requirements for common open spaces include the following:
- Required setback areas shall count as part of the required open space requirement only if they meet the requirements below.
 - The open space shall have a minimum dimension of 15' to provide functional leisure or recreational activity. (Exception: The City may exempt special purpose recreation facilities such as bocce or tetherball courts, children's play areas and vegetable gardens from this dimensional requirement provided the space is appropriate and specially configured with appurtenances for the use.)
 - The space shall feature paths or walkable lawns, landscaping, seating, lighting, play structures, sports courts raised garden beds, or other pedestrian amenities to make the area more functional and enjoyable for a range of users.
 - The space must be oriented to receive sunlight, face east, west or preferably south, when possible.

- Landscaped internal pathways may be counted as “common open space” if they meet the other criteria.
- Common open space shall be separated from ground level windows, streets, service areas and parking lots with landscaping, low-level fencing, and/or other treatments as approved by the City that enhance safety and privacy for both the common open space and dwelling units.
- The space must be accessible to all residents and, as appropriate, from public streets and sidewalks. The space must be oriented to encourage activity from local residents.



Figure 18.120.020.G.2.a. The privacy and identity of these ground related residences are achieved by the fencing, landscaping, and slight elevation of the porches.



Figure 18.120.020.G.2.b. Good examples of common open space, including street level courtyards (pictures on top and lower left), a children’s play area (lower right), and a pedestrian corridor (top).

ii. **Balconies.** To qualify as open space, balconies or patios must be at least 35 square feet,

with no horizontal dimension less than 5’.

iii. Rooftop Decks and Terraces. Decks and terraces may be used to meet open space requirements, provided the following conditions are met:

- Space must be accessible to all dwelling units and ADA compliant.
- Space must provide amenities such as seating areas, landscaping, and/or other features that encourage use, as determined by the City.
- Space must feature surfacing which enables residents to walk on and use the open space.
- Space must incorporate features that provide for the safety of residents, such as enclosures and appropriate lighting levels.



Figure 18.120.020.G.2.c. An attractive roof deck.

iv. On-site indoor recreation areas such as exercise studios, lounges, and play areas may be used to meet the required open space provided the following conditions are met.

- Space must be accessible (ADA) and walkable to all dwelling units.
- The space is designed for and includes equipment for a recreational use (e.g., exercise, group functions, etc.).



Figure 18.120.020.G.2.d. An indoor recreation space. Other less intensive spaces are also appropriate provided that they are primarily for leisure, social, physical or recreational activities.

020.H. SITE PLANNING FOR SECURITY

I. BACKGROUND AND INTENT

Crime Prevention through Environmental Design (CPTED) principles of design address elements of the built environment ranging from small-scale elements (such as the strategic use of shrubbery and other vegetation) to larger scale considerations such as the configuration of buildings and open spaces and public realm aspects of an entire urban neighborhood. This section incorporates CPTED principles with the intent to:

- Increase personal safety and property security.
- Assist law enforcement within the Downtown.
- Provide an indication that unlawful and uncivil behavior is not permitted.

2. REQUIREMENTS

- Entrapment areas.** Ensure that the development does not include “entrapment areas”, where a person could become trapped with no exit route. Provide two means of egress from all outdoor spaces or a wide and visible exit to allow a person to avoid a threat. Ensure entrapment conditions are avoided in the design of rooftop decks, parking areas, waste recycling stations and other areas frequented by residents and workers.

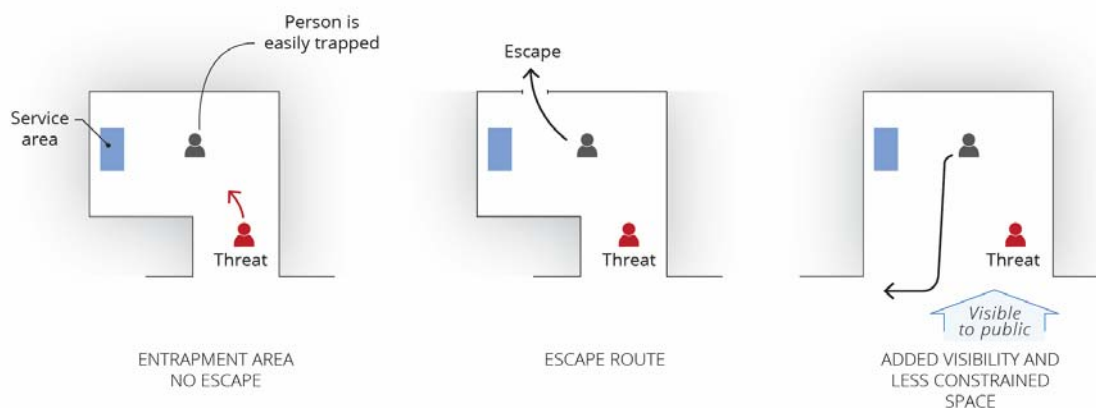


Figure 18.120.020.H.2.a. Measures to avoid entrapment areas

- Visibility.** Ensure that all accessible areas that are well lit and/or visible from a public space or right-of-way. Provide lighting per Guideline 18.120.030.F. Site Lighting.
- Visual obstruction.** Size vegetation and fences to allow visibility into occupiable open space, pathways and building entries. Avoid the inclusion of buildings, vegetation, or other objects (e.g., a storage enclosure) that block visibility into a space or provide places to hide. Opaque front yard fences invite burglary because a thief can enter a house without

fear of being seen.



Figure 18.120.020.H.2.b. Fences that prevent visibility from public ROW and open spaces can decrease security.

- d. Motorists' view. Ensure that screening or landscaping does not block motorists' views of pedestrians crossing streets, driveways, and vehicular circulation areas. Where visibility is necessary to avoid creating an unsecure area to reduce the potential for pedestrian/vehicle collisions, select vegetation that will not obstruct views between 3 feet and 8 feet above the ground. (See Figure 18.120.020.H.2.c.)

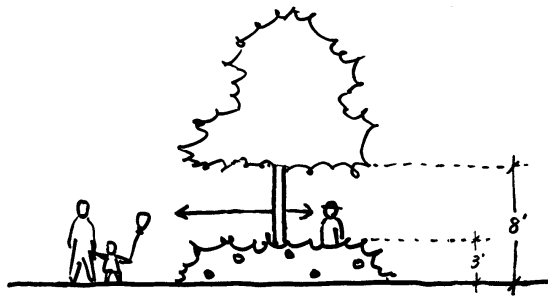


Figure 18.120.020.H.2.c. Keep landscaping open between 3 feet and 8 feet above grade where there is the need for visibility.

- e. Passive surveillance. In the planning of the site and design of buildings and site elements, to the extent feasible provide for "passive surveillance," the ability of people occupying buildings and public spaces to view all parts of accessible spaces.

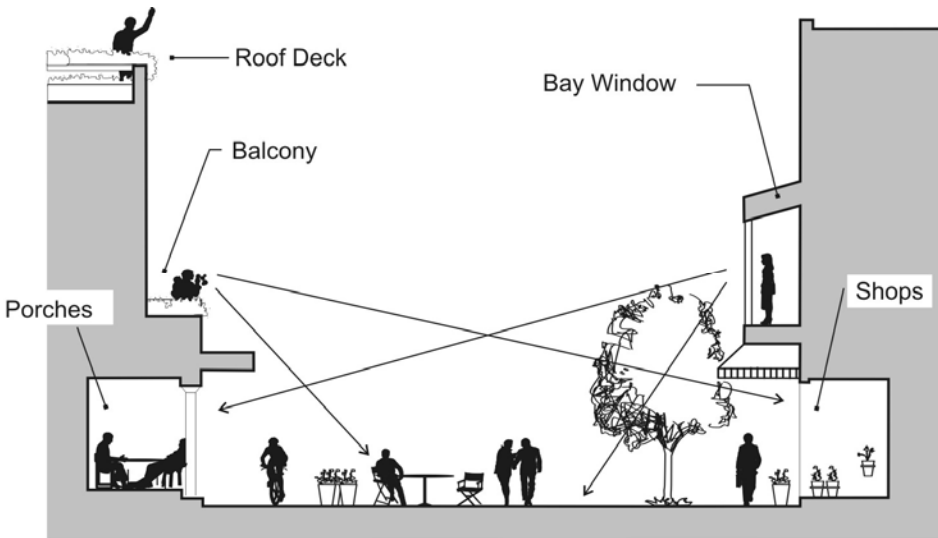


Figure 18.120.020.H.2.d. Passive surveillance or the ability of people in buildings or traveling along roadways to see outdoor spaces, increases security.

- f. **Access control.** Provide appropriate natural access control. Employ features that delineate where the general public should not enter without an invitation. For example, a low fence or hedge can indicate that people should not enter a yard or open space except through a gate or opening. Access control should not limit visibility or passive surveillance.
- g. **Territorial definition.** Clearly indicate through site planning and design measures what parts of the site are open to the public and what parts are not. For example, in commercial development, pedestrian-oriented elements and walkways indicate that the public is welcome but fenced areas generally indicate a private space.



Figure 18.120.020.H.2.e. Note how this complex employs both territorial definition in the foreground seating area and access control in the gate towards the rear of this picture.

- h. **Maintainability.** Incorporate easily maintained and vandal resistant elements. Well maintained sites and buildings indicate that someone cares for the site and tends to

discourage crime. See also guideline 18.120.040.F.2.a that addresses durable building materials.



Figure 18.120.020.H.2.f This residential complex incorporates passive surveillance, territorial definition, and good visibility and lighting to provide a more secure pathway and open space.

18.120.030 SITE ELEMENTS AND DESIGN

2017-12-29

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030.A. INTRODUCTION

This Section 18.130.030 addresses the size and design of site elements such as landscaping, parking areas, pathways, open spaces, site furniture and fences. Whereas Chapter 18.120.020 addresses the location and configuration of such elements within the framework of site planning, this chapter adds provisions for their physical characteristics.

This section does not apply to site features in the public right of way.

030.B. DESIGN OF PATHWAYS AND CIRCULATION ELEMENTS

I. INTENT

- To provide safe and direct pedestrian access that accommodates pedestrians of all ages and abilities, minimizes conflicts between pedestrians and vehicular traffic, and provides pedestrian connections to neighborhoods.
- To accommodate bicyclists riding in an appropriate manner.
- To provide attractive internal pedestrian routes, promote walking and enhance the character of the area.

Notes: The terms “pathway” and “walkway” are interchangeable in this document and refer to paved linear routes for pedestrian circulation.

The provisions of this Section 030.B do not apply to the areas added to sidewalks to meet the setback requirements to achieve a 12 or 10 foot wide sidewalk in 18.120.020.B.3, 4, and 5.



Figure 18.120.030.B.2.a. Nicely designed pedestrian ways are important in urban areas.

