

Ordinance No. _____

AN ORDINANCE OF THE CITY OF OLYMPIA, WASHINGTON, RELATING TO THE AREA KNOW AS THE KAISER-HARRISON OPPORTUNITY AREA; SPECIFICALLY AMENDING THE LAND USE AND URBAN DESIGN CHAPTER AND THE FUTURE LAND USE MAP OF THE OLYMPIA COMPREHENSIVE PLAN; AMENDING CHAPTERS 18.06, 18.16, 18.130, AND 18.100 OF THE OLYMPIA MUNICIPAL CODE; AND AMENDING THE OFFICIAL ZONING MAP AND DESIGN REVIEW MAP OF THE CITY OF OLYMPIA.

WHEREAS, on May 16, 2016, the Olympia Planning Commission received a briefing on the proposal to amend the Olympia Comprehensive Plan Future Land Use Map, Comprehensive Plan text in the Land Use and Urban Design Chapter, the Official Zoning Map, the Official Design Review Map, and text amendments to Unified Development Code (OMC Title 18) (herein the Amendments), in order to implement the Kaiser Harrison Opportunity Area Plan; and

WHEREAS, on June 6, 2016, the Olympia Planning Commission held a public hearing on the proposal and deliberated the Amendments on June 20, 2016; and

WHEREAS, following the public hearing and deliberations, the Planning Commission recommended amendments to the Olympia Comprehensive Plan Future Land Use Map, Comprehensive Plan text in the Land Use and Urban Design Chapter, the Official Zoning Map, the Official Design Review Map, and text amendments to the Unified Development Code; and

WHEREAS, on June 3, 2016, the City of Olympia issued a Determination of Non-significance pursuant to the State Environmental Policy Act, which was not appealed; and

WHEREAS, on February 17, 2016, a Public Participation Plan was issued for the Kaiser-Harrison Opportunity Area Planning Process and posted to the City's project webpage; and

WHEREAS, City staff met with the property owners affected by the Amendments and interested parties on February 18, 2016; and

WHEREAS, public workshops and meetings were held on the Amendments March 9, March 17, March 18, March 24, and March 31, 2016; and

WHEREAS, this comprehensive plan and development regulation amendment is consistent with the City of Olympia Comprehensive Plan; and

WHEREAS, the Amendments meet the goals and requirements of the Washington State Growth Management Act; and

WHEREAS, the Amendments have been reviewed pursuant to the Olympia Comprehensive Plan Amendment Process outlined in Chapter 18.59 of the Olympia Municipal Code; and

WHEREAS, Chapters 35A.63 and 36.70A RCW and Article 11, Section 11 of the Washington State Constitution authorize and permit the City to adopt this Ordinance; and

WHEREAS, this Ordinance is supported by the staff report and materials associated with this Ordinance, along with other documents on file with the City of Olympia; and

WHEREAS, this Ordinance is also supported by the professional judgment and experience of the City staff who have worked on this proposal; and

WHEREAS, City Staff are known to the City Council, and staff's curriculum vitae shall be part of the record in support of this Ordinance;

NOW, THEREFORE, THE OLYMPIA CITY COUNCIL ORDAINS AS FOLLOWS:

Section 1. Amendment of Olympia Comprehensive Plan. The Olympia Comprehensive Plan is hereby amended as shown on the attached Exhibit A, which is hereby incorporated as though fully set forth herein.

Section 2. Amendment of Olympia Comprehensive Plan Future Land Use Map. The Olympia Comprehensive Plan Future Land Use Map is hereby amended as shown on the attached Exhibit B, which is hereby incorporated as though fully set forth herein.

Section 3. Amendment of City of Olympia Zoning Map. The Official City of Olympia Zoning Map is hereby amended as shown on the attached Exhibit C, which is hereby incorporated as though fully set forth herein.

Section 4. Amendment of Design Review Map. The Official Design Review Map is hereby amended as shown on the attached Exhibit D, which is hereby incorporated as though fully set forth herein.

Section 5. Official Maps.

A. The City Manager or his designee is authorized to prepare such maps reflecting this Ordinance. The Mayor is authorized but not required to sign an Official Zoning Map and Official Design Review Map reflecting this Ordinance.

B. Copies of the Official Zoning Map and the Official Design Review Map are and shall be retained on file with the City Clerk.

Section 6. Amendment of OMC Chapter 18.06. Olympia Municipal Code Chapter 18.06 is hereby amended as shown on the attached Exhibit E, which is hereby incorporated as though fully set forth herein.

Section 7. Amendment of OMC Chapter 18.16. Olympia Municipal Code Chapter 18.16 is hereby amended as shown on the attached Exhibit F, which is hereby incorporated as though fully set forth herein.

Section 8. Amendment of OMC Chapter 18.130. Olympia Municipal Code Chapter 18.130 is hereby amended as shown on the attached Exhibit G, which is hereby incorporated as though fully set forth herein.

Section 9. Amendment of OMC Chapter 18.100. Olympia Municipal Code Chapter 18.100 of the Olympia Municipal Code, is hereby amended as shown on the attached Exhibit H, which is hereby incorporated as though fully set forth herein.

Section 10. Official Comprehensive Plan. Copies of the Olympia Comprehensive Plan are and shall be retained on file with the City Clerk.

Section 11. Severability. If any provision of this Ordinance or its application to any person or circumstance is held invalid, the remainder of the ordinance or application of the provisions to other persons or circumstances shall remain unaffected.

Section 12. Ratification. Any act consistent with the authority and prior to the effective date of this Ordinance is hereby ratified and affirmed.

Section 13. Effective Date. This Ordinance shall take effect five (5) days after publication, as provided by law.

MAYOR

ATTEST:

CITY CLERK

APPROVED AS TO FORM:

Darra Nieneke DCA

CITY ATTORNEY

PASSED:

APPROVED:

PUBLISHED:

Land Use and Urban Design



A blending of old and new land uses.

What Olympia Values:

Olympians value neighborhoods with distinct identities; historic buildings and places; a walkable and comfortable downtown; increased urban green space; locally produced food; and public spaces for citizens in neighborhoods, downtown, and along our shorelines.

Our Vision for the Future:

A walkable, vibrant city.

Read more in the Community Values and Vision chapter

Introduction

How we choose to live within, and how we alter, our landscape is critical to our quality of life, and to whether that quality of life can be sustained and improved.

The State's 1990 Growth Management Act called for Olympia to establish land use designations and densities sufficient for at least 20 years. The County-Wide Planning Policies adopted by Thurston County and its seven cities in 1993 describe a common goal of concentrating growth in the urban areas "in ways that ensure livability, preservation of environmental quality and open space, varied and affordable housing, high quality urban services at least cost, and orderly transition of land from County to City."

We can choose to isolate land uses and neighborhoods, or blend them into a single vital community. We can create spaces separated by long travel distances, or provide for a variety of experiences in each part of the city. We can choose to use land efficiently for recreation, housing, and business while setting aside selected areas for open space and communing with nature, or we can create homogenous subdivisions and isolated commercial areas. We can employ architecture and landscaping reflecting Olympia's unique and historic character, or we can build places with little regard to the local landscape and climate. These choices will determine Olympia's form for many generations.

Our community seeks to:

- Encourage development in urban areas where public services and facilities are already present.
- Phase urban development and facility extension outward from the downtown area.
- Establish land use patterns that ensure residential densities sufficient to accommodate 20-years of population growth.
- Focus higher residential densities downtown, along urban corridors, and near neighborhood centers.
- Employ innovative development techniques that create a better community.



A new pair of townhomes reflects Olympia's historic character.

Olympia's "Urban Design Vision and Strategy," appreciation of the area's history and sustainable community philosophy all provide additional direction for this chapter. In particular, the sustainability policies call for us to consider the long-range implications of our land use decisions and to provide for a pattern of development that can be sustained and enjoyed by future generations.

For example, mixed-use 'villages' and opportunities for residential development in commercial areas provide for increasing residential densities by blending land uses. By enabling less reliance on automobiles, by providing for compact development that requires less land, by efficiently providing streets, utilities, and services, and by establishing development densities and site designs that protect environmentally sensitive areas and reflect the capacity of natural systems, we can provide a quality community for coming generations.

We envision:

- Spaces that are safe and pedestrian-friendly
- Development that minimizes harm to the environment
- Densities and land use types consistent with many types of transportation

- Places for quiet residential uses, and places where economic activity is emphasized
- Walkable neighborhoods with unique centers and identities
- Development that complements the historic character of the community
- Recognition of the importance of lands near water
- A process for exploring the unique possibilities of each area with special attention given to Downtown, the Westside core area, the eleven planning 'subareas,' and other special geographic areas within the community

The focus here is on 'built' land uses such as housing and commercial structures and development patterns. Complementary parks, open spaces and natural areas are addressed in the Public Health, Parks, Arts and Recreation and Natural Environment chapters. These land uses cannot be isolated from economic topics, and employment in particular, addressed by the Economy chapter. Facilities and services to support this urban development pattern, including the critical transportation system, are described in the Transportation, Utilities, and Public Services chapters. In many cases the special area plans described in this chapter will touch on all of those topics and more.

The City of Olympia, in cooperation with Thurston County, plays a major role in determining the location, intensity, and form of land uses in the community. This chapter addresses the proposed uses of land in Olympia's Urban Growth Area and the design and locations of buildings and other structures within that landscape. It includes:

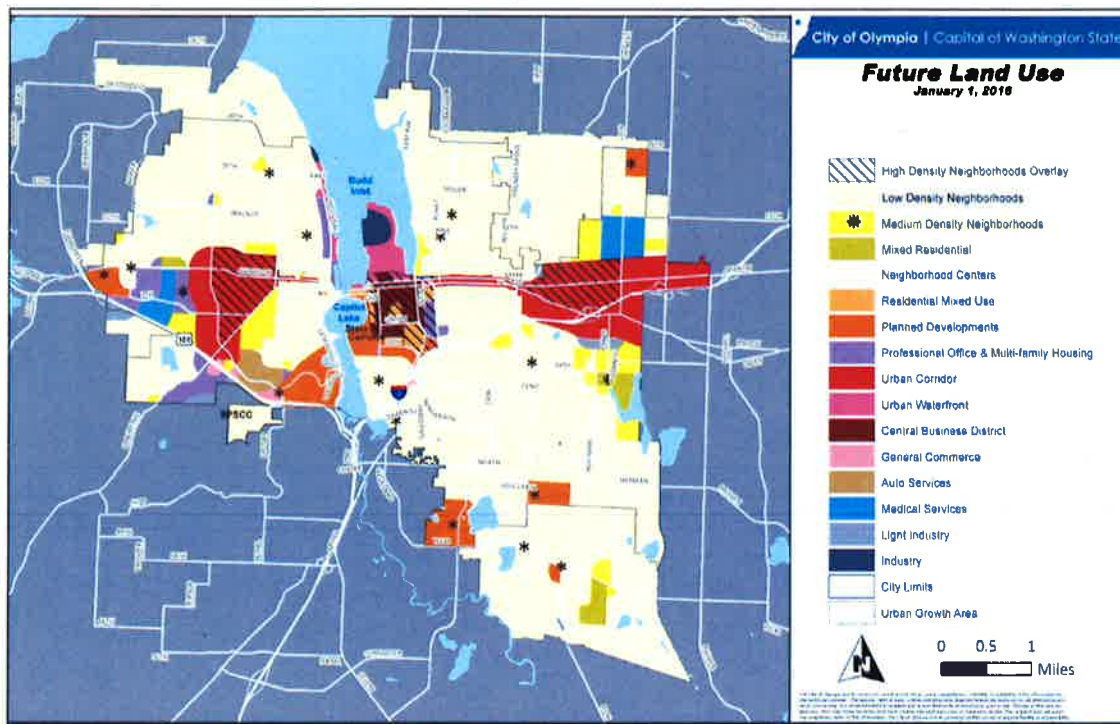
- The location and quantity of those land uses and their relation to each other
- The functional design of those land uses including buildings and surrounding spaces
- Opportunities for historic preservation
- The aesthetic form of the built environment

The Future Land Use Map shows the approximate locations for a variety of land uses in Olympia's Urban Growth Area. This map is not a zoning map. Rather it provides guidance for zoning and other regulations to ensure uses of land and development consistent with this Plan. Although these map lines are approximate, all future land uses should be consistent with the intent of this map and the land use category descriptions in Appendix A as well as the goals and policies of this Plan. In general, zoning and land

uses should not deviate from the Future Land Use Map boundaries by more than about 200 feet. Compatible and supporting land uses, such as parks, schools, churches, public facilities and utilities, streets and similar features, are expected within these areas. See Appendix A regarding acreages, densities, and building heights of each use category.

Proposed rezones shall meet criteria to be adopted into the Olympia Municipal Code that address:

1. Consistency with the Comprehensive Plan.
2. Consistency with the City's development regulations that implement the Comprehensive Plan.
3. Compatibility with adjoining zoning districts and transitioning where appropriate to ensure compatibility.
4. Adequacy of infrastructure in light of development potential of the proposed zoning.



View Future Land Use Map of Olympia and its Urban Growth Area

The community employs regulations, such as zoning, design review, stormwater, engineering, building, and subdivision standards, to ensure that new development conforms to the goals and policies described in this chapter. The regulations are administered by City staff and a Hearing

Examiner selected by the City Council. Equally important to this land use and design vision is capital facility planning and construction by the City of Olympia and other public agencies of the area. Continuing cooperation between the State and the City, among the local governments, and with special purpose governments such as the Port of Olympia and the school districts is critical. And, as envisioned, substantial resources and the support of everyone in the community will be needed to focus more detailed efforts in neighborhoods and other special places.

General Land Use and Design

To achieve our vision of Olympia while accommodating our share of the region's population, we need to plan for quantity at the same time as we pursue quality. Such a community is one in which pattern and mix of land uses supports healthy lifestyles, such as walking to nearby services instead of driving. We need to consider the implications of climate change, and how we can minimize our community's contribution. We must be prepared to adapt our built environment as resources change, while preserving key elements of Olympia's architectural and cultural heritage. At the same time, we need to consider the character of Olympians today, and those of the future. The needs and interests of a more diverse, more urban, and generally older population will differ from those of today.

Olympia was once a port-oriented community with a central business district and compact single-family neighborhoods. Now, its land-use pattern is more suburban, with commercial development taking place outside of downtown, and lower-density neighborhoods with fewer street connections. Over the next 20 years, as Olympia becomes a more urban place, the pattern of land use and design of urban areas will change as we accommodate an expanding population while retaining our community's character and heritage.

This Plan envisions gradually increasing densities in Olympia accompanied by attractive streets and buildings arranged for the convenience of pedestrians. The location, mix and relationship of land uses to each other and to our streets will be crucial as will be the character of commercial and residential areas, parks, and open spaces. The Plan envisions new development that will reinforce the community's identity, urban design preferences, and historic form. Selected major streets will gradually transform into attractive, higher density, mixed residential and commercial "urban corridors" with frequent transit service.

Housing will be available within and near shopping and employment areas. Development will be carefully designed to integrate with the adjacent transportation system, and with key features such as downtown and the hospitals. Neighborhoods and commercial areas will gradually be woven together into a cohesive urban fabric. These "ten-minute" neighborhoods will provide ready-access from homes to supporting businesses, and to parks, schools and other gathering places.

The relationship between the transportation system and other land uses plays a key role in urban life. The Transportation chapter addresses the specific design of streets, such as the number of travel lanes, the presence of bike lanes, transit pull-outs, pedestrian amenities, street trees, and sidewalks. The relationship of these street features to adjacent land uses, the location and supply of parking, and the proximity of buildings to the street is critical to the experience and choices of pedestrians, bicyclists, transit riders and motorists. Thus, to integrate the streets and trails with adjacent uses, development must be carefully designed in combination with the adjacent transportation system. Details must be suited to all users and to the form of the street. For example, major building entrances should face or be conveniently reached from streets, rather than parking lots.

In addition to private activities, such as homes, businesses and industry, some of the lands within the City will be used for public purposes and facilities. Although some those lands are identified in this Plan, such as the locations of future streets, other specific needs are identified in more detailed planning documents of the City, such as the Water System Plan which identifies this utility's need for new water tank sites. Olympia works with Thurston County and other local agencies to identify areas of shared need for public facilities.

The purpose of the goals and policies below is to direct land use patterns, densities, and design standards which:

- Reflect the community's urban design vision
- Maintain or improve the character of established neighborhoods
- Preserve the historic features of Olympia
- Provide for a variety of transportation alternatives
- Provide people with opportunities to live close to work
- Create desirable neighborhoods with a variety of housing opportunities, different lifestyles and income levels, and a sense of community

- Provide for a compact growth pattern
- Promote energy efficiency
- Reflect the land's physical and environmental capability
- Provide space for parks, open spaces, and other community facilities
- Protect views and features of the community's landscape valued by the public

GL1 Land use patterns, densities and site designs are sustainable and support decreasing automobile reliance.

PL1.1 Ensure that new development is built at urban densities or can be readily modified to achieve those densities; and require that development lacking municipal utility service be designed to cost-effectively transform when services become available.

PL1.2 Focus development in locations that will enhance the community and have capacity and efficient supporting services, and where adverse environmental impacts can be avoided or minimized.

PL1.3 Direct high-density development to areas with existing development where the terrain is conducive to walking, bicycling and transit use and where sensitive drainage basins will not be impacted.

PL1.4 Require functional and efficient development by adopting and periodically updating zoning consistent with the Future Land Use Map.

PL1.5 Require new development to meet appropriate minimum standards, such as landscaping and design guidelines, stormwater and other engineering standards, and buildings codes, and address risks, such as geologically hazardous areas; and require existing development to be gradually improved to such standards.

PL1.6 Provide for a compatible mix of housing and commercial uses in commercial districts and village sites that enables people to walk to work and shopping, supports transit, and includes convenience businesses for residents. Integrate adjacent uses with walkways and bike paths leading from residential areas to commercial districts and neighborhood-oriented businesses.

PL1.7 Enable frequent transit service, support housing, utilize existing infrastructure, provide public improvements and concentrate new major shopping, entertainment and office uses downtown, in the medical

services area of Lilly Road, near the Capital Mall, and in the urban corridors.

PL1.8 Buffer incompatible industrial, commercial and residential uses by requiring landscaped buffers or transitional uses, such as plazas, offices, or heavily landscaped parking; use natural buffers where possible and require clustering where warranted.

PL1.9 Require direct and convenient pedestrian access to commercial and public buildings from streets, bus stops and parking lots, and encourage sheltered seating and other uses of vacant sections of the street edge.

PL1.10 In pedestrian-oriented commercial areas, require sidewalk awnings or other weather protection on new and substantially remodeled buildings.

PL1.11 Require businesses along transit routes to accommodate transit use by including building entrances near bus stops or other features such as transit shelters or on-site bus access.

PL1.12 Encourage major commercial projects to include display windows, small shops with separate entrances, and plazas with seating and other well-landscaped gathering spaces.

PL1.13 Require new, and encourage existing, businesses to provide bicycle parking.

Land Use Patterns and Building Forms Determine Whether Energy is Used Efficiently

Land use patterns and development influence energy use. Blending of residential units with work places promotes energy efficiency. Higher densities contribute to the success of bus systems. Higher densities close to offices and commercial districts help reduce fuel consumption by reducing overall commuter and shopper mileage. In contrast, suburban densities and sprawl result in spending a lot of time and energy on transportation.

With a more compact development pattern and other transportation improvements, Thurston County's percentage of drive-alone commuters can be reduced from 85 to 60 percent. Park-and-ride lots, vanpooling, ridesharing and flexible work schedules can help reduce vehicle miles and congestion. Both the public and private sectors can encourage transit use

by offering bus passes and other incentives to employees. A well-laid-out transportation system will also aid in conserving energy. Smoother traffic flows can increase vehicle efficiency by up to five percent. Provisions for pedestrian and bicycle traffic can promote use of the energy saving means of commuting. By these means we could achieve a 10-15 percent transportation energy savings within a decade or two.

The primary residential use of energy is for space-heating. Thus, strengthening building code requirements for energy efficiency is an effective way to reduce energy consumption. When combined with appropriate insulation levels, solar energy can meet half the heating needs of a home in Olympia. Effective layout of subdivisions that allow for solar access and protection from winter winds can help, as can public education on energy conservation.

The competitive environment can stimulate energy efficiency by reducing production costs. Thus the combined industrial and commercial sectors do not use as much energy as either the transportation or residential sectors. Local governments can further influence industrial and commercial energy use through education and incentives.

The government sector is a very visible part of the energy picture and can set an example for efficient and conscientious energy use. Education in this sector includes both educating users, such as employees, and informing the public. Government buildings and equipment can be models of efficiency in the use of construction methods and materials, as well as utilizing efficient pumps, heating systems, and lighting. Government operations can also be models of use of alternative fuel sources and non-motorized travel.

GL2 Buildings, commercial and industrial processes, and site designs use energy efficiently.

PL2.1 Pursue partnerships to promote energy efficient construction and lighting, low-energy designs, and weatherization in both new and existing buildings. Encourage material subsidies for low-income citizens.

PL2.2 Promote public education and provide energy conservation and solar and other renewable energy information in cooperation with local utilities and others.

PL2.3 Encourage local 'cogeneration' of energy when environmentally

sound and not in conflict with other land uses.

PL2.4 Encourage and sometimes require buildings and site designs that result in energy efficiency and use of solar and other renewable energy.

PL2.5 Support efforts to protect solar access in existing structures and to incorporate solar access provisions into new development projects.

Urban Design, Historic Structures and Built Form

Olympia's Urban Design Vision and Strategy of 1991 identified the design and architectural preferences of community residents. This study continues to provide guidance for this Comprehensive Plan and future development. It identified the types of development that citizens feel are appropriate and inappropriate for our community. Study participants particularly valued Olympia's waterfront, downtown, the Capitol Campus, the older established neighborhoods, and views of the Olympic Mountains and the Black Hills. They favored streets that provide an attractive, safe, and inviting place for pedestrians, as well as provide for efficient traffic flow. Specifically, they liked the portions of downtown where buildings form a continuous edge along the street, where it is interesting to walk, and where awnings protect people from the rain.

Much of our community is already built. Many of our neighborhoods are more than 50 years old and our downtown is older still. These established neighborhoods provide the 'sense of place' and character of Olympia. To preserve this character, new buildings incorporated into the existing fabric must reflect both their own time-period and what's come before. We will acknowledge the importance of historic preservation by



The Bigelow House, Olympia's oldest residence.

protecting buildings and districts and celebrating the people and events that shaped our community. We will conserve natural resources by keeping historic buildings properly maintained and in continuous use, thereby avoiding decay and demolition which would waste resources used to create these structures.

However, our heritage extends beyond buildings and back in time before European settlement. Artifacts, photographs, structures, sites and stories of our collective past were entrusted to us and so should be preserved for future generations. Tribes, such as the Squaxin Island Tribe, play a major role in this task. Private property owners shoulder much of the responsibility of protecting historic buildings. And Olympia's Heritage Commission advises the City Council on matters of historic preservation and assists owners of historic buildings in caring for their property. With the community support we can ensure that our heritage is preserved for everyone to appreciate today and always.



Many of our older homes are a source of pride for young families.

Studies of Olympia and other communities also reveal that including open space and appropriate landscaping within site designs improves developments by providing places for relaxing, restoration and outdoor activities in general. In particular, trees provide a valuable public resource, enhance the quality of the environment, provide visual buffers and natural beauty, preserve the natural character of an area, and soften the impact of buildings and streets. Trees and other landscaping help reduce air pollution, noise and glare, provide cooling in summer and wind protection in winter, and in some cases provide materials and food for wildlife and humans. The goals and policies below encompass all of these elements of good design. **L3**

GL3 Historic resources are a key element in the overall design and establishment of a sense of place in Olympia.

PL3.1 Protect and evaluate historic and archaeological sites.

PL3.2 Preserve those elements of the community which are unique to Olympia or which exemplify its heritage.

PL3.3 Protect historic vistas from the Capitol Campus to Budd Inlet and the Olympic Mountains and from Budd Inlet to the Capitol Group.

PL3.4 Safeguard and promote sites, buildings, districts, structures and objects which reflect significant elements of the area's history.

PL3.5 Encourage development that is compatible with historic buildings and neighborhood character, and that includes complementary design elements such as mass, scale, materials, setting, and setbacks.

PL3.6 Plan for land uses that are compatible with and conducive to continued preservation of historic neighborhoods and properties; and promote and provide for the early identification and resolution of conflicts between the preservation of historic resources and competing land uses.

PL3.7 Identify, protect and maintain historic trees and landscapes that have significance to the community or a neighborhood, including species or placement of trees and other plants.

PL3.8 Encourage preservation and discourage demolitions or partial demolitions of intact historic structures. **L4**

GL4 Neighborhoods take pride in their historic identity.

PL4.1 Assist older neighborhoods and districts to discover their social and economic origins and appreciate their historic features. (Also see downtown section below.)

PL4.2 Facilitate the preservation of historic neighborhood identity and important historic resources.

GL5 Historic preservation is achieved in cooperation with all members of the community and is integrated into City decision-making processes.

PL5.1 Work with the State archeologist to protect archeological resources.

PL5.2 Coordinate with adjacent governments; particularly to provide public information about the area's history and development.

PL5.3 Recognize the contributions of minorities, workers, women and other cultures to Olympia's history.

PL5.4 Continue programs -- such as the Heritage Commission, the Heritage Register and the historic marker program -- that effectively identify, recognize, and encourage the preservation and continued use of historic structures, districts, and sites which provide physical evidence of the community's heritage.

PL5.5 Provide incentives and assistance for preserving, restoring, redeveloping and using historic buildings, districts, neighborhoods, streets, structures, objects and sites.

PL5.6 Support public or non-profit acquisition of the most important historic resources to ensure their preservation.

PL5.7 Recognize the value of historic preservation as part of the effort to maintain an affordable housing stock.

PL5.8 Promote economic vitality through historic preservation.

PL5.9 Promote mutual goals in historic areas, including districts, buildings and site, through collaboration among City departments, the Heritage Commission and other commissions. **L6**

GL6 Community beauty is combined with unique neighborhood identities.

PL6.1 Establish a design review process for:

- Commercial and mixed use development adjacent to freeways and public streets
- Other highly-visible, non-residential development, such as the Port of Olympia, campus developments, and master planned developments
- Multifamily residential development and manufactured housing parks
- Detached homes on smaller lots (less than 5,000 square feet) and in older neighborhoods (pre-1940)

- Properties listed on a Historic Register or located within a designated historic district

PL6.2 The design review process should recognize differences in the city with the objective of maintaining or improving the character and livability of each area or neighborhood.

PL6.3 Require commercial and residential buildings to face the street or a courtyard or other common area.

PL6.4 Require multi-family housing to incorporate architectural forms and features common to nearby housing; to include porches, balconies, bay windows and similar details; to have entries oriented to streets or a courtyard, and include accessible open space; and to be reduced in size near lower density residential districts.

PL6.5 Ensure that parking areas do not dominate street frontages or interrupt pedestrian routes, and that they are screened from single-family housing.

PL6.6 Prohibit fences and walls that inhibit walking or isolate neighborhoods from streets, except to reduce noise, provide buffers, or create private rear yards.

PL6.7 Create attractive entry corridors to the community and neighborhoods, especially downtown and along urban corridors; to include adopting design standards and installing significant special landscaping along community-entry corridors.

PL6.8 Enhance neighborhood identity by encouraging interested groups to beautify open spaces, streets and private property.

PL6.9 Require that buildings complement and enhance their surroundings, appeal to and support pedestrian activities, and facilitate transit use.

PL6.10 Preserve and enhance water vistas by retaining public rights-of-way that abut or are within one block of water bodies and by not siting public buildings within associated view corridors.



Percival Landing is enjoyable to view and to enjoy the view.

PL6.11 Plant and protect trees that contribute to Olympia's visual identity and sense of place.

PL6.12 Separate incompatible land uses and activities with treed areas, including buffering residential areas from major streets and freeways.

GL7 Urban green space is available to the public and located throughout the community and incorporates natural environments into the urban setting, which are easily accessible and viewable so that people can experience nature daily and nearby.