2. REQUIREMENTS

- a. Pathway widths. Pathways in developments (not within the public right of way) must be sufficiently wide to serve their intended purpose and level of use.
 - i. All pathways must be at least 4 feet wide with widened areas for two wheel chairs to pass, except for pathways for very occasional use such as paths to a service area serving a single residential unit.
 - ii. Pathways to main entrances to commercial businesses should be at least 6 feet wide.
 - iii. Pathways to assembly type spaces such as theatres and auditoriums where heavy pedestrian traffic is anticipated must be at least 10 feet wide.
 - iv. Pathways where bicycle traffic is anticipated must be at least 10 feet wide with 2 feet wide shoulders and signed and configured for safe pedestrian traffic as well as bicycle movement.
 - v. All required pathway width dimensions above are to be clear of permanent

obstructions. If the pathway is adjacent to angle or parallel parking, provide at least 2 extra feet to account for car overhang. (See 18.120.030.C.2.c)

vi. See Section (d). Pedestrian circulation where facades face parking areas for pathway requirements for pathways along building fronts are adjacent to parking areas.

- b. Pathway safety. Provide clear visibility along the pathway in developments (not within the public right of way). For safety and wayfinding, landscaping must not block visibility to and from a path in a way that conflicts with the guidelines in 18.120.020.H. Maximize visibility where paths cross vehicle routes such as alleys and driveways so that pedestrians and drivers can clearly see each other.
- c. Pathway enhancements. Where pathways in developments (not within the public right of way) are within 5 feet of a “blank wall” (measured perpendicularly to the wall) at least one of the following measures must be incorporated:
 - i. Blank wall treatment per 18.120.040.H, or

- ii. A landscaped strip, at least 3 feet wide between the wall featuring shrubs, trees, and/or vines as approved by the City and conforming to Chapter 18.36 Landscaping and Screening.



Figure 18.120.030.B.2.b A positive example of a building/pathway relationship meeting guideline 2.c directly above.

- d. Pedestrian circulation where facades face parking areas. Where a building's main entrance or a pathway to a primary entrance faces an off-street parking area rather than the street, provide wide pathways with street trees and pedestrian lighting adjacent to the façades of retail and mixed-use buildings. Pathways must be at least 12 feet wide to allow for 8 feet minimum unobstructed width and landscaping and include the following:
 - i. Trees, as approved by the City, must be spaced as directed by the City dependent on the size of the tree at maturity. Trees must be placed in grates or landscaped planting beds. Breaks in the tree coverage will be allowed near major building entries to enhance visibility. However, no less than 1 tree per 60 lineal feet of building façade must be provided. Screening between the building and the parking lot does not count as part of interior parking lot landscaping per 18.36.180.C.
 - ii. Ensure that overhanging perpendicularly or angled parked cars do not infringe on the sidewalk or landscaped area.
 - iii. Lighting must conform to Section 18.120.030.F. Site Lighting.



Figure 18.120.030.B.2.c Left: Illustration of requirements for a building front facing a parking area. Right: Example of a successful pedestrian sidewalk between parking lot and storefront.

- e. Separation between pathways and ground related residential units. For residents' privacy and to maintain a comfortable transition between the pathway and the interior of the residence, residences with ground floor living spaces facing a pathway the building must be set back at least 10 feet from the pathway and/or raised at least three feet above grade. The area between the residence and the path must be either landscaped or stairs/entry or feature a deck, porch or other space for outdoor living.

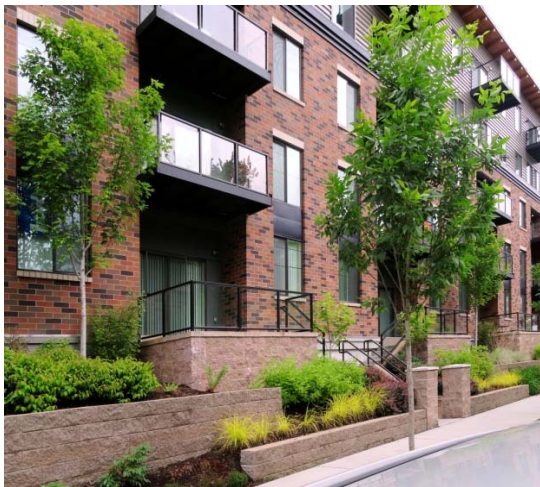


Figure 18.120.030.B.2.d. The raised floors and setbacks from a pathway provide privacy for the residents and an inviting transition from the public realm to the private living space.

030.C. PEDESTRIAN ORIENTED OPEN SPACE

I. INTENT

- To ensure that accessible open spaces, whether public or private, are safe, comfortable, usable and attractive.
- To provide spaces for informal activities such as picnicking, relaxing, meeting friends, and, as appropriate, children's play.

2. REQUIREMENTS

- a. Pedestrian oriented space elements. Providing "Pedestrian-Oriented Open Space" is an option for satisfying some requirements in sections 18.120.020.B.3, 4, and 5 (Allows buildings on type A, B, and C streets to be set back from a public ROW on pedestrian oriented streets if the setback area meets pedestrian oriented open space standards.) Pedestrian oriented open space used to satisfy these requirements must meet the following I through iii.

i. Required Pedestrian-Oriented Open Space features:

- Visual and pedestrian access into the site from a street, private access road, or non-vehicular courtyard.
- Visual access from some dwelling units and/or commercial areas (i.e., maximize "eyes on the open space").
- Paved walking surfaces of either concrete, walkable crushed rock, or approved unit paving.
- Lighting conforming to Section 18.120.030.F. Site Lighting.
- Spaces must be located in or adjacent to areas with significant pedestrian traffic to provide interest and security, such as adjacent to or visible from a building entry.
- At least 2 feet of seating area (a bench or ledge at least 16 inches deep and appropriate seating height) or one individual seat per 60 square feet of plaza area or open space.
- Landscaping components that add visual interest and do not act as a visual barrier. This could include planting beds, potted plants, or both.

ii. Desirable Pedestrian-Oriented Open Space features:

- Pedestrian amenities, such as a water feature, site furniture, artwork, drinking fountains, kiosks, children's play areas or other similar features.
- Adjacent buildings with transparent window and doors covering 75 percent of the

façade between 2 feet and 8 feet above the ground level.

- Solar access, at least during noon and afternoon hours during winter, and appropriate shade during summer.
- Pedestrian weather protection, alcoves, seating, or other features along building edges to allow for outdoor seating areas and a planted buffer.

iii. A Pedestrian-Oriented Open Space must not have:

- Unscreened adjacent parking areas or service areas (e.g., trash areas). Such features must be separated with Type I or Type II screening as described in OMC 18.36.060.
- Adjacent chain-link fences.
- Adjacent "blank walls" without "blank wall treatment." (See 18.120.040.H)
- Outdoor storage that does not contribute to the pedestrian-oriented environment.
- Vehicle travel through the area, except for emergency vehicles and those related to open space activities, such as food trucks.
- "Entrapment areas", such as a dark or blind alcove where a person might not be able to escape a dangerous situation. (See 18.120.020.H.)
- Areas which are not visible from a right of way or adjacent uses. (i.e.: make sure that there are no areas where illegal activities cannot be observed. See 18.120.020.H.)

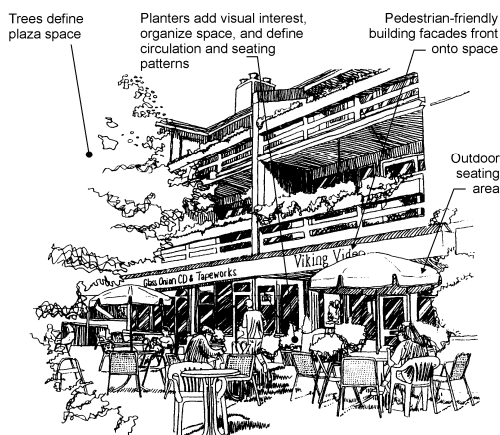


Figure 18.120.030.C.2.a. Illustration of favorable elements of a Pedestrian-Oriented Open Space and example of a successful space on the right.

030.D. SITE LANDSCAPING

I. BACKGROUND AND INTENT

Chapter 18.36 OMC establishes development standards for landscaping as part of site development. The chapter includes requirements for:

- Material and installation standards such as plant size and quality, coverage, irrigation, performance assurance and maintenance.
- A landscape plan submitted with the project application that the City reviews for conformance.
- Screening types and perimeter landscaping.
- Parking lot landscaping.

These guidelines do not supersede Chapter 18.36 OMC. They augment those standards with design requirements specific to the Downtown. Where there is a conflict between the provisions of this section and Chapter 18.36, City will determine which provisions apply. These guidelines do not apply to landscaping in the public right of way. For the purpose of this section “landscaping” includes plant materials and their beds, pavements, walls and fences, water features (except shorelines covered by the City’s Shoreline Master Program), site furnishings, trellises, planter boxes, “green walls” and related elements.

Intent:

- To support the provisions of Chapter 18.36 OMC: Landscaping and Screening.
- To make landscaping, including plantings, ground forms, natural elements paving and hardscape, site furniture and other site features an integral part of the overall site and community design.
- To complement the architecture, other site elements and the visual appearance of the neighborhood, as well as the Northwest environment.
- To encourage compositions of plant materials and site features that create an appropriate visual character, such as formal, informal, or natural.
- Use landscape design to help define a distinctive design character that distinguishes different Downtown Design Districts and Downtown Olympia as a whole.
- To reduce the summertime heat and glare buildup within and adjacent to parking lots.
- To improve the aesthetic appearance of parking lots by providing screening and landscaped areas to soften the expanse of paved areas.
- To reduce landscape maintenance requirements and help ensure that the landscape elements are attractive and healthy over time.

2. REQUIREMENTS

- a. Submittals - Landscape Character. In the design of landscape features, incorporate elements and characteristics relevant to the applicable Downtown Design District. The applicant must describe in narrative and in the landscape plan (if required by OMC 18.36.080) form how the project addresses and supports the landscape design characteristics described below for the applicable District. The City may require revision of an application if it does not substantially address the criteria in this guideline.

While this requirement includes no explicit physical standards, it is intended to allow the City and the applicant to discuss the general qualities of the proposed site development and how it relates to its surroundings and the objectives listed.

The desired landscape characteristics of each District are:

CORE, SOUTH CAPITOL, AND NORTH CAPITOL Districts

General Character and Intent

Because this area is envisioned to be relatively intensively developed, landscaped areas are likely to be limited. Therefore, landscape elements should enhance building forms and provide smaller scale visual interest. More formal (that is, characterized by uniform plantings in a liner or geometric pattern, plants pruned into regular shapes and symmetric landscape layouts with straight or pathways) rather than naturalistic landscape layouts (with irregular or asymmetric site plans, irregular paved areas, meandering pathways and plants featuring a variety of shapes and textures) will be appropriate.

Plant Materials

Sturdy, ornamental trees and shrubs to complement building elements are most appropriate. Plantings can be structural, space defining features.

Pavements

High quality pavements such as concrete and pavers are appropriate. Asphalt and gravel are not.

Furniture and Site Furnishings

Site fixtures such as walls should complement the materials and character of the development's building. Furniture should reflect the historic character of the Core and Capitol Way Corridor. Lighting should be consistent with the historical character of street improvements and surrounding architecture.



Figure 18.120.030.D.2.a. Relatively formal space defining landscaping is appropriate where there is room in the Core and along Capitol Way.



Figure 18.120.030.D.2.b. Small scale landscape enhancements are also appropriate Downtown if they are maintained.