PL7.1 Provide urban green spaces in which to spend time. Include such elements as trees, garden spaces, variety of vegetation, water features, "green" walls and roofs, and seating.

PL7.2 Provide urban green spaces that are in people's immediate vicinity and can be enjoyed or viewed from a variety of perspectives.

PL7.3 Establish a maximum distance to urban green space for everyone in the community.

PL7.4 Increase the area of urban green space and tree canopy within each neighborhood proportionate to increased population in that neighborhood.

PL7.5 Establish urban green space between transportation corridors and adjacent areas.

GL8 Community views are protected, preserved, and enhanced.

PL8.1 Implement public processes, including the use of digital simulation software, to identify important landmark views and observation points.

PL8.2 Use visualization tools to identify view planes and sightline heights between the landmark view and observation point.

PL8.3 Prevent blockage of landmark views by limiting the heights of buildings or structures on the west and east Olympia ridge lines.

PL8.4 Avoid height bonuses and incentives that interfere with landmark views.

PL8.5 Set absolute maximum building heights to preserve publicly-identified observation points and landmark views.



Percival Landing with the Olympics in the distance.

GL9

GL9 Built and natural environmental designs discourage criminal behavior.

PL9.1 Incorporate crime prevention principles in planning and development review and educate designers regarding those principles.

PL9.2 Modify public facilities and properties to enhance crime prevention.

Industry

Industrial uses represent a relatively small but key component of Olympia's jobs. Olympia's waterfront has supported forest-related industries and maritime shipping for decades. The Olympia area also contains a few scattered, relatively small, light-industrial districts which support a variety of uses. Industrial districts in Tumwater, Lacey, and in the County will likely absorb most of the area's new, non-waterfront-dependent industrial uses. However, the industrial land along Budd Inlet provides the only sites in the area for water-dependent industrial uses. This Plan aims to focus industrial development:

- Along Budd Inlet (in industrial districts)
- At Mottman Industrial Park, and

- Along Fones Road

while encouraging opportunities for small-scale industry integrated with other uses of land.

The Port of Olympia owns approximately two hundred acres and adjacent tidelands of what is known as the 'Port peninsula,' an area equivalent to about 80 city blocks. The Port peninsula includes a variety of industrial, commercial, retail, and recreational facilities. The centerpiece of the Port peninsula is its international marine shipping terminal. The East Bay waterfront is the location of the East Bay Marina, with moorage, a boat launch, and support facilities. On the northern end of the peninsula, the 17-acre Cascade Pole site is a contaminated area, used from 1940 to 1986 to treat wood poles with creosote and other chemicals. Although cleanup of that site is underway, future use will be restricted.



Batdorf and Bronson Coffee Roasters at the Port of Olympia.

The industrial portion of the Port peninsula will continue to be the community's key industrial center. It has been, and should continue to be, a local source of family-wage jobs, handling inbound and outbound cargo by rail, truck and ship. Large buildings are anticipated for boat building and repair. A one-stop, full-service marine facility with a large vessel haul-out and repair center may be added.

GL10 Industry and related development with low environmental impact is well-located to help diversify the local economy.

PL10.1 Encourage industry that is compatible with surrounding land uses and diversifies and strengthens the local economy.

PL10.2 Designate and preserve sufficient land for industrial uses consistent with the regional strategy for 'build out' of the community and competitive land prices.

PL10.3 Encourage full, intensive use of industrial areas while safeguarding the environment. Ensure land-use compatibility by buffering, height limits, landscaping, traffic routing, building design, and operation and maintenance standards.

PL10.4 Limit non-industrial uses in industrial areas to those which do not conflict with industry; and eliminate or reduce the size of industrial areas only if not expected to be needed or not suitable for industry.

PL10.5 Focus major industries in locations with good freeway access, adequate utilities, minimal environmental constraints, sufficient space and minimal land-use conflicts. Specific areas identified for industrial use include the Port Peninsula, the Mottman Industrial Park, and the vicinity of Fones Road.

PL10.6 Coordinate with the Port of Olympia to allow for long-term viability of Port peninsula industry, compatibility with surrounding uses, and continuation of marina uses along East Bay. This coordination should address – at a minimum - transportation, pedestrian and recreation facilities, environmental stewardship, and overwater development.

PL10.7 Design industrial areas for convenient freight access.

PL10.8 Provide opportunities for light industrial uses in commercial areas consistent with the commercial and multi-family uses of those areas, such as low-impact production within buildings with retail storefronts.

Commercial Uses and Urban Corridors

More intensive development in commercial areas will increase their vitality and make better use of the City's transit and street systems. For this reason, major new commercial areas are not to be created. Any new commercial areas will be limited to allowing neighborhood-oriented businesses and services in the neighborhood centers of residential areas that reduce the need for residents to travel far to shop.

Over time, we envision our existing commercial areas becoming more attractive to pedestrians and customers, to the point where they can attract a more balanced and attractive mix of commercial, residential, and recreational uses. Significant changes will need to occur for some of our commercial areas to increase their appeal as places to shop, live, work, and visit and to become more inviting higher-density, pedestrian-friendly, mixed-use areas for pedestrian and transit users.

GL11 Adequate commercial land conveniently serves local and regional trade areas.

PL11.1 Encourage increasing the intensity and diversity of development in existing commercial areas by mixing commercial and multi-family development along with entertainment and cultural centers in a way that will reduce reliance on cars and enable people to work, shop, recreate and reside in the same area.

PL11.2 Provide incentives for housing in commercial districts near transit stops.

PL11.3 Work with developers to identify commercial areas for infill and redevelopment, to remove unnecessary barriers to this type of development, and to provide the infrastructure needed for intensive commercial and mixed use development.

PL11.4 Locate and size commercial areas to decrease reliance on cars, improve community life, and maintain the tax base.

PL11.5 Encourage the efficient use and design of commercial parking areas; reduce parking space requirements (but avoid significant overflow into residential areas); support parking structures, especially downtown and in urban corridors; and designate streets for on-street parking where safe.

PL11.6 Encourage new commercial uses adjacent to the arterial street edge and in mixed-use projects.

PL11.7 Provide convenient pedestrian access to and between businesses.

PL11.8 Prohibit new and expanded commercial 'strips;' and allow conversion of such existing uses to a multi-use development with greater depth and integration of residential units.

PL11.9 Outside urban corridors provide for low-intensity commerce that depends on automobile access and allow wholesale businesses near major customers or where resulting traffic will not impact retail areas.

GL12 Commercial areas are attractive, functional and appealing.

PL12.1 Work with businesses and residents to help make commercial areas functional and attractive.

PL12.2 Establish maximum building heights that are proportional to streets, retain scenic views and result in compatibility with adjoining development.

PL12.3 Seek opportunities to create or enhance town squares framed by commercial or civic buildings, pocket parks, plazas and other small public or private spaces in downtown or other high-density areas.

PL12.4 Ensure that commercial uses are compatible with adjoining residential districts. This might include prohibiting reflective surfaces, screening solid waste and parking areas, regulating emissions, building size reductions and increased setbacks near residential districts, screening parking areas, and requiring facades with architectural features that reduce the appearance of a commercial building's size, such as stepbacks and tiering above three stories.

PL12.5 Require site designs for commercial and public buildings that will complement nearby development and either maintain or improve the appearance of the area. This may include building designs with a defined bottom, middle, and top; appealing architectural elements such as windows, wall detailing; fountains, vendor stations; and the use of balconies, stepped back stories and pitched roofs that reduce the perceived size of the building.

PL12.6 Create visual continuity along arterial streets through coordinated site planning, landscaping, building designs, signage and streetscapes.

PL12.7 Require screening of unattractive site features such as mechanical equipment and large solid waste receptacles, while maintaining good access for collection and maintenance.

PL12.8 Use design standards to ensure pedestrians and bicyclists have direct, convenient access to commercial and public buildings.

PL12.9 Require a form of parking that retains aesthetics and minimizes pedestrian barriers and inconvenience by including screening along streets and residential areas; limits parking lots to one contiguous acre; and locates them at the rear of buildings, or, if the rear is not possible, then on the side, but with minimal street frontage.

PL12.10 Ensure that business signs identify the business but do not create visual clutter or dominate the character of the area; require the use of low or façade-mounted signs where possible.

Urban Corridors

Portions of our major arterial streets are lined with low-density residential and office uses and typical strip-commercial development. Driveways to each business interrupt and slow the flow of vehicular and pedestrian traffic; the pattern of buildings behind parking lots makes pedestrian access difficult and uninviting; and the disjointed signage, landscaping, and building designs are often unattractive. As a result, these areas have limited appeal as places to live, work, and shop.

Over time, thoughtful planning will change some of these sections of major streets into 'urban corridors' that will have a mix of high-density uses, and where people will enjoy walking, shopping, working, and living. See [Transportation Corridors Map](#). Urban corridors like this are key to avoiding sprawl by providing an appealing housing alternative for people who want to live in an attractive, bustling urban environment close to transit, work and shopping. Redevelopment along these corridors will be focused in areas with the greatest potential for intensive, mixed-use development so that public and private investment will have maximum benefit. These corridors, first described in the 1993 Thurston Regional Transportation Plan, also should include land uses that support the community, such as community centers, day care centers, social service

offices, educational functions, parks, and other public open space.

In cooperation with Lacey, Tumwater and Thurston County, this Plan calls for gradually redeveloping these urban corridors (listed below) with:

- Compatible housing, such as apartments and townhouses, within or near commercial uses
- Excellent, frequent transit service
- Housing and employment densities sufficient to support frequent transit service
- Wide sidewalks with trees, attractive landscaping, and benches
- Multi-story buildings oriented toward the street rather than parking lots
- Parking spaces located behind the buildings or in structures

The land use designations along these streets vary (see Future Land Use Map at the end of this chapter), to promote a gradual increase in density and scale of uses that supports and remains in context with the adjacent neighborhoods. Slightly less intensive land uses at the fringes of these corridors will create a gradual transition from the activity of the major street edge to less-dense areas in adjacent neighborhoods. Similarly, areas furthest from the downtown core are expected to infill and redevelop with excellent support both for cars and for those who walk, bike and use public transit.

These outer reaches of the urban corridors will feature buildings and walkways with safe and easy pedestrian access. Walkways will link those on foot to bus stops, stores, neighboring residences, free-standing businesses on corners, and perimeter sidewalks.

"Gateways" to Olympia are to be located at the entry/exit points of landscaped "civic boulevards," at city boundaries, topographical changes, transition in land use, and shifts in transportation densities. Three of the eight gateways are located at the city limits and may include "Welcome to Olympia" signage. Gateways provide a grand entrance into the capital city of the State of Washington. Gateways are to be densely planted with trees and native understories; consideration will be given to the maximum landscaping and amenities feasible. Each civic boulevard will have a distinctive special environmental setting that is shaped by a public planning process that involves citizens, neighborhoods, and city officials. Civic boulevards are to be densely planted with trees and native understory; consideration will be given to the maximum landscaping and

amenities feasible. **GL1**



GL13 Attractive urban corridors of mixed uses are established near specified major streets.

PL13.1 Establish urban corridors as shown on the [Future Land Use Map](#) with potential employment and residential density to support frequent transit service, encourage pedestrian traffic between businesses, and provide a large customer base and minimize auto use for local trips.

PL13.2 Regionally coordinate urban corridor planning and improvements including public facilities and services in these areas to ensure redevelopment is continuous, consistent, and balanced.

PL13.3 Transform urban corridors into areas with excellent transit service; multi-story buildings fronting major streets with trees, benches and landscaping; parking lots behind buildings; and a compatible mix of residential uses close to commercial uses.

PL13.4 Establish minimum housing densities in urban corridors to support frequent transit service and sustain area businesses.

PL13.5 Ensure appropriate transitional land uses from high intensity land

uses along the arterial streets of the urban corridors to the uses adjacent to the corridors; corridor redevelopment should enhance both the corridor and quality of life in adjacent residential neighborhoods.

PL13.6 Focus public intervention and incentives on encouraging housing and walking, biking and transit improvements in the portions of the urban corridors nearest downtown and other areas with substantial potential for redevelopment consistent with this Plan. These include, for example, the area from the Fourth Avenue/Pacific Avenue intersection east to Pattison Avenue, and the area near the intersection of Harrison Avenue and Division Street.

PL13.7 Designate different categories of corridors generally as follows:

- Areas nearest downtown along Harrison Avenue east of Division Street and the upper portions of the State Street/Fourth Avenue corridor to the intersection of Fourth Avenue and Pacific Avenue should blend travel modes with priority for pedestrian, bicycle and transit systems. These areas should provide for a mix of low-intensity professional offices, commercial uses and multifamily buildings forming a continuous and pedestrian-oriented edge along the arterial streets. There will be a 35 feet height limit if any portion of the building is within 100' from a single-family residential zone, provided that the City may establish an additional height bonus for residential development except in areas adjacent to a designated historic district.
- The area along Harrison Avenue west from the vicinity of Division Street to Cooper Point Road - and the portions of Martin Way and Pacific Avenues from Lilly Road to the intersection of Fourth Avenue and Pacific Avenue - will transition away from cars being the primary transportation mode to a more walkable environment, where bicycling and transit are also encouraged. Redevelopment of the area will create more density and new buildings that gradually create a continuous street edge and more pedestrian-friendly streetscape.
- The outer portions of the urban corridors west of the vicinity of the Capital Mall and east of Lilly Road will primarily be accessed by motor vehicles with provisions for pedestrian and bicycle travel; gradual transition from existing suburban character is to form continuous pedestrian-friendly streetscapes, but more regulatory flexibility will be provided to acknowledge the existing suburban nature of these areas. (See Capital Mall special area below.)

GL14

GL14 Olympia's neighborhoods provide housing choices that fit the diversity of local income levels and lifestyles. They are shaped by thorough public planning processes that involve citizens, neighborhoods, and city officials.

PL14.1 Establish eight gateways with civic boulevards that are entry/exit pathways along major streets to downtown Olympia and the Capitol.