Figure 18.120.030.D.2.c. Landscaping can be very effective in enhancing and softening formal building elements, especially in more intense areas such as the Core and along Capitol Way.

WATERFRONT District

General Character and Intent

The waterfront, especially in the Percival Landing segment, features a variety of landscape characters ranging from naturalistic compositions of grasses and driftwood to lawn areas with floral edges and small areas with special plantings. Seasonal displays to enhance specific areas (typically outdoor dining areas) add color. Development improvements should reflect these characteristics.

Sites along East Bay, especially if they are separated from the water, should feature more extensive landscaping. Because Market Drive NE is a “Downtown Entry Street, greater emphasis should be placed on screening of parking areas per OMC Chapter 18.36 and on landscaping that complements the buildings and streetscape.



Figure 18.120.030.D.2.d. Existing landscaping in the Waterfront District features a wide variety of plantings and characteristics. Note the mix of seasonal floral and naturalistic plantings, and the backdrop of large evergreen trees.

Plant Materials

For properties along West Bay and Percival Landing a wide range of landscape character may be appropriate, depending on the available space, pedestrian activities and adjacent buildings. On sites facing East Bay, plant materials should reinforce streetscape design and provide screening of unsightly areas. All plants should be salt and ground water hardy.

Pavements

Pavements along the esplanade should conform to the standards established by the City. Other pedestrian walking surfaces should continue the esplanade standards or feature decorative pavement to accentuate special areas.

Furniture and Site Furnishings

Functional and weather resistant furniture will be most appropriate. If a more specialized design is desired, reflect a maritime character.

ART/TECH District

General Character and Intent

The Art/Tech District is intended to provide for a mix of robust activities that may require frequent deliveries and outdoor work space. The character should reflect that quality, so landscape features should be hardy and limited to those areas not needed for active uses. At the same time, the visual character of the area can be greatly enhanced with compact landscape screening and plantings in areas where people will pass or congregate, such as building entrances and outdoor seating areas. Because of the envisioned variety of site uses and conditions, individual, site-specific landscape solutions may be necessary and desirable. Therefore, a wide variety of plant materials and site fixtures may be used, provided that they are durable and maintainable.



Figure 18.120.030.D.2.e. Some very informal landscaping treatments that would be appropriate in the Art/Tech area but not necessarily in other Districts.

Some developments in the Art/Tech District, such as those below in Figure 18.120.030.D.2.f, include much more extensive garden areas that act as oases in an otherwise industrial setting. Such gardens with extensive plantings, special features and art are certainly welcome. The guidance below is primarily for sites with more intensive industrial activities.



Figure 18.120.030.D.2.f. Attractive gardens that provide excellent examples of oases within an otherwise industrial setting. This intimate juxtaposition of workshop and garden experiences is quite unique to Downtown Olympia and a major asset.

Plant Materials

A wide variety of plant species are appropriate in this District but should be maintainable in the areas they are planted. Large scale trees, used to soften the “semi-industrial” character of this area are also desirable where there is room for the crown and the root system. On sites where vehicle traffic is expected, plantings and their root systems should be protected with measures such as raised beds, bollards, and special soil treatments.

Pavements

Utilitarian pavements such as asphalt and concrete are appropriate, but artistic pavements and details are also encouraged, especially if they relate to on site activities.

Furniture and Site Furnishings

A wide variety of furnishings are acceptable. Lighting can be utilitarian or industrial in character but should not produce glare, especially onto adjacent properties.

ENTERTAINMENT District

General Character and Intent

There is generally very little space for substantial landscaping in the Entertainment District because most buildings front directly on the street. There are exceptions, such as City Hall, which does feature a small, nicely appointed plaza. The intent of the guidelines for the Entertainment District is to create an especially vibrant, colorful day and nighttime pedestrian environment with a lot of interesting details. Small scale landscape features such as baskets and planters can play a role in creating such an experience.



Figure 18.120.030.D.2.g. Plant beds in front of City Hall. Note the raised beds that enable easy maintenance and the bands of ornamental pavement.

Plant Materials

Small scale ornamental plants would add color and texture to the area.



Figure 18.120.030.D.2.h. Small scale plantings that add pedestrian interest. Note how the simple planter with grass defines the space and makes it more inviting.

Pavements

Standard concrete pavements are acceptable, but since most private open spaces and entry areas will likely be small, unique and high quality, pavements such as tile and terrazzo are encouraged to add pedestrian interest without a lot of cost.

Furniture and Site Furnishings

Furniture must be easily maintainable and made of durable materials. Ideally, the character of site furnishings and lighting should relate to the building's architecture.

SOUTH RESIDENTIAL NEIGHBORHOOD District

General Character and Intent

The South Residential Neighborhood District includes two distinct types of land uses: 1) single and multifamily residences and 2) office buildings, often associated with the Capitol Campus. Additionally, small retail is encouraged in some areas. Therefore, there are two alternate approaches to landscaping. For residences and mixed use projects, a mix of trees, shrubs and ground cover and perhaps some lawn are appropriate to both frame and enhance new development. Storefronts should feature pedestrian oriented facades so there may not be opportunities for landscaping if small retail shops are developed. Landscape screens may be used to enhance privacy and reduce impacts on adjacent properties, although dense screens over 4 feet high are not appropriate across the lots front facing the street. Landscape layouts can either be formal, with symmetric, space-defining plantings, or, they can be informal with irregular plantings in a more “natural” composition.



Figure 18.120.030.D.2.i. Formal residential landscaping (left) and informal landscaping (right).

New office buildings may either feature one of the residential options, or if there is sufficient open space on the site, new developments may emulate the Campus’s “Olmstedian” landscape character. The term “Olmstedian” refers to the Campus’s original landscape designers, the Olmsted brothers, whose aesthetic features asymmetric plantings of large trees, curvilinear pathways and earth forms, and relatively large lawn areas.



Figure 18.120.030.D.2.j. The Capitol Campus reflects an “Olmstedian character informal groupings of large trees, curvilinear pathways and lawn areas.

Plant Materials

A wide range of plant materials is appropriate for residential development . Residential lots may feature formal or informal gardens, lawn areas, or more naturalistic landscaping emphasizing native species. For non-residential development , if the Olmstedian landscape character is selected, large (signature) trees and lawn areas should predominate with shrubs used as building enhancements and space defining elements. The use of landscaping to facilitate the transition between large and small buildings or between residential and commercial development is encouraged.



Figure 18.120.030.D.2.k. An “Olmstedian” landscape design approach applied to an urban setting.

Pavements

Well constructed concrete pavements are generally most appropriate but unit pavers or decorative elements are also encouraged, especially if they can add interest to multi-family residences or office complexes.

Furniture and Site Furnishings

Site furniture is not necessary in residential development, although trellises, canopies, attractive gateways and other site features can enhance a residential complex. All site lighting, including parking lot lighting must be pedestrian scaled with luminaires less than 15' above grade.



Figure 18.120.030.D.2.i. An (unintended) multi-use gateway cum play structure.

- b. Parking lot landscaping. Adhere to standards in OMC 18.36.180 or, as an alternative, parking lot perimeter screening may consist of the following (interior parking lot landscaping is still required):

Provide a planting bed, at least 5 feet wide that incorporates a low masonry wall, 2-1/2 feet to 3-0 feet tall with landscaping and/or trellis as described below. The planting bed shall be on the street facing front of the wall, provide irrigation and feature the following plantings:

- i. Deciduous and/or evergreen trees spaced to create a continuous canopy (approximately 30 feet on center or as directed by the City). Alternatively, a trellis and shrubs, as in Figure 18.120.030D.2.m may be substituted for the trees.
- ii. Shrubs provided at the rate to provide coverage of the planting strip within three years of planting.
- iii. The wall shall be constructed of brick, stone, decorative concrete or concrete block, or other permanent material that provides visual interest and helps to define the street edge as determined by the City. (See Figure 18.120.030D.2.j. for an example). The wall and bed must be relatively continuous but may feature breaks at key points for pedestrian access.

- iv. The City may require breaks in the wall and planter strip to facilitate pedestrians entering the site.

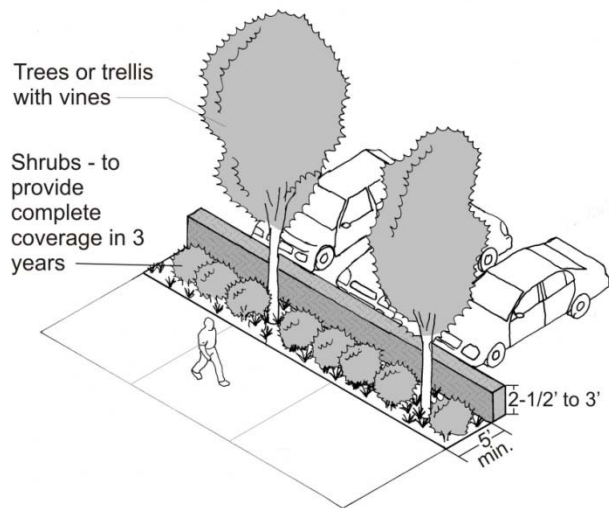


Figure 18.120.030D.2.m. Parking area planting buffer with low wall and landscaping. Illustration with trees (left) and example with trellis (right).

- c. Green walls. Green walls are acceptable in all Districts except the in the Downtown Historic District or on designated historic properties. A green wall may be acceptable in these instances if the City determines that it does not detract from the site's historic character. Green walls must be properly irrigated and maintained.
- d. Stormwater management techniques. Rain gardens and other landscape techniques to reduce stormwater runoff are encouraged in the South Capitol Way and South Residential Neighborhood Districts as these areas are sufficiently high in elevation to potentially make such measures effective.
- e. Sea level rise considerations. Of particular importance is the potential for sea level rise. While this is not an issue for short lived plants, it is a consideration in the selection of trees and more permanent plantings

030.E. DESIGN OF PARKING AREAS

I. INTENT

- To reduce the visual presence of parking on Downtown streets, public space and adjacent development.
- To increase pedestrian access.
- To augment the provisions of OMC 18.36.180 and 18.38.220.

2. REQUIREMENTS

- a. Parking Lot design standards. Adhere to standards in OMC 18.38.220 unless otherwise allowed in accordance with the guidelines below. See also 18.120.030.D.2.b and 18.36.180) Street Fronts for parking lot location guidelines related to specially designated streets.
- b. Pathways through Parking Areas. Developments must provide specially marked or paved pathways through parking areas. Generally, pathways must be provided at least every four rows or at least every 180 feet. Where possible, align the pathways to connect with major building entries or other sidewalks, pathways, and destinations. The pathway must be at least wide enough for two shopping carts to pass one another. This requires an unobstructed clear width excluding vehicle overhang of at least 4 feet for grocery stores but may be larger for big-box or building product stores.



Figure 18.120.030.E.2.a. Parking area pathway examples. Note that clear pathway width must account for vehicle overhang.