PL14.2 Concentrate housing into three high-density Neighborhoods: Downtown Olympia, Pacific/Martin/Lilly Triangle; and the area surrounding Capital Mall. Commercial uses directly serve high-density neighborhoods and allow people to meet their daily needs without traveling outside their neighborhood. High-density neighborhoods are highly walkable. At least one-quarter of the forecasted growth is planned for downtown Olympia.

PL14.3 Preserve and enhance the character of existing established Low-density Neighborhoods. Disallow medium or high-density development in existing Low-density Neighborhood areas except for Neighborhood Centers.

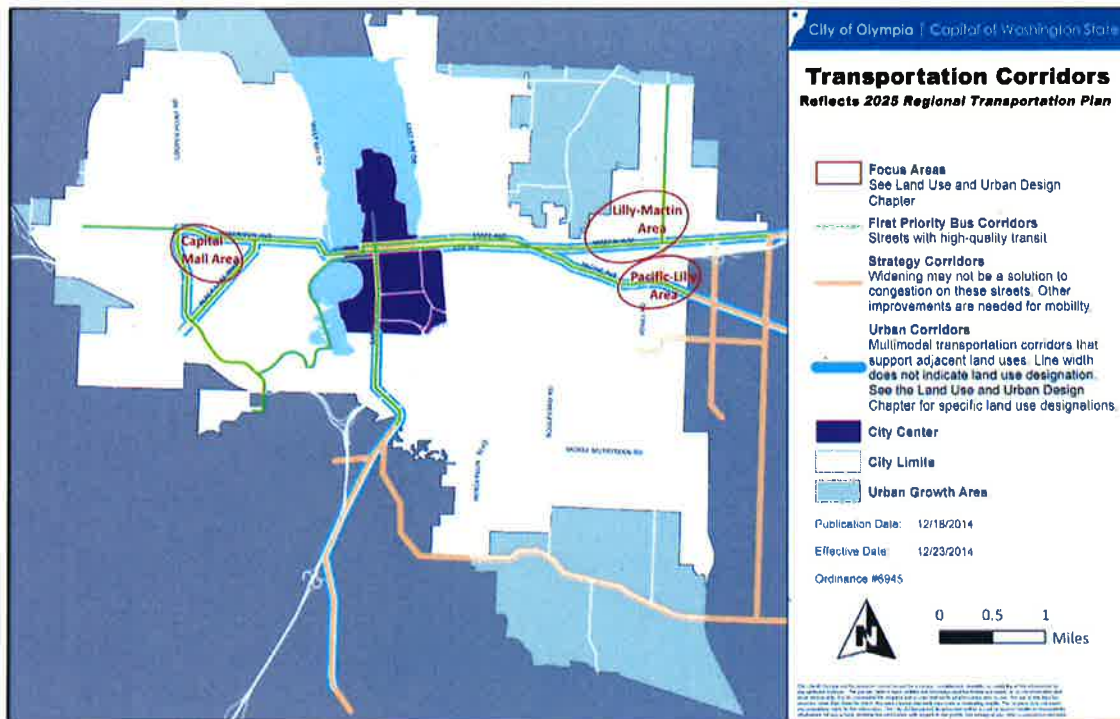
PL14.4 In low-density Neighborhoods, allow medium-density Neighborhood Centers that include civic and commercial uses that serve the neighborhood. Neighborhood centers emerge from a neighborhood public process.

Focus Areas

The City prepares plans and studies to help guide the future of targeted areas within our community. Leadership for plan preparation will vary by location and purpose, and priorities depend on funding availability and the potential for appropriate development or redevelopment. Generally, these plans feature the location, size and type of land uses; residential and employment density targets; pedestrian amenities; street system and parking location and quantity; and other public improvements. A few specific areas have been identified; more may be identified in the future.

Several of the city's commercial and industrial areas have distinct roles, opportunities, and limitations. This section provides further guidance for the future of some of these areas. The City envisions some areas, such as

the vicinity of Capital Mall, as areas that will gradually convert into urban neighborhoods with a mix of land uses. Others, such as the Auto Mall area, will be reserved for one or two primary uses. In cooperation with landowners and others, the City will be focusing its planning efforts on three of these urban corridor 'focus areas', possibly in the form of a 'master plan' that addresses issues such as land use, infrastructure and design.



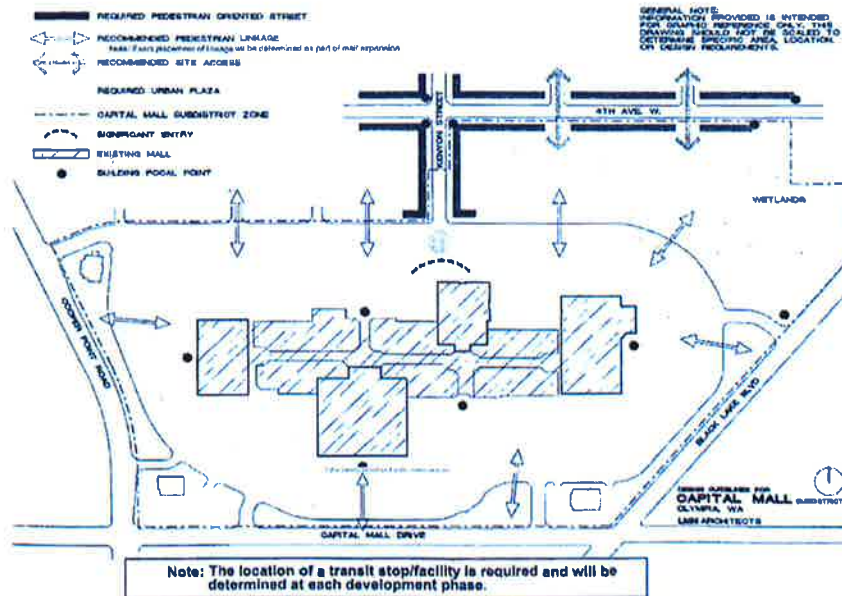
See Transportation Corridors map.

In addition to the focus areas described below, the City works with the State of Washington in its preparation of the Capitol Campus Master Plan and with the Port of Olympia in its planning of its properties including the Port peninsula. Included in these efforts is the continuing goal of integrating these areas with downtown Olympia. The Future Land Use Map frames all of these planning efforts.

Capital Mall Area

The Capital Mall area is a regional shopping center, which also includes one of the area's best balances of jobs within walking distance of medium-density housing. This area should continue to be economically viable and contribute to the community's goals with infill, redevelopment, and connections to adjacent areas for all modes of travel. It is to evolve into a complete urban neighborhood with a mix of jobs, housing, and services. Redevelopment and incremental expansion consistent with community goals will allow the mall to flexibly adapt to retail trends. Design standards will encourage continued infill and redevelopment in the vicinity of 4th

Avenue and Kenyon Street so that the potential of the mall and its surrounding properties can be fully realized. As illustrated below, redevelopment to the north, south, east and west will incorporate vehicle access and circulation with the addition of building focal points, significant entries and better access for walking from surrounding neighborhoods.



A plan for linking Capital Mall to its neighborhood.

Auto Mall Area

The Olympia Auto Mall is the region's major center for auto sales and specialized services. Most of Thurston County's new and used car dealers are located here, along with firms offering light trucks and motorcycles, auto rentals, body repair and detailing, and other auto-oriented businesses. Because it offers so many opportunities for comparison shopping in one location, it is a highly successful group of businesses, attracting customers from a regional trade area, and a significant employment center. Its proven formula should continue to serve the community successfully for many years to come.



Landscaping enhances auto dealerships.

Lilly and Martin Area

The Medical Services district along Lilly Road near Martin Way is home to a regional hospital and numerous medical and dental clinics and offices. However, portions of Martin Way, once a rural highway, are little changed. These areas have the potential for additional health-care related uses, and multi-family, senior citizen, and assisted-living housing, as well as supporting retail and service businesses. Thus this area is expected to continue to evolve into a medically-oriented neighborhood with jobs, housing, and supporting services.

Pacific Avenue and Lilly Road Area

The area surrounding the intersection of Pacific Avenue and Lilly Road, like the nearby Stoll Road area, has the potential to become a unique area within an urban corridor. It is located next to a regional trail, lies between two shopping centers, and includes a nearly complete street grid with many single-family homes. This location provides good access to retail services for daily and weekly shopping needs within easy walking distance for its residents, and is large enough for planned creative designs. Transit service on both Pacific Avenue and Lilly Road is excellent. But the area

also has its challenges, such as substandard public improvements, no nearby parks, and surrounding traffic. City plans call for this area to be developed with a mix of retail, service, and high-density residential uses consistent with its location in an urban corridor.

West Bay Drive

The West Bay Drive area has a challenging mix of opportunities and constraints. Several sites along the shore are significant in Squaxin Island Tribal cultural history. Industrial use of this waterfront dates to the nineteenth century. The shallow waters along this shoreline continue to provide crucial habitat for young salmon leaving the Deschutes River basin. Birds, marine and upland mammals, and other wildlife species are relatively common for an urban area. The area known as the Port Lagoon, which is subject to a U.S. Fish and Wildlife Service conservation easement, serves as a fish and wildlife conservancy area.

Most industry has left this area, and only fragments of waterborne commerce remain. The community foresees continued transition of the West Bay Drive area toward a mix of urban uses and habitat improvements, while also allowing existing industries and shipping facilities to remain economically viable. The resulting mix of uses should form the foundation for a vibrant mix of light-industrial, office, restaurant, commercial, recreational, and residential uses, that also provides improved habitat for fish and wildlife. Future development and street improvements in this corridor will be consistent with the West Bay Drive Corridor. **L15**

Kaiser Harrison Opportunity Area

The Kaiser Harrison Opportunity Area Plan identifies a preferred alternative for a mixed use, pedestrian and bicycle friendly neighborhood. The area is intended to be walkable, accessible by transit, and to provide amenities such as gathering spaces and outdoor seating. The area has a distinct character with a lifestyle retail center that includes outdoor seating and gathering spaces. The lifestyle retail center will be a place that accommodates cars but is also designed for the safe and convenient enjoyment of bicyclists and pedestrians. A multi-use trail and neighborhood park are planned. Future development and street improvements in this opportunity area will be consistent with the Kaiser Harrison Opportunity Area Plan.

GL15 Focus areas are planned in cooperation with property owners and residents.

PL15.1 Maximize the potential of the Capital Mall area as a regional shopping center by encouraging development that caters to a regional market, by providing pedestrian walkways between businesses and areas; by increasing shopper convenience and reducing traffic by supporting transit service linked to downtown; by encouraging redevelopment of parking areas with buildings and parking structures; and by encouraging multifamily housing.

PL15.2 Maximize the potential of the Olympia Auto Mall as a regional auto sales and services center by encouraging its use for auto sales and services and limiting incompatible activities, and by imposing auto-oriented design guidelines along Cooper Point Road that ensure pleasing landscaping, minimal visual clutter, and easy pedestrian and vehicle access.

PL15.3 Enhance the Lilly Road hospital area as a medical services center by encouraging health-care supporting uses such as restaurants, florists, child care, and convenience shops, and upper floor and rear multi-family and senior housing nursing homes; and by prohibiting non-medical uses that would generate high traffic volumes or noise disruptive of recuperation.

PL15.4 Plan for redevelopment of the Stoll Road area and that area bounded by Lilly Road, Pacific Avenue and I-5 as 'focus areas' adjacent to the Pacific Avenue and Martin Way urban corridors to include retail, office, personal and professional services and high density housing. Planning for these areas should encompass consideration of redevelopment and improvement of nearby portions of the urban corridor.

PL15.5 In the West Bay Drive area provide for a mix of recreation and urban uses that enhance wildlife habitat and cultural resources; limit industrial uses to existing sites; minimize blockage of upland views of Budd Inlet; and connect the area to the south with an urban trail.



South Puget Sound Community College is a valued feature of Olympia.

PL15.6 Work cooperatively with the State of Washington on planning for the Capitol Campus, and the Port of Olympia in planning for its properties. Provide opportunities for long-term 'master planning' of other single-purpose properties of at least 20 acres, such as hospitals, colleges, and high-school campuses.

Housing

Adequate and affordable housing is critical to a healthy community. The Growth Management Act directs each community to plan for it by:

- Encouraging affordable housing for all economic segments of the population
- Promoting a variety of residential densities and housing types
- Encouraging preservation of existing housing stock
- Identifying sufficient land for housing, including government-assisted housing, housing for low-income families, manufactured housing, multi-family housing, group homes, and foster-care facilities

The strategies of this chapter depend on well-formulated design standards to promote flexibility and stimulate innovation while preserving and enhancing the character of neighborhoods. We seek to establish and

encourage diversity in housing opportunities and link diverse neighborhoods. With a strong foundation in preserving our heritage, our community can incorporate new housing and other developments in a manner that continues our legacy of well-planned neighborhoods. The housing goals and policies below provide a framework for residential land uses in Olympia's area. See the City's related programs for supporting affordable housing in the Public Services chapter.



An apartment building is added to the City's housing stock.

Many factors contribute to the need for more and varied housing:

- Olympia's growing residential population
- Varying household incomes
- The capitol's legislative session creates a demand for short-term housing
- College students seek affordable housing near transportation corridors and services
- Households are getting smaller
- The proportion of senior citizens is increasing

The City will annually provide information to citizens on affordable housing, family incomes, and market-rate housing.

Olympia is part of a larger housing market extending throughout Thurston

County and beyond. Thus planning for housing is done based on anticipated shares of this larger area. The 2010 Census indicated that Olympia and its urban growth area included almost 26,000 housing units. As estimated in the Thurston Regional Planning Council "Profile," 57% were single-family homes, 39% were multi-family (shared-wall) units, and 4% were manufactured housing. The 2014 Buildable Lands Report for Thurston County estimated that about 13,000 new housing units will be needed by 2035 to accommodate population growth in Olympia's urban growth area. Of these, about 45% are expected to be single-family homes.

Based on existing zoning and development patterns, that Buildable Lands Report indicated the area could accommodate about 16,000 new housing units. In addition to large areas zoned for single-family development, almost 400 acres of vacant multi-family-and duplex zoned land were available. And, an additional 500 acres of vacant and partially-used commercial land could be redeveloped for new housing.

Because Olympia generally allows small group homes and manufactured housing wherever single-family homes are permitted, allows larger group homes by special approval, and does not discriminate with regard to government-assisted housing, foster-care, or low-income housing, the area is expected to be adequate to accommodate all types of housing.

Similarly, the 2008 Thurston County Consolidated Plan for housing indicates that there is no shortage of land for affordable housing. However, there is a "mismatch" between the availability of affordable housing and the need for such housing, both at the lowest end of the income scale and the upper end of the moderate-income bracket. That Plan and the Public Services Chapter of this Plan describe efforts to close these gaps and make adequate provisions for all economic segments of the community.

To meet all housing needs, we must keep growth compact, so it can preserve space for future residents and reduce the cost of public services. To ensure this happens, we will need to allocate enough land that will be suitable for a variety of housing types and costs including detached homes, duplexes, group homes, small cottages, apartments, special needs housing, manufactured housing, and accessory dwellings. This approach can provide both variety and affordable options. For example, factory-built manufactured housing governed by federal standards and modular housing built to state standards are often less expensive than site-built housing. This Plan provides for these types of units and more luxurious

and higher-priced shared-wall housing, including condominiums and townhouses.

Housing costs in the Olympia area rose rapidly from 1990 until the economic recession of 2008. In general the cost of owner-occupied housing rose more rapidly than income, while rents roughly corresponded to income changes. Those changing costs and availability of land for development, combined with public preferences, resulted in gradual changes in the area's ownership. While county-wide owner-occupancy rose from 65% to 68% between 1990 and 2010, owner-occupancy in the City declined from 52% to 50%. The type of housing structures being added to the housing stock has varied as a result of similar factors. As a result, multi-family housing county-wide increased gradually from about 16% in 1970 to about 22% by 2010. In the Olympia city limits multi-family structures provided 28% of the housing in 1970, and gradually increased to about 42% by 2010 as most new apartments were being built inside the urban areas.

GL16 The range of housing types and densities are consistent with the community's changing population needs and preferences.

PL16.1 Support increasing housing densities through the well-designed, efficient, and cost-effective use of buildable land, consistent with environmental constraints and affordability. Use both incentives and regulations, such as minimum and maximum density limits, to achieve such efficient use.

PL16.2 Adopt zoning that allows a wide variety of compatible housing types and densities.

PL16.3 Allow 'clustering' of housing compatible with the adjacent neighborhood to preserve and protect environmentally sensitive areas.

PL16.4 Disperse low and moderate-income and special needs housing throughout the urban area.

PL16.5 Support affordable housing throughout the community by minimizing regulatory review risks, time and costs and removing unnecessary barriers to housing, by permitting small dwelling units accessory to single-family housing, and by allowing a mix of housing types.

PL16.6 Promote home ownership, including by allowing manufactured

homes on individual lots, promoting preservation of manufactured home parks and allowing these parks in multi-family and commercial areas, all subject to design standards ensuring compatibility with surrounding housing and land uses.

PL16.7 Allow single-family housing on small lots, but prohibit reduced setbacks abutting conventional lots.

PL16.8 Encourage and provide incentives for residences above businesses.

PL16.9 In all residential areas, allow small cottages and townhouses, and one accessory housing unit per home -- all subject to siting, design and parking requirements that ensure neighborhood character is maintained.

PL16.10 Require effective, but not unreasonably expensive, building designs and landscaping to blend multi-family housing into neighborhoods.

PL16.11 Require that multi-family structures be located near a collector street with transit, or near an arterial street, or near a neighborhood center, and that they be designed for compatibility with adjacent lower density housing; and be 'stepped' to conform with topography.

PL16.12 Require a mix of single-family and multi-family structures in villages, mixed residential density districts, and apartment projects when these exceed five acres; and use a variety of housing types and setbacks to transition to adjacent single-family areas.

PL16.13 Encourage adapting non-residential buildings for housing.

PL16.14 Provide annual information on affordable homeownership and rentals in the City, including the operative definitions of affordable housing, criteria to qualify for local, state, and federal housing assistance, data on current levels of market-rate and affordable housing, demand for market-rate and affordable housing, and progress toward meeting market-rate and affordable housing goals.

Downtown and other Neighborhoods

Our community is composed of many neighborhoods. Some, like the downtown area, are composed of commercial, cultural and residential activities and land uses. Other neighborhoods are primarily residential, with nearby parks and schools. This section of the Plan addresses these varied and unique places that together form Olympia.

Downtown Olympia

A community needs a "heart." For our community, the downtown area performs this role, not just for our city, but for the larger region. Downtown Olympia thus deserves and receives special attention. A city

with a thriving downtown has more potential for bolstering community spirit and providing a healthy local economy.

Olympia's downtown includes over 500 acres. It is bounded generally by the State Capitol Campus, Capitol Lake, Budd Inlet, and Plum Street. This area includes Olympia's retail core, State and other office uses, and access to the waterfront, and is the center of most major transportation links. It is the social, cultural, and economic center of the area.

Downtown will continue to be an attractive place to live, work and play. Future office, retail and residential development will support downtown's role as a regional center and home of state government, commerce, and industry. Given its history, physical location and established identity, downtown Olympia will continue to be the heart of Olympia and the region. **L17**

GL17 Regional urban activity is centered in downtown Olympia.

PL17.1 Adopt a Downtown Plan addressing - at minimum - housing, public spaces, parking management, rehabilitation and redevelopment, architecture and cultural resources, building skyline and views, and relationships to the Port peninsula and Capitol Campus.

PL17.2 Include public art and public spaces in the downtown landscape.

PL17.3 Through aggressive marketing and extra height, encourage intensive downtown residential and commercial development (at least 15 units and 25 employees per acre) sufficient to support frequent transit service.

PL17.4 Encourage development that caters to a regional market.

PL17.5 Coordinate with State of Washington and Port of Olympia to ensure that both the Capitol Campus plan and Port peninsula development are consistent with and support the community's vision for downtown Olympia.



The Farmers Market, where downtown meets the Port.

PL17.6 Landscape the downtown with trees, planters and baskets, banners, community gardens and other decorative improvements. **GL18**

GL18 Downtown designs express Olympia's heritage and future in a compact and pedestrian-oriented manner.

PL18.1 Regulate the design of downtown development with specific but flexible guidelines that allow for creativity and innovation, enhance historic architecture and recognize distinct areas of downtown, and do not discourage development.

PL18.2 Require that downtown development provide active spaces, adequate sunlight and air-flow and minimize 'blank' walls at street level.

PL18.3 Require development designs that favor pedestrians over cars by including awnings and rain protection that blend with historic architecture, create interest, and minimize security and safety risks; development designs should also foster cultural events, entertainment, and tourism.

PL18.4 Provide for private use of public lands and rights-of-way when in the best interest of the community.

PL18.5 Design streets with landscaping, wide sidewalks, underground

utilities and a coordinated pattern of unifying details.

PL18.6 Designate 'pedestrian streets' where most of the frontage will have 'people-oriented' activities and street-level buildings will have a high proportion of glass. Prohibit parking lots along these streets, except when preserving scenic views and instead provide surface parking along other streets.

PL18.7 Plant, maintain, and protect downtown trees for enjoyment and beauty; coordinate planting, with special attention to Legion Way and Sylvester Park and a buffer from the Port's marine terminal.

PL18.8 Limit drive-through facilities to the vicinity of the Plum Street freeway interchange.

PL18.9 Limit building heights to accentuate, and retain selected public views of, the Capitol dome.

GL19 Downtown's historic character and significant historic buildings, structures, and sites are preserved and enhanced.

PL19.1 Promote the Downtown Historic District to provide a focal point of historic interest, maintain the economic vitality of downtown, and enhance the richness and diversity of Olympia.

PL19.2 Minimize damage to significant historic features or character during rehabilitation projects.

PL19.3 Design new development and renovations so they are compatible and harmonious with the established pattern, alignment, size and shape of existing downtown area.

PL19.4 Incorporate historic buildings into redevelopment projects and restore historic facades.

Neighborhoods

This section contains the goals and policies that will protect and improve the character and livability of our established neighborhoods and shape our new neighborhoods. All of the city's neighborhoods are envisioned as places where many features are available within a ten-minute walk. A

variety of housing types located along pleasant, pedestrian-oriented streets will provide quality living opportunities. Most housing will be single-family detached homes, but higher-density housing will be available near major streets and commercial areas to take advantage of transit, other services, and employment opportunities. Housing types and densities will be dispersed throughout the city to minimize social problems sometimes associated with isolating people of similar means and lifestyles.



One of Olympia's many attractive neighborhoods.
Each neighborhood should have:

- Narrow, tree-lined streets that are easy and interesting to use for walking, bicycling, and travel by transit
- A system of open space and trails with a neighborhood park
- A readily-accessible elementary school or other place of public assembly
- Diverse housing types that accommodate varying income levels, household sizes, and lifestyles
- Sufficient housing densities to support frequent transit service and sustain neighborhood businesses
- A 'neighborhood center' with businesses serving area residents



A neighborhood grocery near the Capitol.

A large portion of Olympia's residents are to live within a quarter-mile of a neighborhood center. These centers will be focal points of neighborhoods. Although they will vary by location, they generally should contain small-scale convenience and service businesses, a transit stop and a neighborhood park and be bounded by moderate or high-density housing. These neighborhood centers will serve as activity hubs or small-scale town squares that foster social interaction and a sense of community and accommodate nearby residents' routine shopping needs.

Where possible, a network of walking and biking routes that provide both recreational and commuting opportunities will connect these neighborhood centers to parks, schools, and downtown. To minimize traffic impacts and provide for transit service, these centers will be near major streets. Approximate locations for these centers are shown on the [Future Land Use Map](#).

Although neighborhoods will have some common features, each is unique. Recognizing this, the City envisions a public process where the needs of specific neighborhoods can be individually addressed. This process is described in the Public Participation Chapter and will focus on twelve planning areas. And, as described below, site-specific plans will be prepared for a few select other areas of the community. Managing these areas well will be critical to the success of this Comprehensive Plan and deserves extraordinary attention.



Shady sidewalks provide neighborhood character.

GL 20

GL20 Development maintains and improves neighborhood character and livability.

PL20.1 Require development in established neighborhoods to be of a type, scale, orientation, and design that maintains or improves the character, aesthetic quality, and livability of the neighborhood.

PL20.2 Unless necessary for historic preservation, prohibit conversion of housing in residential areas to commercial use; instead, support redevelopment and rehabilitation of older neighborhoods to bolster stability and allow home occupations (except convalescent care) that do not degrade neighborhood appearance or livability, nor create traffic, noise or pollution problems.

PL20.3 Allow elder care homes and seniors-only housing and encourage child care services everywhere except industrial areas; but limit hospice care to multi-family and commercial districts.

PL20.4 Support development and public improvements consistent with healthy and active lifestyles.

PL20.5 Prevent physical barriers from isolating and separating new developments from existing neighborhoods.

GL21 Neighborhood centers are the focal point of neighborhoods and villages.

PL21.1 Establish a neighborhood center at each village site, encourage development of the neighborhood centers shown on Future Land Use Map, and add additional centers when compatible with existing land uses and where they are more than one-half mile from other commercial areas.

PL21.2 Locate neighborhood centers along collector or arterial streets and within about 600 feet of a transit stop.

PL21.3 Support housing, a food store, a café or bakery, and a neighborhood park or civic green at all neighborhood centers. Allow churches, schools, and convenience businesses and services that cater primarily to neighborhood residents. Prohibit auto-oriented uses. Vary the specific size and composition of such centers for balance with surrounding uses. Where practical, focus commercial uses on civic greens or parks. Limit the size of commercial uses. (Note: A larger urban center is permitted in the Briggs Urban Village.)

PL21.4 Allow neighborhood center designs that are innovative and provide variety, but that ensure compatibility with adjoining uses. Consider appropriate phasing, scale, design and exterior materials, as well as glare, noise and traffic impacts when evaluating compatibility. Require that buildings primary access directly from street sidewalks and be oriented toward the neighborhood and any adjacent park or green. Require that signage be consistent with neighborhood character.

PL21.5 Locate streets and trails for non-arterial access to the neighborhood center.

GL22 Trees help maintain strong and healthy neighborhoods.

PL22.1 Use trees to foster a sense of neighborhood identity.

PL22.2 Identify, protect and maintain trees with historic significance or other value to the community or specific neighborhoods.

PL22.3 Encourage the use of appropriate fruit and nut trees to increase local food self-sufficiency.

Sub-area Planning

Much of this Plan applies to the entire Olympia community. However, this is a large area of over twenty-four square miles with tens of thousands of residents. Thus this Plan cannot address all of the details of our community. Twelve planning areas, including downtown, are to be established to provide that opportunity. In general, planning areas will be comparable to the scale of an elementary school service area with five to ten thousand residents. As described in the Public Participation and Partners chapter, this scale will provide the opportunity for interested parties to focus on furthering the community's plan for these areas. These sub-area efforts must be consistent with this Comprehensive Plan.

GL23

GL23 Each of the community's major neighborhoods has its own priorities.

PL23.1 In cooperation with residents, landowners, businesses, and other interested parties, establish priorities for the planning sub-areas. The specific area, content, and process for each sub-area is to be adapted to the needs and interests of each area. (See Goal 5 of Public Participation and Partners chapter.)

PL23.2 Create sub-area strategies that address provisions and priorities for community health, neighborhood centers and places of assembly, streets and paths, cultural resources, forestry, utilities, open space and parks.

PL23.3 Develop neighborhood and business community approaches to beautification that include activities in residential and commercial areas.

'Villages' and other Planned Developments

Sites for 'neighborhood villages,' one 'urban village,' and the older Evergreen Park planned unit development, each with a compatible mixture of single and multi-family housing and businesses, are designated within the urban area. These mixed-use projects are to provide for a coordinated,

compatible mixture of single and multi-family housing arranged around a readily-accessible neighborhood center. The locations and mix of land uses and the design of the street and trail system in these areas are to create an environment that encourages walking, biking and use of transit, while providing direct, pleasant routes for motorists. These 'villages' will foster efficient land use through compact, higher-density development with residential uses near bus stops and basic retail and support services.