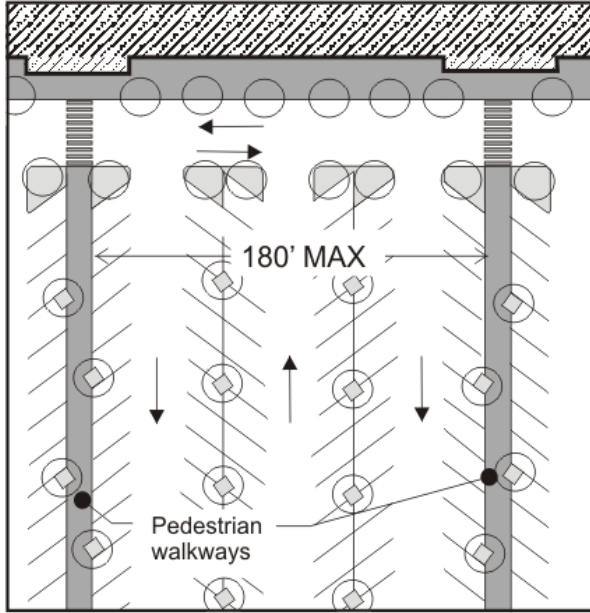


Figure 18.120.030.E.2.b. Example parking area pathway configuration.

- c. Bumper overhang next to pathways and landscaping. Where angle or perpendicular parking abuts a landscape strip or pathway, provide a 2 feet wide paved area to prevent the vehicle's overhang from infringing on the walkway or landscaping. Do not use separate wheel stops to address this issue.

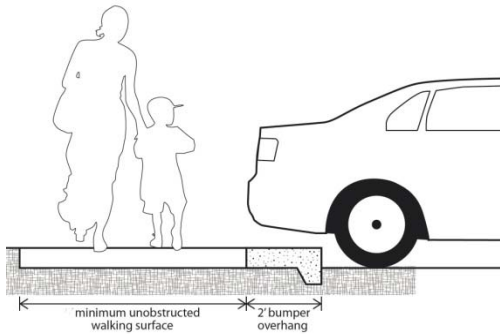


Figure 18.120.030.E.2.c. Concrete extension to accommodate bumper overhang.

030.F. SITE LIGHTING

I. INTENT

- To encourage the use of lighting as an integral design component to enhance buildings, landscaping, or other site features.
- To increase night sky visibility and to reduce the general illumination of the sky.

- To reduce horizontal light glare and vertical light trespass from a development onto adjacent parcels and natural features.
- To increase personal safety and security.
- To enhance the night time pedestrian environment, especially in the Entertainment District.
- To prevent the use of lighting for advertising purposes.
- To increase design consistency within the development.

2. REQUIREMENTS

a. Site Lighting Levels. All publicly accessible areas shall be lighted with levels as follows:

Condition	Lighting levels in foot-candles on the ground		
	Minimum	Maximum	Preferred
Low or non-pedestrian and vehicular traffic areas, private parking lots, storage areas, etc.	.2	2	1
Moderate pedestrian areas and building entries, most pathways, some pedestrian oriented open spaces, etc	1	4	2
High pedestrian areas such as building entries and areas where personal security is a concern	2	4	3
Public parking lots	1	2	1

The maximum exterior light level anywhere is 5 foot candles, measured on the ground. Lighting shall be provided at consistent levels, with an average lighting level to minimum lighting level uniformity ratio no less than 3:1, to create gradual transitions between varying levels of lighting and between lit areas and unlit areas. Highly contrasting pools of light and dark areas must be avoided.

- b. Light quality, height, and shielding. Adhere to the following unless there is a compelling reason to the contrary:
- Parking area lighting fixtures shall be fully shielded; dark sky rated and mounted no more than 20 feet above the ground (15' high maximum in the South Residential Neighborhood District), with lower fixtures preferable so as to maintain a human scale.
 - Exterior lighting must not spill onto adjacent properties.
 - Pedestrian lighting shall have a maximum height of 15 feet.

- c. Architectural Lighting. Steady, non-flashing lighting of building features, artwork, and special landscape elements may be allowed, subject to the findings of the City that the light causes no significant adverse impact.
- d. Character of light fixtures and mounting. The design character of site and building mounted lighting must be consistent with the site's architecture. Alternatively, the design of site and building mounted lighting may be unobtrusive without embellishment or specific character, provided the lighting fixture's visibility is minimized.

030.G. THE DESIGN OF OTHER SITE FEATURES

I. INTENT

- To ensure that site features such as walls, fences, poles, gates, screens, are well constructed and easily maintainable
- To enhance the pedestrian environment and the general appearance of Downtown.

2. REQUIREMENTS

- a. Fences and walls.
 - i. Except where full screening of service areas, utilities, mechanical equipment or other facilities is desired, fences must not obscure building fronts or attractive site features.
 - ii. Walls or fences between a primary building and a public right of way must extend no higher than 4 feet above sidewalk grade unless no other option is available due to topography or other physical constraint. Avoid solid walls or sight obscuring fences higher than 3 feet above grade where visibility is important for security.
 - iii. Fences and walls should be made of durable, easily maintainable and vandal resistant materials. To reduce the likelihood of graffiti, avoid walls with smooth surfaces unless they can easily be repainted or cleaned.
 - iv. Chain link fences are prohibited along the front of properties in the Downtown.
 - v. Wooden fences are prohibited in the Core, Entertainment and South Capitol Districts as they are less permanent and do not contribute to the historic and urban character desired there.

- vi. Where a retaining wall adjacent to a public sidewalk is necessary for development, step the wall back so that the top of the wall surface closest to the sidewalk is no more than thirty inches high. If greater height is required construct terraces no more than 30 inches high with at least 18 inch horizontal steps between each terrace riser. See Figure 18.120.030.F.2.a. The City may allow other configurations, such as a higher wall set back from the sidewalk and landscaped provided such a wall is necessary for development and the objectives of this section are achieved.

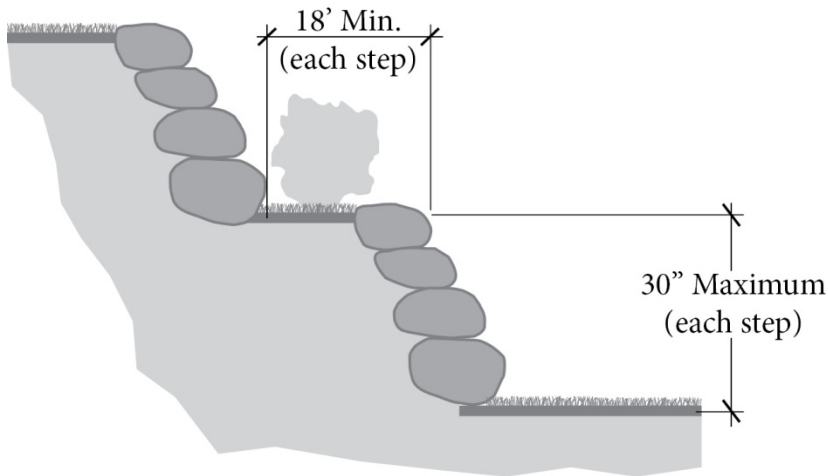


Figure 18.120.030.F.2.a. Allowable terrace configuration for wall or rockery adjacent to a public sidewalk. Note that this illustration shows a stone wall but other masonry and concrete walls are acceptable as well.

- b. Poles and other vertical elements. Minimize vertical site elements except for those intended specifically to add to the pedestrian environment.
- i. Underground all site utilities.
 - ii. Wires and other hanging features are prohibited except for otherwise allowed temporary features such as permitted signs.
 - iii. See also guidelines for service areas and mechanical equipment, Section 18.120.020.F
- This guideline (b) does not apply to desirable vertical elements intended to enhance the pedestrian environment such as weather protection, flag poles, banners, supports for hanging flower baskets, trellises and supports for green walls.

18.120.040 BUILDING DESIGN

2018-02-15

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040.A. INTRODUCTION

This section addresses the function, quality and character of building exteriors. Its purpose is to:

- Improve the pedestrian environment.
- Increase the distinctiveness and individuality of the Downtown Design Districts.
- Produce buildings that are more durable, easily maintainable and vandal resistant.
- Enhance buildings' functionality, especially as it relates to the function of buildings' facades and entries.
- Add to Downtown's overall visual quality.

040.B. BUILDING CHARACTER

I. INTENT

- To reinforce a distinct building design character for each of Downtown's character areas.
- To retain Downtown's diversity of design character.
- To create more design continuity and compatibility between near-by building facades.
- To support preferred uses in each of the districts.
- To coordinate with street design measures to create attractive and functional streetscapes.

- To allow for creativity in the design of new buildings.

2. REQUIREMENTS

- a. Submittals. The applicant must identify which Design District (District) identified in 18.120.010.C the project is consistent with. That District will be the basis for all the requirements in this section. The applicant must describe in narrative and graphic form how the project addresses and supports the architectural characteristics described below for the applicable District. The City may require revision of an application if it does not substantially address the criteria in this guideline.

If the project application is for a building remodel or addition to an existing structure the applicant may either describe how the proposal reflects the desired characteristics of the applicable District or how the new building elements positively relate to the architectural character of the existing building.

While this requirement includes no explicit physical standards, it is intended to allow the City and the applicant to discuss the general qualities of the proposed building and how it relates to its surroundings and the objectives listed. Other building design guidelines include directions for specific aspects of building design.

Core District: The early 20th century architecture of the historic retail core sets the design foundation of this Design District. New buildings in the core should reinforce its historic character which features excellent examples of classically detailed buildings and early 20th Century storefronts. Buildings may be modern in appearance, materials and detailing but should reinforce the historic qualities by incorporating some of the design characteristics:

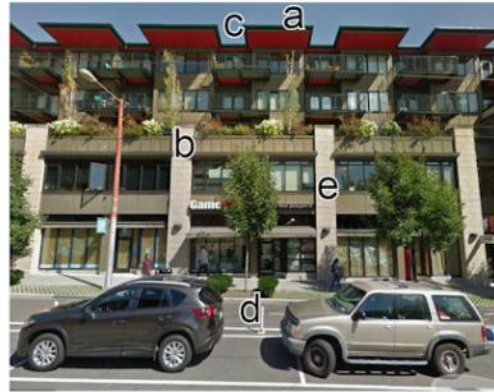
- a. A vertical façade composition with a top, middle and base.
- b. Building modulation and upper story setbacks.
- c. Groupings or regular patterns of vertically oriented windows.
- d. Reinforcement of the spatial qualities of the street by locating the front façade on or near the front property line – or by including pedestrian oriented space along the front property line.
- e. Traditional materials such as brick, tile, and concrete in the front façade.

Existing Character Defining Buildings

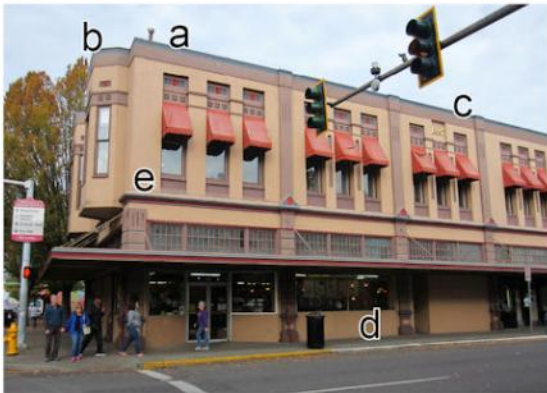
Examples of New Buildings with Desired Characteristics



Classic styled building



Contemporary styled building with elements that complement the core



Nicely detailed (except for ground floor remodel) commercial building






A new traditionally styled building

South Capitol Way. The architectural design objectives for development along South Capitol Way is to enhance the visual connection between the State Capitol Campus and the Core. Capitol Way is an Type A Pedestrian Oriented Street (See 18.120.B.3). Similarly, the objective along Plum Street (which is a Downtown Entry Street as described in 18.120.B.8) is to encourage high quality buildings and an attractive streetscape as an entry into Downtown. Since the building design emphasis for both streets is placed on substantial, high quality buildings, the South Capitol Way design character is an option on sections of Plum Street. These objectives should be accomplished by including some of the design characteristics and elements below.