The smaller 'neighborhood villages' will typically consist of single-family detached homes, townhouses and multi-family units, surrounding a small neighborhood center. The 'urban village' will be more diverse and intensely developed. The businesses of the urban village will serve a larger area and may include a supermarket, offices, and a broad array of predominantly neighborhood-oriented businesses and services. Both the neighborhood villages and urban villages are to be designed as coordinated, integrated projects with a compatible mix of land uses. Development phasing requirements will ensure that each project component and amenity is developed at the appropriate time. While these villages and the Evergreen Park PUD will have many characteristics in common, the design and composition of each project will vary in response to site conditions, location, market demand, available street and utility capacity, and the character of the surrounding neighborhood, and will evolve over time.

GL24

GL24 Mixed use developments, also known as "villages," are planned with a pedestrian orientation and a coordinated and balanced mix of land uses.

PL24.1 Require planned development sites shown on the Future Land Use Map to develop as coordinated, mixed-use projects.

PL24.2 Provide for any redevelopment or redesign of planned developments including the Evergreen Park Planned Unit Development to be consistent with the 'village vision' of this Plan.

PL24.3 Require 'master plans' for villages that encompass the entire site and specify the project phasing, street layout and design, lot arrangement, land uses, parks and open space, building orientation, environmental protection and neighborhood compatibility measures.

PL24.4 Provide for a compatible mix of housing in each village with pleasant living, shopping and working environment, pedestrian-oriented

character, well-located and sized open spaces, attractive well-connected streets and a balance of retail stores, offices, housing, and public uses.

PL24.5 Require a neighborhood center, a variety of housing, connected trails, prominent open spaces, wildlife habitat, and recreation areas in each village.

PL24.6 Require that villages retain the natural topography and major environmental features of the site and incorporate water bodies and stormwater ponds into the design to minimize environmental degradation.



Landscaping enhances a stormwater pond.

PL24.7 Locate parking lots at the rear or side of buildings, to avoid pedestrian interference and to minimize street frontage. Landscape any parking adjacent to streets and minimize parking within villages by reducing requirements and providing incentives for shared parking.

PL24.8 Require village integrity but provide flexibility for developers to respond to market conditions.

PL24.9 Limit each village to about 40 to 200 acres; require that at least 60% but allow no more than 75% of housing to be single-family units; and require at least 5% of the site be open space with at least one large usable open space for the public at the neighborhood center.

PL24.10 Require that 90% of village housing be within a quarter mile of

the neighborhood center and a transit stop.

PL24.11 Provide for a single "urban village" at the intersection of Henderson Boulevard and Yelm Highway; allowing up to 175,000 square feet of commercial floor area plus an additional 50,000 square feet if a larger grocery is included; and requiring that only 50% of the housing be single-family.

GL25

GL25 Local Thurston County food production is encouraged and supported to increase self-sufficiency, reduce environmental impact, promote health, and the humane treatment of animals, and support the local economy.

PL25.1 Actively partner with community organizations to provide education and information about the importance of local food systems.

PL25.2 Encourage home gardens as an alternative to maintaining a lawn.

PL25.3 Collaborate with community partners to ensure that everyone within Olympia is within biking or walking distance of a place to grow food.

PL25.4 Encourage for-profit gardening and farming in the community.

PL25.5 Purchase locally grown food when possible.

PL25.6 Allow food-producing gardens on rooftops, and offer incentives to include greenhouses for year-round food production.

PL25.7 Recognize the value of open space and other green spaces as areas of potential food production.

PL25.8 Work with community organizations to develop strategies, measure, and set goals for increasing local food production.

PL25.9 Work with local governments throughout the region to help protect existing agricultural lands and develop and promote a vibrant local food economy.

PL25.10 Partner with community organizations to help educate citizens who are interested in raising animals for food in the city. This might include information about protecting animals from predators, maintaining sanitary conditions, and treating animals humanely.

PL25.11 Educate and encourage citizens to purchase from local farms and small producers as an alternative to factory farms that may engage in inhumane treatment of animals.

Appendix A - Future Land Use Map Designations

The land use designations of the Future Land Use Map are described below and summarized in the Future Land Use Designations Table. Note that those indicated as symbols on the Future Land Use Map generally are not to exceed ten acres each.

Low-Density Neighborhoods. This designation provides for low-density residential development, primarily single-family detached housing and low-rise multi-family housing, in densities ranging from twelve units per acre to one unit per five acres depending on environmental sensitivity of the area. Where environmental constraints are significant, to achieve minimum densities extraordinary clustering may be allowed when combined with environmental protection. Barring environmental constraints, densities of at least four units per acre should be achieved. Supportive land uses and other types of housing, including accessory dwelling units, townhomes and small apartment buildings, may be permitted. Specific zoning and densities are to be based on the unique characteristics of each area with special attention to stormwater drainage and aquatic habitat. Medium Density Neighborhood Centers are allowed within Low Density Neighborhoods. Clustered development to provide future urbanization opportunities will be required where urban utilities are not readily available.

Medium-Density Neighborhoods. This designation provides for townhouses and multi-family residential densities ranging from thirteen to twenty-four units per acre. Specific zoning is to be based on proximity to bus routes and major streets, land use compatibility, and environmental constraints. Specific zoning will include minimum and maximum densities to ensure efficient use of developable land and to ensure provision of an adequate variety of types of housing to serve the community. Higher densities should be located close to major employment or commercial areas. Clustering may be permitted.

Mixed Residential. This designation requires a mixture of single and multifamily housing at densities ranging from seven to eighteen units per acre. Specific density ranges and mandatory mixes should be based on

land use compatibility and proximity to bus routes and major streets, while also ensuring availability of a variety and blending of housing types and choices.

Neighborhood Centers. This designation provides for neighborhood-oriented convenience businesses and a small park or other public space. Although the locations shown on the Future Land Use Map are approximate, these centers should be along major streets and generally near areas of higher residential densities. The exact location and mix of uses of the centers in these areas will be established at the time of development approval. In general they should be focused on serving nearby residents, be well integrated with adjacent land uses, and have excellent pedestrian and bicyclist access with minimal car parking.

Residential Mixed Use. To provide opportunities for people to live close to work, shopping, and services, this designation provides for high-density multifamily housing in multistory structures combined with limited commercial uses in parts of downtown, near the State Capitol Campus, and near urban corridors and other activity centers. This designation helps to achieve density goals, to create or maintain a desirable urban living environment for residents of these areas, and to ensure that new urban residential buildings incorporate features which encourage walking and add interest to the urban environment. The commercial uses are intended to help support the residential use of the area by providing retail and personal services within walking distance of the housing. Housing in these high amenity areas will contribute to community vitality, include well-designed buildings on continuous street edges, link one area with another, encourage pedestrian activity, and include visible public spaces that increase safety and decrease vandalism.

Planned Developments. This designation includes areas of mixed uses where specific 'master plans' are required prior to development. These master plans are prepared and proposed by one or a few parties and subject to review and confirmation by the City. This designation is intended to achieve more innovative designs than in conventional developments but which are also compatible with existing uses in the area. Innovative designs may include offering a wider variety of compatible housing types and densities, neighborhood convenience businesses, recreational uses, open space, trails and other amenities. Generally residential densities should range from seven to thirteen units per acre, but the specific mix of land uses will vary with the zoning, environment, and master plan of each site. In addition to a variety of housing types,

these areas may include neighborhood centers as described below. Each of the two planned developments along Yelm Highway may include a larger neighborhood-oriented shopping center with a supermarket. The planned development designation also includes retaining certain existing, and potentially new, manufactured housing parks in locations suitable for such developments. Two unique planned developments include substantial government office buildings and related uses - these are the Capitol Campus; and Evergreen Park, which includes the site of the Thurston County courthouse.

Professional Offices & Multifamily Housing. This designation accommodates a wide range of offices, services, limited retail uses specifically authorized by the applicable zoning district, and moderate-to-high density multifamily housing in structures as large as four stories.

Urban Corridors. This designation applies to certain areas in the vicinity of major arterial streets. Generally more intense commercial uses and larger structures should be located near the street edge with less intensive uses and smaller structures farther from the street to transition to adjacent designations. Particular 'nodes' or intersections may be more intensely developed. Opportunities to live, work, shop and recreate will be located within walking distance of these areas.

Urban Waterfront. Consistent with the State's Shoreline Management Act, this designation provides for a compatible mix of commercial, light industrial, limited heavy industrial, and multifamily residential uses along the waterfront.

Central Business District. This designation provides for a wide range of activities that make downtown Olympia the cultural, civic, commercial and employment heart of the community. A dense mix of housing, pedestrian-oriented land uses and design and proximity to transit make a convenient link between downtown, the State Capitol, the waterfront, and other activity centers in the region. The scale, height and bulk of development reinforce downtown Olympia's historic character, buildings, places and street layout.

General Commerce. This designation provides for commercial uses and activities which are heavily dependent on convenient vehicle access but which minimize adverse impact on the community, especially on adjacent properties having more restrictive development characteristics. The area should have safe and efficient access to major transportation routes. Additional "strip" development should be limited by filling in available

space in a way that accommodates and encourages pedestrian activity.

Auto Services. This designation conserves areas for concentrating land uses associated with automobile and other motor vehicle sales and services. Alternative uses such as professional offices may be permitted if compatible with the primary purpose of the designation.

Medical Services. This designation conserves areas in the vicinity of hospitals for concentrating medical services and facilities, associated uses, and moderate to high-density housing.

Light Industry. This designation provides for light industrial uses, such as assembly of products and warehousing, and compatible, complementary commercial uses.

Industry. This designation provides for heavy industrial development, such as manufacturing, transportation terminals and bulk storage, and complementary commercial uses in locations with few land use conflicts, minimal environmental constraints, and adequate freight access.

High-Density Neighborhoods Overlay: Multi-family residential, commercial and mixed use neighborhoods with densities of at least 25 dwelling units per acre for residential uses that are not re-using or redeveloping existing structures. New mixed-use developments include a combination of commercial floor area ratio and residential densities that are compatible with a high-density residential neighborhood. The height in these neighborhoods will be determined by zoning and based on the "Height and View Protection Goals and Policies."

Table: Future Land Use Designations

FUTURE LAND USE DESIGNATION	PRIMARY USE¹	RESIDENTIAL DENSITY²	BUILDING HEIGHTS³	ESTIMATED ACREAGE⁴	PERCENTAGE OF UGA⁵
Low-Density Neighborhoods (LDN)	Single-family Residential	Up to 12 units per acre	2 to 3 stories	11,000 11,495 ac.	71 68%
Medium-Density Neighborhoods (MDN)	Multi-family Residential	13 to 24 units per acre	Up to 3 stories	600 615 ac.	4%
Mixed Residential	Single & Multi-family	7 to 18 units per acre	Up to 4 stories	150 200 ac.	1%
Neighborhood Centers	Commercial	Variable	2 to 3 stories	Variable	N/A
Residential Mixed Use	Multi-family Residential	Not limited	3 to 5 stories	100 ac.	≤1%
Planned	Mixed Use	Residential areas: 7	Varies by site and	725 875 ac.	5%

Developments		to 13 units per acre	land use		
Professional Offices & Multifamily Housing	Mixed Use	Minimum 7 units per acre	3 to 4 stories	375 <u>405</u> ac.	2%
Urban Corridors	Commercial	Minimum 15 units per acre	3 to 6 stories	1,500 ac.	10%
Urban Waterfront	Mixed Uses	Minimum 15 units per acre	3 to 7 stories	200 <u>670</u> ac.	14 %
Central Business District	Commercial	Minimum 15 units per acre	Up to 8 stories	200 <u>195</u> ac.	1%
General Commerce	Commercial	Minimum 7 units per acre	3 to 6 stories	75ac.	<1%
Auto Services	Commercial	Not applicable	Up to 3 stories	125 <u>115</u> ac.	≤1%
Medical Services	Commercial	Minimum 7 units per acre	Up to 6 stories; plus taller hospitals	250 ac.	2 <u>1</u> %
Light Industry	Industry & Wholesaling	Not applicable	5 stories	100 <u>110</u> ac.	≤1%
Industry	Industrial	Not applicable	3 to 6 stories	75 <u>160</u> ac.	<1%

¹Primary Use is the anticipated use of the majority of building floor area in each category. Substantial other uses are likely.

²Residential Density is a general range for planning purposes and subject to variation based on site suitability. Specific allowed ranges should be established by development regulations.

³Building Heights is the approximate size of the taller buildings anticipated in each category. Specific height or stories limits should be established by development regulations.

⁴Estimated Acreage is a rough approximation based on the Future Land Use Map with recognition of the indistinct nature of the category boundaries.

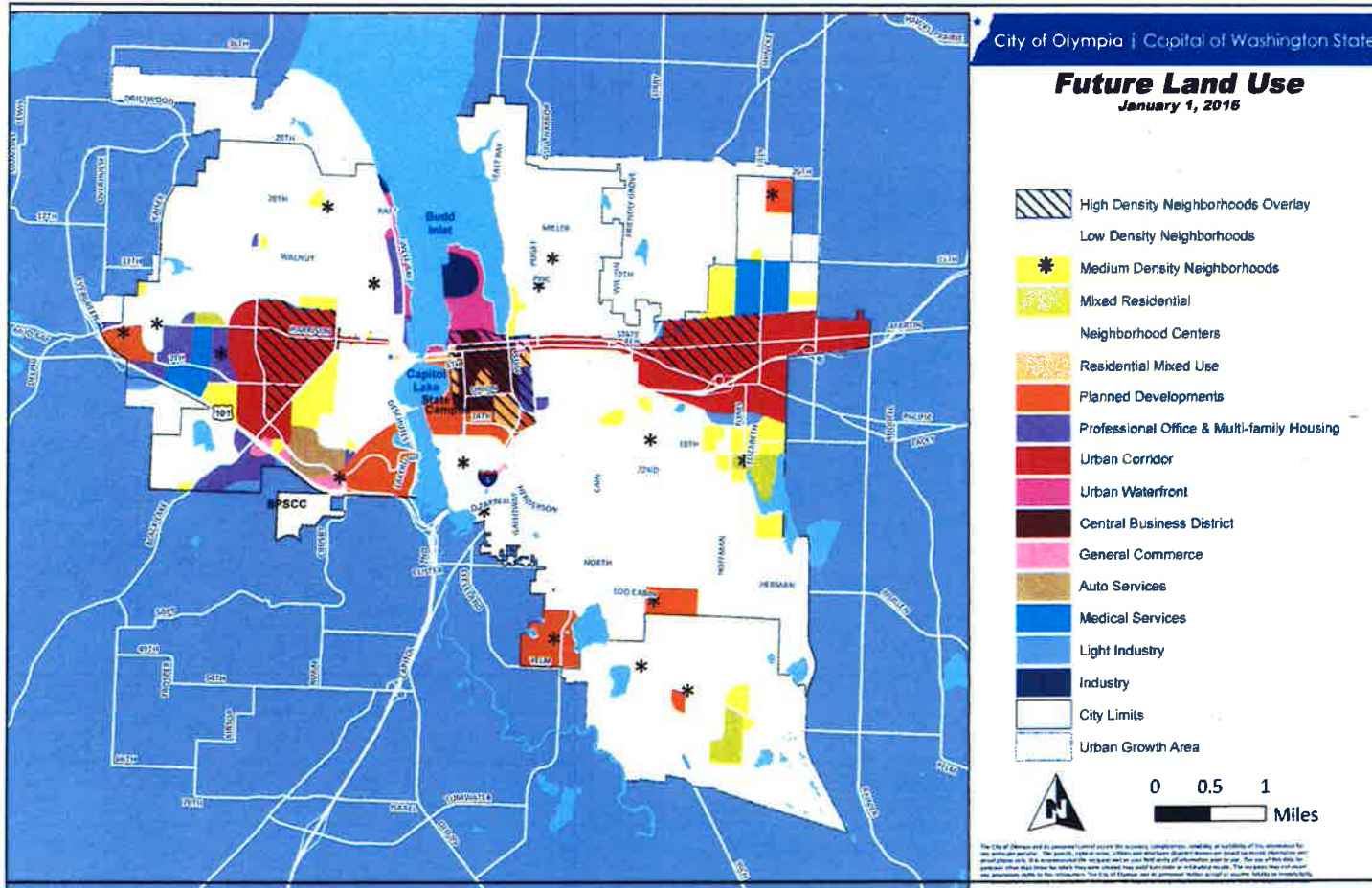
⁵Percentage of UGA is a rounded number provided for convenience based on the 'estimated acreage' and an assumption of approximately 24 square miles of land in the Urban Growth Area.

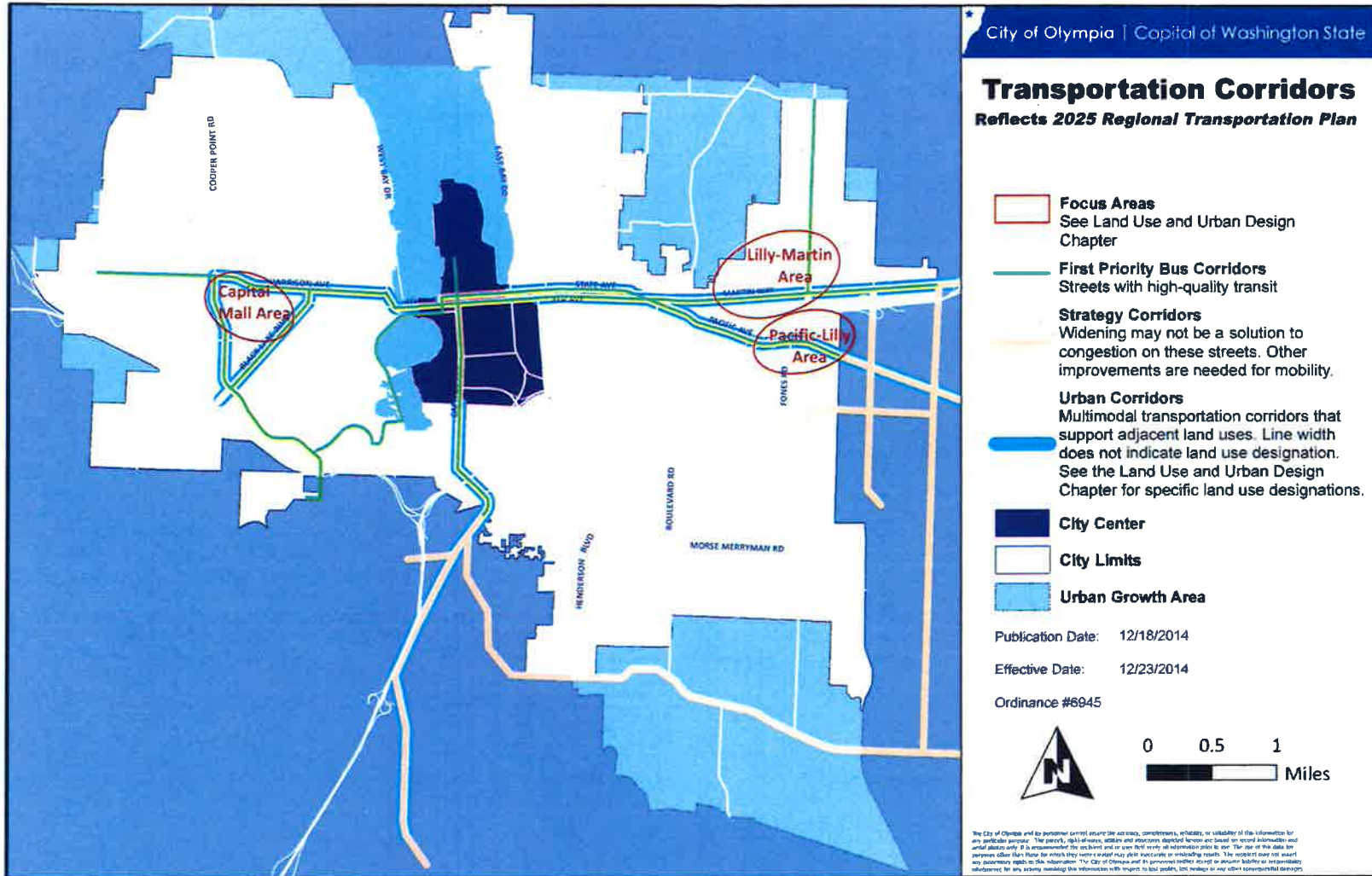
For More Information

- The Buildable Lands Report prepared for Thurston County by the staff of the Thurston Regional Planning Council helps Olympia to determine the quantity of land to provide for population and employment growth
- The Capitol Master Plan prepared by the Department of Enterprise Services describes the State's plans for certain lands within and

adjacent to downtown

- The Port of Olympia's Planning documents describe the Port's vision for the future of its lands within Olympia, as well as its role within Thurston County in general
- The Downtown Plan focuses on the city center and was formerly a part of this Comprehensive Plan. It is now a separate document adopted by the City Council
- The Urban Corridors Task Force Recommendations, adopted by Thurston Regional Planning Council in 2012, describes challenges and opportunities for the urban corridors of Olympia, Lacey and Tumwater
- The Kaiser Harrison Opportunity Area Plan describes a mixed use pedestrian and bicycle friendly subarea plan with a lifestyle retail center, multi-use trail, and a neighborhood park.



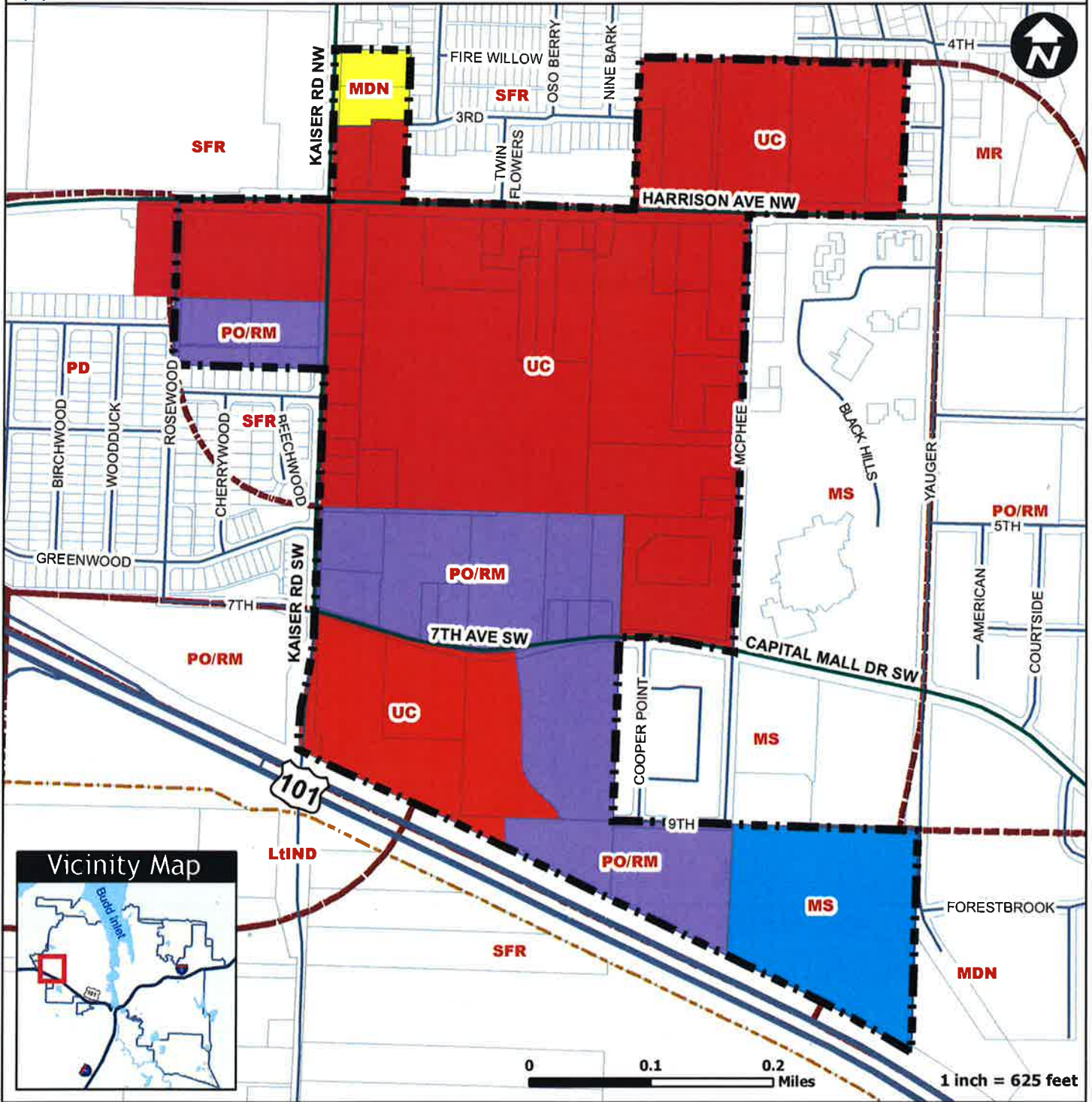




Kaiser-Harrison Opportunity Area

Proposed Comprehensive Plan - Future Land Use Map Designations

Date Published: 8/8/2016



Future Land Use Designation

- Urban Corridor
- Medium Density Neighborhood
- Medical Services
- Professional Office & Multi-family Housing
- Kaiser-Harrison Opportunity Area
- City Limits
- Urban Growth Area
- MS Future Land Use

This map is intended for 8.5x11" portrait printing.

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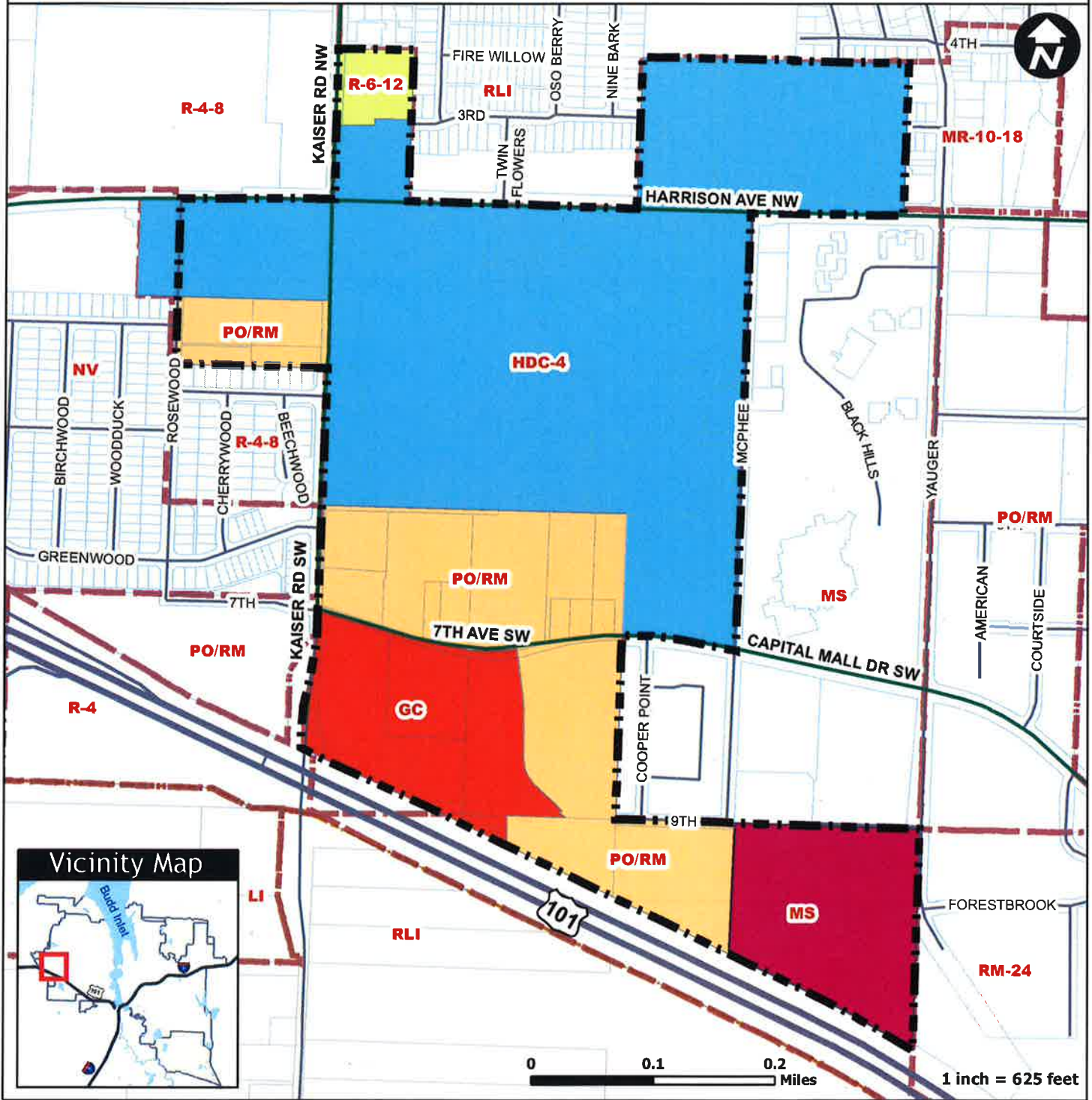