- a. Substantive new buildings with high quality, permanent materials.
- b. Welcoming entries or plazas with landscaping.
- c. Artwork and other pedestrian amenities.






- d. Ground floor pedestrian activities, especially on Capitol Way.
- e. A façade that enhances the street wall by having some portions of the front façade on or near the property line.
- f. Building modulation and upper story setbacks.
- g. Special corner treatment (e.g.: corner entry or building element) at intersections.

Existing Character Defining Buildings	Examples of New Buildings with Desired Characteristics
 <p data-bbox="313 800 521 825">New mixed-use building</p>	 <p data-bbox="727 800 951 825">Hotel with corner feature</p>
	 <p data-bbox="727 1119 886 1144">Mixed-use building</p>
	 <p data-bbox="727 1434 886 1459">Mixed-use building</p>

North Capitol Way. This area is an important pedestrian connection that has significant development opportunities. Currently there is a wide variety of building types in the area with the industrial vernacular of the new market buildings standing out. The goal is to enhance pedestrian connections between the Core, and the Market. To accomplish this, buildings should feature some of the following characteristics:

- a. Building elements that provide a human scale and that break up large facades,
- b. Ground floor materials with texture (such as brick or wood siding.)







- c. Relatively high level of transparency at the ground floor. (See Guideline 18.120.020.B.3 and 4.) Buildings with residences on the ground floor can address this issue by satisfying Guideline 18.120.020.B.6)
- d. Active pedestrian uses or substantial landscaping,
- e. Building modulation and upper story setbacks.

Existing Character Defining Buildings	Examples of New Buildings with Desired Characteristics
 <p data-bbox="349 829 738 850">Market - Utilitarian with nice detailing</p>	 <p data-bbox="771 829 1161 850">Small office with nice materials</p>
 <p data-bbox="349 1155 738 1186">Industrial vernacular adapted to pedestrian-oriented commercial.</p>	 <p data-bbox="771 1155 1161 1186">Mixed use with storefronts</p>
	 <p data-bbox="771 1459 1161 1480">Ground floor residential</p>

Art/Tech. Utility is an architectural design driver for the informal Art/Tech Design District, suggesting opportunities for adaptive reuse of existing buildings, and buildings that are functional for commercial, light manufacturing and artisanal activities.. The incorporation of “warm industrial” materials—weathered wood, metal, etc.—and design elements—garage doors or large entryways that engage the street, parklets, etc.—are appropriate.






Buildings in this District should incorporate some or all of the following characteristics:

- a. An eclectic character that accommodates “workshop” (e.g., light manufacturing; commercial kitchens; technical, art, and artisan studios) as well as other activities.
- b. Adaptive reuse and expansion of existing structurally sound buildings is encouraged.
- c. Buildings with simple architectural elements, loading areas, and minimal detailing.
- d. Industrial fixtures and materials that are durable including metal and wood siding, masonry, and durable panel systems.
- e. Simple or functional building forms that reflect the enclosed activities.
- f. Water and energy efficient architecture.

Existing Character Defining Buildings	Examples of New Buildings with Desired Characteristics
 <p data-bbox="310 982 495 1003">Repurpose warehouse</p>	 <p data-bbox="693 982 894 1003">Expanded older building</p>
 <p data-bbox="310 1276 548 1297">Industrial building with mural</p>	 <p data-bbox="693 1276 894 1297">New mixed use building</p>
 <p data-bbox="310 1575 418 1596">Adaptive use</p>	 <p data-bbox="693 1575 857 1596">New office building</p>


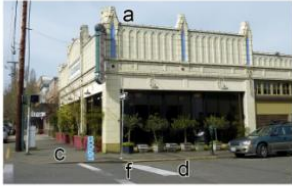


Waterfront. Better connecting Downtown to its waterfront location—visually, physically, and symbolically—is a primary goal of this District. Design elements found at Percival Landing—furnishings, light fixtures, marine hardware, wood pilings and boardwalks, rope, etc.—should be incorporated into new developments to help draw the aesthetics of the waterfront deeper into Downtown. Designs and materials might reflect additional waterfront themes of shoreline ecology, Northwest Native American history, historic industrial uses, sailing and boating, fishing and aquaculture, and commercial trading at the Port. Buildings should reinforce the maritime character by incorporating some of the following characteristics and elements:

- a. Wide overhangs facing the shoreline for weather protection.
- b. Large windows on the ground floor facing the water (Guideline 18.120.B.7 requires a pedestrian oriented façade facing the water.)
- c. Simple building forms with sloped roofs.
- d. A simple palette of façade materials. Materials with texture such as wood or metal siding are encouraged.

Existing Character Defining Buildings	Examples of New Buildings with Desired Characteristics
 <p>Waterfront restaurant with maritime character.</p>	 <p>Wide esplanade with shelter</p>
 <p>Hands-on Children's Museum</p>	 <p>Building assemblage with maritime elements</p>
	 <p>Traditional maritime industrial buildings</p>

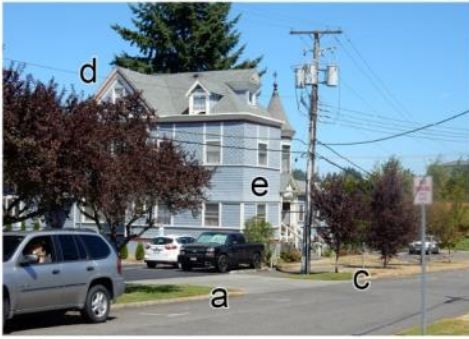

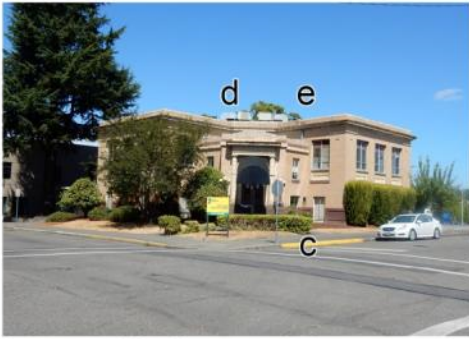


Entertainment. Art and creative expression defines this hub of dining, nightlife, and the performing arts. Architectural elements here may focus on art, theatricality, imagination, with glittery night lighting and more expressive detailing. Buildings (and streetscapes) should support entertainment and pedestrian activities. Intimate but safe pedestrian spaces, consistent and inviting lighting, windows that put “eyes on the street” and clear wayfinding are important. Architectural design should feature some of the following characteristics:

- a. Expressionistic architectural styles.
- b. Artwork.
- c. Ample pedestrian amenities above what is required in Section 040.E.
- d. Human scaled elements with colorful accents.
- e. Unique and colorful lighting.
- f. Transparency into ground floor activities.

Existing Character Defining Buildings	Examples of New Buildings with Desired Characteristics
 <p data-bbox="370 1077 621 1098">Landmark sign and ornate architecture</p>	 <p data-bbox="683 1077 837 1098">Contemporary building</p>
 <p data-bbox="370 1318 659 1339">Ornate style with excellent street presence</p>	 <p data-bbox="683 1318 837 1339">New storefront block</p>
	 <p data-bbox="683 1560 922 1581">Well-maintained renovated buildings</p>

South Residential Neighborhood. This area is envisioned as an urban residential neighborhood with an organic mix of housing and building types. While architectural variety is important to the area, an emphasis should be placed on ensuring the compatibility between old and new structures. Land use zones for some blocks allow office development. New commercial and institutional buildings should be located and designed to minimize conflict with the area's residential emphasis. New buildings should emphasize a pleasant human scaled character by featuring some of the following characteristics:

- a. A Residential building elements, especially those that contribute to pedestrian comfort, safety and ambiance such as porches, balconies, and enhanced entries.
- b. Compatibility between old and new buildings and potential impacts to existing residences. Side yard setbacks and step backs of upper stories may be necessary to protect the privacy of neighbors.
- c. Front yard setbacks with landscaping where appropriate.
- d. A variety of materials, building elements and/or roof forms.
- e. Human scaled elements.

Existing Character Defining Buildings	Examples of New Buildings with Desired Characteristics
 <p data-bbox="316 611 451 642">Older house</p>	 <p data-bbox="818 611 1138 642">5-6 story multi-family building</p>
 <p data-bbox="316 997 532 1029">Institutional building</p>	 <p data-bbox="818 997 1117 1029">3 story multi-family building</p>
	 <p data-bbox="818 1381 1235 1444">Ground related multi-family residences (townhomes)</p>

3. EXCEPTIONS AND OPPORTUNITIES FOR FLEXIBILITY

The City may allow a building with a special civic purpose (such as a public building, assembly building, museum or facility built for public activities) that does not meet the criteria above but contrasts with its neighbors to create a distinctive and high quality landmark.

040.C. SPECIAL PROVISIONS FOR BUILDINGS IN THE HISTORIC DISTRICT AND INDIVIDUALLY DESIGNATED HISTORIC PROPERTIES

I. INTENT

- To support the provisions of OMC 18.12, and the Secretary of the Interior’s Standards for the Treatment of Historic Properties (as amended).
- To reinforce the historic character and pedestrian orientation of the Core Downtown Design District. (See Section 18.120.040.B.2.)
- To maintain the Downtown Historic District as the “heart” of the community.
- To allow new positive development with a contemporary design character provided the new building does not detract from the overall historic character of the Downtown Historic District.

2. REQUIREMENTS

- a. For rehabilitation and minor additions to buildings and properties that are Individually Designated Register Properties or are Contributing Properties within the Downtown Historic District, adhere to OMC 18.105.
- b. For rehabilitation and minor additions to buildings and properties that are Non-Contributing properties within the Downtown Historic District, adhere to OMC 18.105 and the guidelines of this Chapter 18.120.
- c. For substantial additions to buildings and properties that are Individually Designated Register Properties, Contributing or Noncontributing Properties within the Downtown Historic District, and new construction within the Historic District, adhere to OMC 18.105 and the guidelines of this Chapter 18.120.
- d. If there is any inconsistency or conflict between the provisions in this Chapter 18.120 and OMC 18.105 the provisions of OMC Chapter 18.105 shall apply.

040.D. ARCHITECTURAL COMPOSITION, MASSING AND ARTICULATION

I. INTENT

- To employ façade articulation techniques to reduce the perceived scale of large buildings and add visual interest from all observable scales.
- To create a skyline that is visually interesting.
- To create clear and welcoming building entries.

- To encourage development that is compatible with buildings on the same block in terms of “architectural scale”. Architectural scale is the perceived height and bulk of a building relative to that of neighboring buildings. A building’s apparent height and bulk may be reduced by articulating or modulating façades and other means as described below.
- To add visual interest to buildings.

2. REQUIREMENTS

Note: Building elements or features used to address guidelines in Section 040.D may also be used to concurrently satisfy requirements in Sections 040.E and 040.F and vice versa, provided the elements or features are named as options in the applicable sections. Similarly, an element or detail may used to concurrently satisfy requirements in this section.

- a. Façade articulation for non-residential buildings and uses - Storefronts and other facades of buildings facing type A and B Pedestrian Oriented Streets (See Section 18.120.020.B) shall include articulation elements at intervals no greater than 50 feet to reflect the original plat divisions and to provide visual interest and variety for a pedestrian walking down a block.

At least three of the following elements shall be included on the ground floor of non-residential building facades facing A and B Pedestrian Oriented Streets and all above grade parking garages at intervals no greater than 50 feet.

- i. Window fenestration patterns and/or entries.
- ii. Use of non-continuous weather protection features.
- iii. Use of vertical piers, pilasters or columns.
- iv. Change in building material or siding style.
- v. Vertical elements such as a trellis with plants, green wall, art element.
- vi. Providing vertical building modulation of at least 12 inches in depth combined with a change in roofline modulation per subsection “d” below or a change in building material, siding style, or color.
- vii. Built in pedestrian amenities such as planters or seating.
- viii. Other design techniques that effectively reinforce a pattern of small storefronts, individual businesses or uses.
- ix. Building modulation (the stepping back or extension forward of a portion of a building façade).
- x. Roofline modulation per subsection “d” below.

Exceptions: Façade articulation is not required in the Waterfront or Art/Tech districts.



Figure 18.120.040.D.2.a. Façade articulation examples Upper buildings use substantial modulation, separate awnings, change in façade materials, and a window fenestration pattern. The lower left building features vertical piers, and a strong window fenestration pattern (note the articulated entry. The lower right image incorporates a roofline change, façade details and vertical piers with ornamentation to effectively articulate the façade.

b. Façade articulation for residential buildings and residential portions of mixed-use buildings.

Residential portions of new buildings shall include articulation features at intervals that relate to the location/size of individual units within the building (or no more than every 40 feet) to break up the massing of the building and add visual interest and compatibility to the surrounding context. At least three of the following features shall be used at intervals no greater than the unit interval (the width, facing the street, of the dwelling unit). If the unit width is less than 30 feet, then the articulation interval must be no more than 40 feet.

- i. Use of window patterns and/or entries.
- ii. Change in roofline per subsection “d” below.

- iii. Change in building material, siding style, and/or window fenestration pattern. (This option is not applicable in the Core District.)
- iv. Providing vertical building modulation of at least 12 inches in depth combined with a change in roofline modulation per subsection 18.120.040.D.2.d below or a change in building material, siding style, or color. Balconies may be used to qualify for this option if they are recessed or projected from the façade by at least 2 feet. “Juliet balconies” (balconies that are so shallow that they cannot be occupied) or other balconies that appear to be tacked on to the façade will not qualify for this option. The depth of the modulation must be at least 3 feet if not accompanied by roof modulation or a change in materials.
- v. Vertical elements such as a trellis with plants, green wall, or art element.
- vi. Other design techniques that effectively break up the massing at no more than 40-foot intervals.



Figure 18.120.040.D.2.b. Examples of articulation of residential building facades. All examples use window fenestration and vertical building modulation elements. The upper examples use a mixture of material changes and detail components to further articulate the façades. In the lower left example, the balcony element’s dramatic mix of color, material and projection are effective in meeting the standards. The lower right example adds a combination of material and color changes and building modulation to help meet the standards.

Exceptions: Façade articulation is not required in the Waterfront or Art/Tech Districts, even for residential portions of buildings.

- c. **Maximum façade width.** Building facades wider than 120 feet that face a public street or park shall include at least one of the following measures (i-iii) to break up the massing of the building and add visual interest.

Note: the City may exempt buildings in the Art/Tech District from Guideline “c” where the applicant can demonstrate that they require large spaces for fabrication, storage, industrial or other special activities. Office and retail uses do not qualify for this exemption.

- i. Provide vertical building indentation at least 15 feet deep and 30 feet wide. For multi-story buildings, the modulation shall extend through more than one-half of the building floors.



Figure 18.120.040.D.2.c Example of vertical indentation to visually break up the scale of a large building.

- ii. Use a contrasting vertical modulated design element such as a turret, glazed area, or set of columns or pilasters featuring all of the following:
 - Element extends through all floors above the first floor fronting on the street.
Exception: upper floors that are set back more than ten feet horizontally from the façade are exempt.
 - Utilizes a change in building materials that contrast from the rest of the façade.
 - Element is designed to include roofline modulation per subsection “d” below.
- iii. A building façade that includes different sections with contrasting articulation that make it appear like two or more distinct buildings. To qualify for this option, these contrasting façades shall employ at least two of the following:
 - Different building materials and/or configuration of building materials.
 - Contrasting window design (sizes or configurations).
 - Different ground floor articulation, entry features and store front design.
 - Other measure approved by the City



Figure 18.120.040.D.2.d. Example of a architecturally dividing a façade into sections that give the impression of a composition of smaller buildings.

d. Roofline modulation.

In order to qualify as a facade articulation feature in Section 040.D.2.b. and c above, rooflines shall employ one or more of the following:

- i. Different roof heights. For flat roofs or façades with horizontal eave, fascia, or parapet, the minimum difference between the two sections of roofline or cornice is 3 feet for one and two story buildings and 5 feet for buildings with more than two stories.
- ii. A pitched roofline or gabled roofline segment. The pitched or gable roof must be at least 20 feet in width with a minimum slope of 4:12)
- iii. An arched roof line or a roof with a special geometry. The City may count an unusual or distinctive roof element such as an arch, tower, clock, or similar feature as an articulating feature.



Figure 18.120.040.D.2.e Three local examples meeting the criteria for roof modulation.

e. New buildings in the South Residential Neighborhood. The South Residential Neighborhood is envisioned to feature a variety of different residential types and building sizes. The challenge will be to minimize shading and impacts to privacy to neighboring residences and to create a harmonious relationship between adjacent buildings and an attractive street front. The City may require a proposed building design to:

- i. Step back stories 4 and above up to 15' from the allowed side and rear yard building wall in order to minimize shading to adjacent properties. This requirement will be most

important if the new building's shadow extends 10 feet onto an adjacent property to the north or east when the sun's angle is at 45 degrees from the horizontal.

- ii. Replicate or approximate roof forms and pitch found on existing residential structures in the neighborhood.
- iii. Employ window patterns and proportions similar to those on existing residential structures in the neighborhood – provided they also meet guideline 060.B.2.
- iv. Employ building façade materials similar to those featured on existing buildings in the neighborhood. Such materials may be secondary or accent materials and need not cover the whole façade.
- v. Maintain a relationship to the street that reflects other buildings on the block. This may be done by:
 - Setting the building back a distance between the setbacks of buildings on either side of the project site.
 - Employing similar landscaping.
 - Featuring similar entry configurations. (E.g.: walks, porches, etc.)
 - Other method approved by the City.

f. Modulation required to enhance views. In order to enhance the quality of views from designated view points, vertical modulation is required on the west face of new buildings over 4 stories on the properties in the following blocks:

- The block bounded by Capitol Way, Washington Street, 8th Avenue and 9th Avenue
- The block bounded by Franklin Street, Washington Street, 8th Avenue and 9th Avenue
- The block bounded by Franklin Street, Washington Street, 9th Avenue and 10th Avenue

Modulation is not required on the bottom 4 stories of buildings on blocks noted above.

The vertical modulation must conform to the following:

- i. Be articulated at intervals not greater than 60 feet measured horizontally
- ii. Project out or in from the building face at least 5 feet in depth if the interval is 35 feet or less and 10 feet if the interval is greater than 35 feet.
- iii. The minimum width of the projection parallel to the building face is 5 feet if the interval is less than 35' and 10 feet if the interval is greater than 35 feet.

- iv. The modulation must be accompanied by roof modulation or the projection must feature a change in materials or colors.

Buildings on sites noted above with west facing facades over 120 feet in length measured horizontally must feature modulation projection of indentation at least 15 feet deep and 30 feet wide to break the building face down into intervals no more than 60 feet wide.

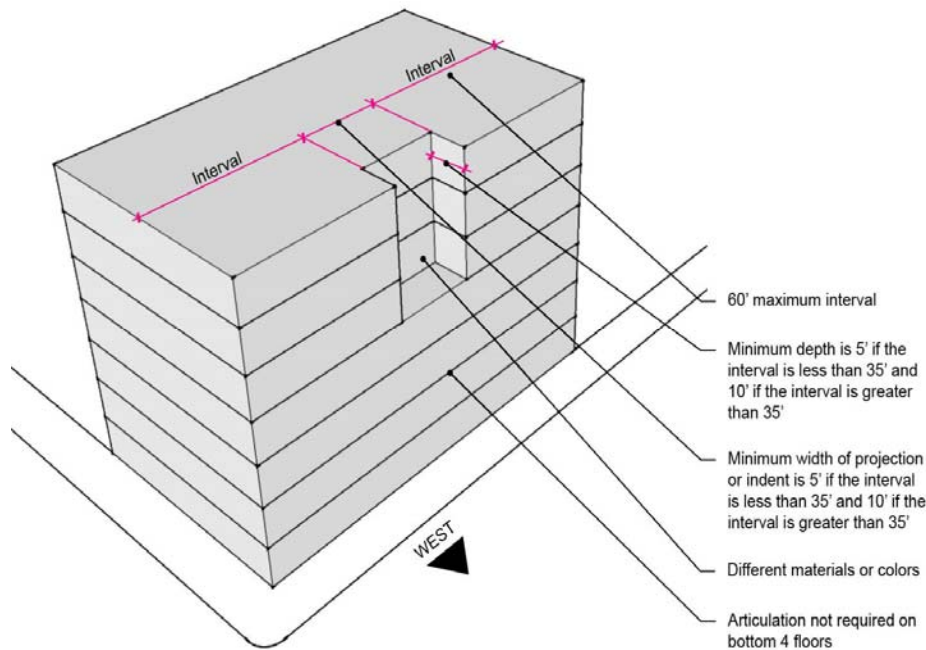


Figure 18.120.040.D.2.f Modulation dimensional relationships for specific properties to enhance views. This diagram is for illustrative purposes. Refer to guideline 040.D.2.f for stated requirements.

040.E. HUMAN SCALE BUILDING ELEMENTS AND DETAILS

I. BACKGROUND AND INTENT

“Human scale” addresses the relationship between a building and the human body. Generally, buildings attain a good human scale when they feature elements or characteristics that are sized to fit human activities, such as doors, porches, and balconies.

Building elements refer to larger building features such as balconies, cornices, bay windows, turrets, etc. Details refer to smaller items such as lights, furniture, artwork, signs, door and window moldings, etc. Generally, building elements can be seen from a distance (80’ or more) while details are perceived from a shorter distance.

Intent:

- Provide an attractive, interesting and welcoming pedestrian environment.
- Make new buildings more inviting and interesting.

- Increase the design quality of buildings when viewed close up, (roughly within 80 feet of the building).
- Enhance the quality of building facades.
- Provide the building with an appropriate human scale that adds interest and a sense of well-crafted quality.
- Add interest to the streetscape, both for those travelling in vehicles and those on foot.

Note: Building elements or features used to address guidelines in Section 040.E may also be used to concurrently satisfy requirements in Sections 040.D and 040.F and vice versa, provided the elements or features are named as options in the applicable sections. Similarly, an element or detail may be used to concurrently satisfy 040.E.2.a, b, and c.

2. REQUIREMENTS

- a. Human scaled elements. Buildings on A and B Pedestrian Oriented Streets, buildings facing Capitol Way, a water body or public park, and buildings in the Core, Waterfront, and South Residential Neighborhood Districts must incorporate a minimum of four human scale building elements into new buildings and structures.

Other buildings for which the above paragraph does not apply must use at least 2 of the human scale measures below.

Human scale measures include:

- i. Balconies or decks in upper stories, provided there is at least one balcony or deck per upper floor on the façades facing streets (Alleys are not counted as streets for this particular guideline.).
- ii. Bay windows or other window treatments that extend out from the building face;
- iii. At least 100 square feet of pedestrian-oriented space, as described in 18.120.030.C, for each 100 lineal feet of building façade;
- iv. First floor individual windows, generally less than 32 square feet per pane with molding or trim around each window;
- v. A porch or covered entry;
- vi. Spatially defining building elements, such as a trellis, overhang, canopy, or other element, that defines space that can be occupied by people;
- vii. Upper story setbacks, provided one or more of the upper stories are set back from the face of the building at least 6 feet;

- viii. Placement of smaller building elements near the entry of pedestrian-oriented street fronts of large buildings. Figure 18.120.040.E.2.a illustrates how human scale can be achieved using elements such as multiple canopies, an extended café area, and upper deck);
- ix. Landscaping components that meet these guidelines;
- x. Public art or hand crafted or well detailed ornamental sign that incorporates elements of a normal human scale (e.g.: life size sculpture.);
- xi. Upper story windows with molding at least 4” wide or clusters of adjacent windows separated by molding (ribbon windows and curtain walls do not apply); and
- xii. Other elements that the City determines meets the intent of these guidelines.



Figure 18.120.040.E.2.a Examples of elements that provide a human scale. Human scale elements include outdoor seating and furniture, landscaping, multi-paned windows canopies, trellises, a porch, lighting fixtures and traditional materials.

- b. Building entries. Building entries must feature all of the following.
- i. All buildings must have a primary building entrance facing the street or open on to a courtyard directly facing the street (Exception: buildings that are situated immediately on a shoreline esplanade or public walkway must include a primary building entrance facing the shoreline.)
 - ii. All primary building entries must be covered with weather protection at least 6' wide and 5' in depth.
 - iii. All Primary building entries must feature a light for visibility and security, and a building number.
 - iv. Primary building entries within the Core, North Capitol, South Capitol, Waterfront and Entertainment Districts must be enhanced with at least 2 of the facade details noted in "a" or have a recessed entry (alcove).
- c. Facade details – non-residential and mixed-use buildings. All new buildings and additions shall employ at least one detail element from each of the three categories below on all façades facing a street or public space.
- i. Window and/or entry treatment, such as:
 - Display windows divided into a grid of multiple panes.
 - Transom windows. (A strip of smaller windows located above display windows)
 - Roll-up windows/doors that promote outdoor activities.
 - Decorative door.
 - Etched or stained decorative glass
 - Multi-paned windows on upper stories (At least 4 panes/window)
 - Other decorative or specially designed window or entry treatment that meets the intent of the standards.

Exception: Ground floor residences are exempt from the requirements of a above.



Figure 18.120.040.E.2.b Examples of special window treatments. Note transom windows in the Melrose Market façade.

ii. Building façade details, such as:

- Custom-designed weather protection element such as a steel canopy, cloth awning, or retractable awning.
- Decorative, custom sign(s) with bottom edge no more than 12 feet above grade.
- Unique or hand crafted artwork
- Decorative building-mounted light fixtures.
- Bay windows, trellises, towers, and similar elements.
- Other details or elements that meet the intent of these standards.



Figure 18.120.040.E.2.c Examples of attractive building details including artwork, special lighting, interesting window patterns, special features, and pedestrian oriented signs.

iii. Materials that provide a decorative or textural enhancement, such as:

- Use of decorative or textural building materials. Examples include decorative use of brick, tile, or stonework.
- Artwork on building (such as a mural) or bas-relief sculpture.
- Decorative kick-plate, pier, beltcourse, or other similar feature.
- Hand-crafted material, such as special wrought iron or carved wood.
- Etched or stained decorative glass.
- Other details that meet the intent of the guidelines.

“Custom,” “decorative,” or “hand-crafted” elements referenced above shall be distinctive or “one-of-a-kind” elements or unusual designs that require a high level of craftsmanship. However, these terms do not necessarily mean that the elements be ornate. They may be simple if finely detailed or manufactured. It is more important that the façade details fit the style and character of the building.



Figure 18.120.040.E.2.d Examples of unique use of materials that add to façade's interest.

- d. Window design. Buildings in all Districts except the Art/Tech District shall employ techniques to add quality and interest to fenestration. Whereas B.I.a is focused on storefronts, this guideline also applies to upper story windows. Options to accomplish this are to:
- i. Recess or project individual windows above the ground floor at least two inches from the façade or incorporate other design treatments that add depth, richness, and visual interest to the façade.
 - ii. Add trim or shading elements to the window
 - iii. Arrange the grouping of windows in a distinctive pattern
 - iv. Multi-paned windows (more than four panes per window)
 - v. Visually significant window elements such as wider frames, lintels, casings, etc.
 - vi. Other method approved by the City that meets the intent of these guidelines.

e. High Visibility Street Corners. All development proposals located at designated High Visibility Street Corners and gateway sites per the map at Figure 18.120.020.B.2.a or identified below shall locate a building or structure within 15 feet of the street corner and include special design features approved by the City that accentuate the street corner. Alternatively, the building could be configured with a corner plaza. Corner design features could include a cropped building corner with a special entry feature, decorative use of building materials at the corner, distinctive façade articulation, sculptural architectural element, or other decorative elements that meet the intent of the standards. The following are Designated High Visibility Street Corners:

- Intersection of Plum Street and Union Avenue – NE, NW, & SW corners
- Intersection of Plum Street and 4th Avenue - NE, NW, & SW corners
- Intersection of Plum Street and State Avenue - NE, NW, & SW corners
- Intersection of Plum Street and Olympia Avenue - NW, & SW corners
- Intersection of Union Avenue and Capitol Way – NE & NW corners
- Intersection of 5th Avenue and Water Street – NE & SE corners
- Intersection of 4th Avenue and Water Street – NE & SE corners.
- Intersection of 8th Avenue and Plum Street –SE corner.

Figure 18.120.040.E.2.e below illustrates acceptable examples.

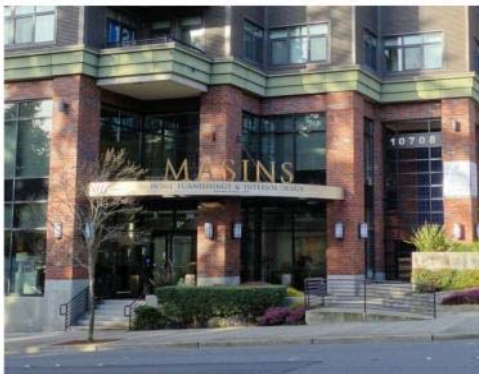


Figure 18.120.040.E.2.e. Examples of positive corner treatments. The buildings in the upper right and lower left feature large scale building elements while the Olympia City Hall features a curved glass curtain wall and landscaping. The lower right example breaks down the building massing to a human scale and adds a corner entrance and a graceful canopy.

- f. Design Character of Building Elements and Details. The building elements and details must be consistent with the building’s architectural style and character. For example, generally do not use ornate historic elements on a contemporary styled building. And, do not use single family residential details in large buildings.



Figure 18.120.040.E.2.f While emphasizing the entry is laudable, the classical arch and columns clash with the other more contemporary façade elements.

The applicant must be prepared to demonstrate to the City's satisfaction that the building elements are consistent with the building's architecture and that they respond to the desired character of the applicable District as noted in 040.B.2.

040.F. PEDESTRIAN ORIENTED FACADES AND WEATHER PROTECTION

I. BACKGROUND AND INTENT

Guidelines 18.120.020.B.3 and 4 include provisions for "Pedestrian Oriented Facades" This section establishes the requirements for such Pedestrian Oriented Facades.

Intent:

- Provide a consistently interesting pedestrian environment.
- Encourage walking.
- Strengthen retail activities in the Downtown.
- Allow pedestrians to view the inside activities of businesses and associated non-residential uses.
- Increase safety by adding "eyes on the street".

2. REQUIREMENTS

- a. Where pedestrian-oriented facades are required, the building shall meet all of the following:

- i. Transparent window areas;; or window displays over at least 75 percent of the ground floor façade between 2 feet and 8 feet above sidewalk. The windows shall look into the building’s interior or be configured as merchandise display windows. The building must be designed so that the windows satisfying the requirement for “pedestrian oriented facades” do not look into service or storage areas or other unsightly rooms.

Special purpose buildings such as theaters that do not sell merchandise may propose an alternate solution to providing an interesting pedestrian experience. (Offices, banks, workshops, and restaurants do not qualify for this exception.) If the applicant’s proposal includes other than fully transparent windows to satisfy this requirement, the non-transparent materials must be approved by the City. Windows with neon signs may be considered transparent.

- ii. A primary building entry facing the street front. (See Section 18.120.040.E.2.b for entry enhancement requirements.)
- iii. Weather protection (e.g.: canopy, awning or other cover from the rain) at least 5 feet wide over at least 65 percent of the front facade. The weather protection must be located between 8 feet and 15 feet above grade unless there is a compelling reason to the contrary.
- iv. A floor to floor ground floor height of at least 15 feet. The intent of this guideline is to provide space that can be adaptable for a variety of uses. For example, galleries typically require higher ceilings and restaurants require space for mechanical systems. The City may waive this requirement if there are special conditions such as the provision of a mezzanine or special use.

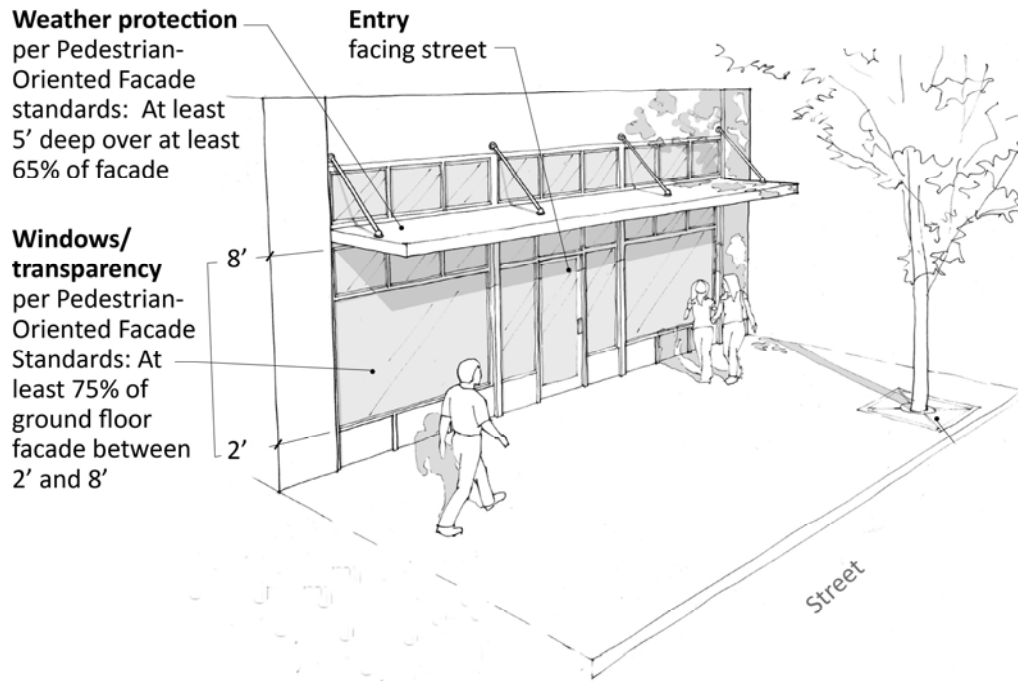


Figure 18.120.040.F.2.a An example of a pedestrian-oriented façade.

Exceptions: The City may approve other methods for providing an attractive ground floor building front that meet the intent of this guideline.

040.G. MATERIALS AND COLORS

I. INTENT:

- Encourage the use of durable, high quality, and urban building materials that minimize maintenance cost and provide visual interest from all observable vantage points.
- Promote the use of a distinctive mix of materials that helps to articulate facades and lends a sense of depth and richness to the buildings.
- Place the highest priority for the quality and detailing of materials on the first two to three building floors.

2. REQUIREMENTS

- a. Quality building materials. Building facades must be constructed of high quality durable materials. This is most important at a building's ground floor, particularly for commercial and mixed-use buildings in the Core, Waterfront, North and South Capitol Way and Entertainment Districts where building façades are near sidewalks and more exposed to damage and vandalism. At a minimum, stone, masonry, or architectural concrete shall be used (excluding window and door areas) for first floor building exterior cladding on all non-residential or mixed-use buildings.
- b. Flood proofing. If there is an unavoidable conflict between the guidelines in Chapter 18.120 and the City's requirements for flood proofing to address sea level rise, those flood proofing requirements shall apply.
- c. Specific material limitations. The following special conditions and limitations for the use of concrete block, metal siding, Panel systems (e.g.: Hardiplank or Hardie Board) and exterior insulation and finish system (EIFS) cladding shall apply.
 - i. Concrete block (a.k.a. CMU) may be used as indicated in the Permitted Materials Chart in Section 18.120.040.G.2.d provided special enhancements are included. Special enhancements might include a combination of textures and/or colors to add visual interest. For example, combining split or rock-façade, other masonry or colored tile units with smooth blocks can create distinctive patterns. The figures below illustrate acceptable concrete block use/designs.



Figure 18.120.040.G.2.a. Tile used as an appropriate CMU enhancement.



Figure 18.120.040.G.2.b. The above façade illustrates an acceptable alternative example, as CMU is used as the primary cladding material. Note the use of split-façade CMU's above each of the awnings and coupled with the use of smooth-façade CMU's on the vertical columns (which employ black accent tiles for added interest).

- ii. Metal siding may be used as noted in Permitted Materials Chart in Section 18.120.040.G.2.d, (in most cases if it is incorporated with other permitted materials) and it complies with the following:
 - It features visible corner molding and trim and does not extend to the ground level of non-residential and mixed-use buildings and no lower than two feet above grade for residential buildings. Masonry, concrete, or other durable material shall be incorporated between the metal siding and the ground plane.
 - Metal siding shall be factory finished, with a matte, non-reflective surface.
 - Where metal siding is the primary or secondary siding material on upper floors, the layout of the panels must mesh with the location and patterns of windows, balconies, and modulated building surfaces.



Figure 18.120.040.G.2.c. Metal siding used appropriately in combination with other materials.

- iii. The use of Exterior Insulation and Finish System (EIFS) is prohibited in the Downtown.
- iv. Cement board paneling/siding (E.g. Hardie Plank) may be used where indicated in the Permitted Materials Chart in Section 18.120.040.G.2.d., provided:
 - Except in the Art/Tech District, where cement board paneling/siding is the primary or secondary siding material, the paneling joints must be arranged in a pattern that is consistent with windows, balconies, and modulated building surfaces and must be enhanced with façade details that add visual interest from the ground level and adjacent buildings.
- d. District specific materials requirements. The chart below identifies where the following materials are permitted in each of the Downtown Design Districts according to the following markings.
 - P = Permitted as a primary, secondary or accent material.
 - S = Permitted as a secondary or accent material.
 - A = Permitted as an accent material.
 - N = Not permitted
 - “C” added to the marking means that the material may be permitted by the City but that the City may require added conditions in order to ensure the durability and quality of materials, provide a high level of design, construction and finish, and increase consistency with the desired design character for each District as described in 18.120.040.B.

The color green indicates the material is generally acceptable in the District

Yellow indicates there are some conditions or limitations for using the material

Red indicates the material is not acceptable for that District.

Primary material means any single material that covers more than 35% of the façade in question.

Secondary material means any single material that covers 35% or less of a building façade.

Accent material means any single material that is used for a small building element such as a door, canopy, weather protection, light fixtures cornice, etc; trim (E.g.: window trim), signs or artwork.

Other materials not covered in this section will be evaluated by the City based on the intent statement of this section.

Exceptions: The City may allow materials otherwise prohibited if the applicant can demonstrate to the City's satisfaction that the material is durable, appropriate for the proposed purpose and application, and meets the objectives of this section.

PERMITTED MATERIALS CHART

Materials	Core - bottom 2 floors	Core - above 2 floors	South Capitol - bottom 2 floors	South Capitol - above 2 floors	North Capitol - bottom 2 floors	North Capitol - above 2 floors	Waterfront	Art/Tech	S Neighborhood - ground floor	S Neighborhood - upper floors	Entertainment - ground floor	Entertainment upper floors
Brick, stone, masonry except for CMU	P	P	P	P	P	P	PC	P	P	P	P	P
CMU, Plain	N	N	SC	SC	SC	SC	SC	P	AC	AC	AC	PC
CMU with enhancements	SC	SC	SC	SC	P	P	S	P	P	P	P	P
EIFS	N	N	N	N	N	N	N	N	N	N	N	N
Metal siding	N	N	AC	N	PC	P	P	P	AC	SC	AC	P
Cement panels	N	PC	N	PC	SC	P	PC	P	AC	P	AC	P
Lap siding or similar	AC	N	AC	N	P	P	P	P	P	P	PC	PC
Wood panels with special finish and texture.	AC	N	AC	N	P	P	P	P	P	P	PC	PC
Mirrored or highly reflective surfaces	N	N	N	PC	N	PC	N	PC	N	N	A	N
Plastic or sheet fiberglass	N	N	N	N	N	N	N	N	N	N	N	N
Chain link fence (site)	N	N	N	N	N	N	N	PC	N	N	N	N
Ceramic tile and similar	P	P	P	P	P	P	P	P	P	P	P	P
Concrete	C	C	C	C	C	C	P	P	C	C	C	C
Stucco	C	C	C	C	C	C	C	C	C	C	C	C

- e. Building Color. Bright, highly saturated colors such as red, yellow, orange, bright green, bright blue, purple, and black must be limited to their use as accent colors on small architectural features such as a door, canopy, weather protection, light fixtures, cornice, trim (E.g.: window trim), signs or artwork. If the applicant chooses to use one of these colors for other than accent purposes, color samples and an exterior color scheme must be presented to the City for approval. The City will determine whether or not the color is permitted.

040.H. BLANK WALL TREATMENTS

I. INTENT:

- Ensure that large expanses of uniform walls visible from a public roadway or park do not detract from the Downtown's appearance or the pedestrian environment.
- Add interest to local streetscapes.

2. REQUIREMENTS

- a. New blank walls. Untreated blank walls facing a public street, pedestrian-oriented space, common usable open space, or pedestrian pathway are prohibited unless treated as described below. The City may waive or relax this provision in the Art/Tech District if the wall is not on a designated Pedestrian Oriented Façade and if it is not visible and not within 300' from Plum Street NE, Union Ave SE, Capitol Way, or 4th Ave SE. This requirement does not apply to existing blank walls where improvements are not proposed.

Acceptable methods to treat blank walls can include:

- a. Display windows at least 16 inches of depth to allow for changeable displays. "Tack-on" display cases shall not qualify as a blank wall treatment.
- b. Landscape planting bed at least 5' wide or a raised planter bed at least 2' high and 3' wide in front of the wall with planting materials that are sufficient to obscure or screen at least 60% of the wall's surface within three years.
- c. A vertical trellis in front of the wall with climbing vines or plant materials.
- d. A mural that is not a sign, This only applies to proposed murals on an existing building..
- e. Special building detailing that adds visual interest at a pedestrian scale. Such detailing shall use a variety of surfaces; monotonous designs will not meet the purpose of the standards.
- f. Other method approved by the City.

For large visible blank walls, a variety of treatments may be required to meet the purpose of the standards.



Figure 18.120.040.H.2.a. Acceptable blank wall treatments.

Exception: Firewalls along property lines that are not visible to the general public or are not within 300' from a public right of way or public park are exempt from the above standards.

040.I. PARKING GARAGE DESIGN

I. INTENT

To minimize negative visual impacts of parking garages

2. REQUIREMENTS

- a. Parking garages must be designed to obscure the view of parked cars at the ground level. This does not require opaque screening that would create security issues in conflict with Section 18.120.020.H. Site Planning for Security.
- b. Where the garage wall is built within 10' of the sidewalk edge, the grade level façade shall incorporate a combination of artwork, grillwork, special building material or treatment/design, and/or other treatments as approved by the City that enhance the pedestrian environment. Small setbacks with terraced landscaping elements can be particularly effective in softening the appearance of a parking garage.
- c. Upper-level parking garages must use articulation treatments that break up the massing of the garage and add visual interest as described in 040.D.2.a.
- d. Alternatively, for parking garages screened from public roadways by a building (i.e.: a

building located between the garage and the public street). In this case, (a) and (b) above do not apply.

- e. Parking garages must adhere to the requirements of 18.120.020.H. Site Planning for Security, including those for visibility, lighting and avoidance of entrapment areas.



Figure 18.120.040.I.2.a The side of this parking garage includes decorative grillwork, and a raised brick planter to enhance the pedestrian environment.



Figure 18.120.040.I.2.b. This building uses openings on its second level parking area to resemble windows.



Figure 18.120.040.1.2.c. Design parking garages to obscure the view of parked cars. Note the landscaping that separates the garage from pedestrians.