Kaiser-Harrison Opportunity Area

Proposed Zoning Districts

EXHIBIT C

Date Published: 8/9/2016



Proposed Zoning Districts

- General Commercial
- Medical Services
- High Density Corridor 4
- Professional Office & Multi-family
- Kaiser-Harrison Opportunity Area
- Residential 6-12
- City Limits
- Urban Growth Area
- Current Zoning

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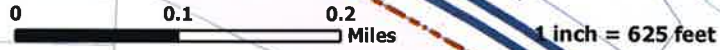
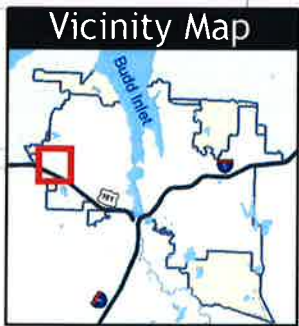
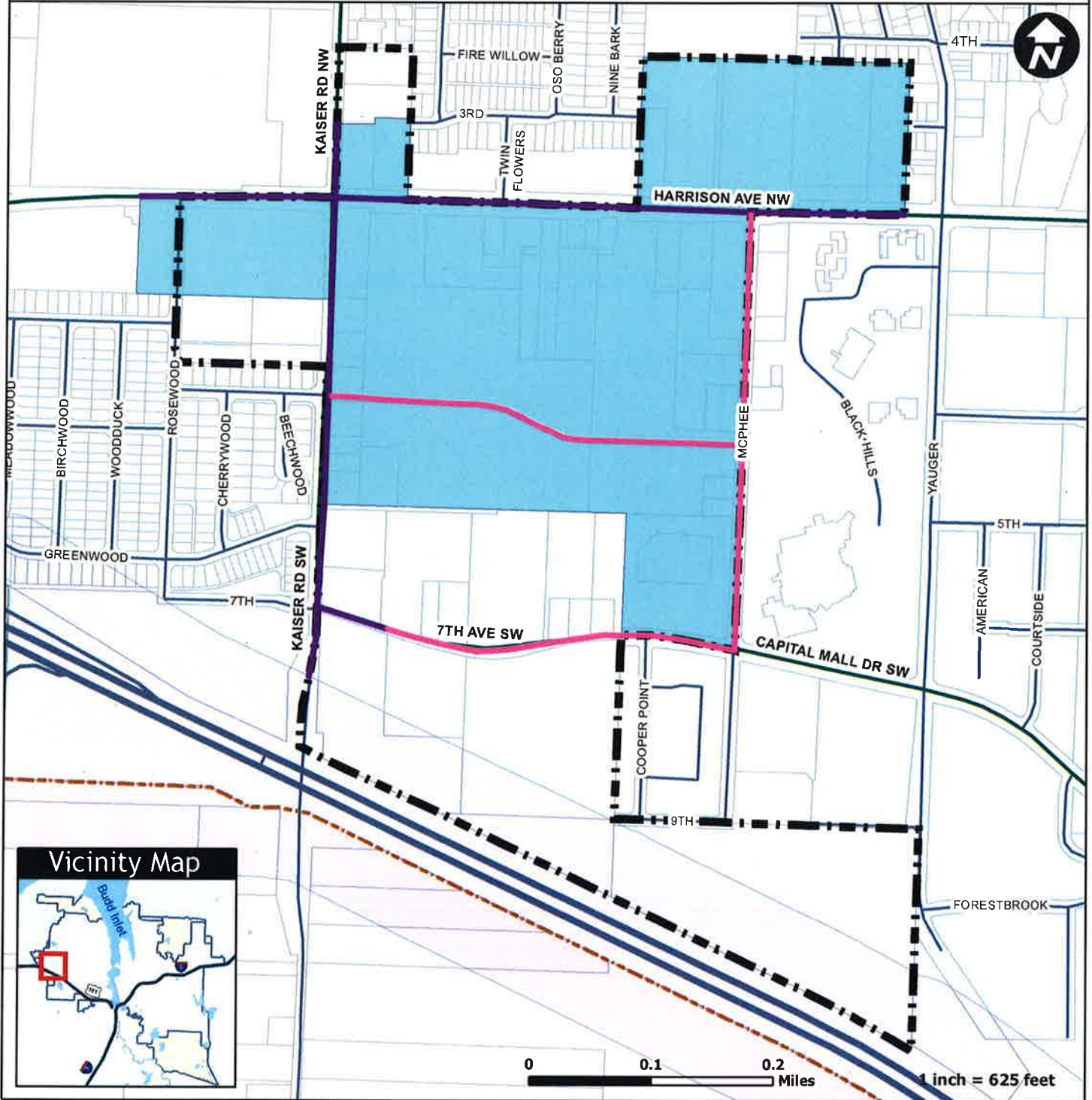


Kaiser-Harrison Opportunity Area





Design Review Map Amendments



EXHIBIT **D**

Date Published: 7/15/2016



Proposed Design Review Districts

-  High Density Corridor
-  Freeway Corridor
-  Pedestrian Street A
-  Pedestrian Street B
-  Kaiser-Harrison Opportunity Area

-  City Limits
-  Urban Growth Area

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Chapter 18.06 COMMERCIAL DISTRICTS

18.06.000 Chapter Contents

Sections:

- 18.06.020 Purposes.
- 18.06.040 Permitted, conditional, and prohibited uses.
- 18.06.060 Commercial districts' use standards.
- 18.06.080 Commercial districts' development standards (General).
- 18.06.100 Commercial districts' development standards (Specific).
- 18.06.120 Additional regulations.

18.06.020 Purposes

A. The general purposes of this Chapter are as follows:

1. To provide appropriate commercial areas for retail and service establishments, neighborhood convenience and office uses required by residents of the City in a manner consistent with the Comprehensive Plan.
2. To provide employment opportunities for existing and future residents of the City and those of adjacent communities.
3. To provide for land uses which meet the needs of and attract regional populations, in addition to local residents.
4. To provide adequate space to meet the needs of commercial development, including off-street parking and loading.
5. To protect commercial and adjacent areas from excessive noise, illumination, unsightliness, odor, smoke, and other objectionable influences.
6. To provide for an intensity of development and activity within commercial areas which will increase their vitality, facilitate mass transit, and make better use of available infrastructure.
7. To accommodate a balanced mix of commercial, residential, and recreational uses in commercial areas which will enable people to live, shop, work, and play within walking distance; thereby reducing dependence on motor vehicles and potentially reducing traffic congestion, energy consumption, and air pollution.
8. To establish standards for the development of commercial areas which will make them easily accessible and inviting for pedestrians, bicyclists, and transit riders, as well as motorists.
9. To establish guidelines for the design of commercial areas which will improve their appearance, function, and appeal.

B. The purpose of each commercial district is as follows:

1. Community Retail District (CMR).

This district is intended to:

- a. Permit businesses which offer the most frequently needed consumer goods and services, in districts of sufficient size to provide a relatively wide range of such goods and services.

- b. Provide for site development standards which will achieve the clustering of buildings and/or business establishments in such fashion as to create a safe, convenient and attractive pedestrian environment, including access for transit riders and bicyclists, as well as safe and convenient parking and access.
- c. Allow for clustering of commercial services along urban arterials adjacent to residential neighborhoods, in order to reduce the amount of vehicular travel required of the consumer to acquire such goods and services.
- d. Allow for residential and mixed-use projects to increase the opportunities for people to live, work, shop, and recreate within walking distance.
- e. Not locate new community retail districts closer than three-fourths of a vehicular mile from one another, or from any other district providing similar services or facilities.

2. Commercial Services - High Density District (CS-H).

This district is intended to:

- a. Allow limited commercial services that supplement or enhance activities on the capitol campus, not large-scale retail sales for regional markets.
- b. Allow high-density multifamily residences near the chief employment centers such as the Downtown Business District, the Capitol Campus, and the central waterfront.
- c. Be located where high land values and public necessity warrant this type of development.

3. Auto Services (AS) District.

This district is intended to:

- a. Encourage development of a regional center for auto sales and services and related uses.
- b. Discourage development incompatible with auto sales and services.

4. Urban Waterfront (UW) District.

This district is intended to:

- a. Integrate multiple land uses in the waterfront area of downtown and the West Bay in a way that improves the City's appeal and identity as the Capital City on Budd Inlet.
- b. Encourage high-amenity recreation, tourist-oriented, and commercial development which will enhance public access and use of the shoreline.
- c. Encourage development that protects views of Budd Inlet, the Olympics, Mt. Rainier, and the Capitol, and preserves a sense of openness on the waterfront.
- d. Encourage water-dependent and water-related development (as defined in the Shoreline Master Program for the Thurston Region) on shoreline properties and permit light manufacturing uses which support nearby industrial and marine related uses.
- e. Provide shoreline public access to significant numbers of the population, which is a major goal of the Shoreline Master Program for the Thurston Region. It is also the intent of this district to integrate the policies of the Shoreline Master Program for the Thurston Region into zoning designations applicable to waterfront properties. It is not the intent of this district, however, to make the restrictions of the Shoreline Master Program legally applicable outside the shoreline management zone.

5. Downtown Business District (DB).

This district is intended to:

- a. Encourage a wide range of activities which make downtown Olympia the cultural, civic, commercial, and employment heart of the community.
- b. Retain existing downtown housing and encourage additional development of a dense mix of urban housing which is located near jobs, shopping, and transit.
- c. Provide a full range of urban services, tourism, recreation, and entertainment activities to support downtown workers, residents, and visitors.
- d. Encourage pedestrian-oriented land uses and design, in order to link downtown activity to the Capitol Campus and the waterfront, and to the gateways to the City.
- e. Permit development of a scale, height, and bulk which reinforces downtown Olympia's historic character, buildings, places, and street layout. Modern architecture is appropriate if it is consistent with the City's urban design vision.

6. General Commercial District (GC).

This district is intended to:

- a. Provide for those commercial uses and activities which are heavily dependent on convenient vehicular access.
- b. Encourage the location of such uses on sites having safe and efficient access to major transportation routes.
- c. Discourage extension of "strip" development by filling in available space in areas where substantial auto-oriented commercial development already exists.
- d. Provide development standards which enhance efficient operation of these districts, and lead to more pedestrian-oriented development.
- e. Achieve minimum adverse impact on the community, especially on adjacent properties having more restrictive development characteristics.

7. Medical Service District (MS).

This district is intended to:

- a. Allow the development of major and full-service health care facilities to serve a regional population.
- b. Permit the clustering of interrelated and complementary health care facilities.
- c. Permit limited types of nonmedical uses which provide convenience services primarily to medical facility users and employees.
- d. Allow relatively high density housing near medical facilities to help meet the needs of the large number of people employed there.
- e. Allow developments which will provide maximum convenience to medical facility users and employees, and an environment conducive to the healing arts. Incompatible land uses and traffic generated by uses other than those providing medical and related services are to be avoided.

8. Neighborhood Retail District (NR).

This district is intended to:

- a. Permit small retail establishments which offer a limited range of goods within a residential neighborhood.
- b. Protect existing neighborhood retail districts and permit new establishments where local economic demand and appropriate design can assure compatibility with the neighborhood.
- c. Be located not less than one-half (1/2) mile from another neighborhood retail district or any other commercial district providing similar services or facilities.
- d. Have a maximum size for a Neighborhood Retail district of not more than one (1) acre.
- e. Limit the size, scale and expansion of such establishments in order to minimize traffic volumes and congestion, and other adverse impacts on the neighborhoods in which said establishments are located.
- f. Ensure that development in this district is characterized by small buildings, low traffic generation, considerable walk-in trade, quiet operations and little or no night activity.

9. Professional Office/residential Multifamily District (PO/RM).

This district is intended to:

- a. Provide a transitional area, buffering residential areas from more intensive commercial uses. Development within this district should be compatible with residential uses and generate low vehicular traffic characteristic of less intrusive uses.
- b. Provide for a compatible mix of office, moderate- to high-density residential, and small-scale commercial uses, in order to provide opportunities for people to live, work, and recreate in a pedestrian-oriented area.

10. High Density Corridor-1 (HDC-1).

This district is intended to:

- a. Provide for a compatible mix of office, moderate to high-density multifamily residential, and small-scale commercial uses.
- b. Ensure that residential and mixed-use projects are built within walking distance to transit.
- c. Establish a street edge that is as continuous as possible with buildings which are close to the street and which have multiple floors, distinctive windows facing the street, and entrances that are visible from the street.
- d. Ensure that projects are designed, using a neighborhood area design theme in order to blend with the historic buildings in the corridor and the adjacent neighborhoods.
- e. Create a safe, convenient, and attractive environment for pedestrians, transit riders and bicyclists, and which includes parking and convenient access for vehicles.

11. High Density Corridor-2 (HDC-2).

This district is intended to:

- a. Provide for a compatible mix of office, medium intensity commercial and moderate to high-density multifamily residential uses.
- b. Ensure that residential and mixed-use projects are built within walking distance to transit.

- c. Establish a street edge that is as continuous as possible with buildings which are close to the street and which have multiple floors, distinctive windows facing the street, and entrances that are visible from the street.
- d. Ensure that projects (buildings) are designed, using a neighborhood area design theme in order to blend with the historic buildings in the corridor and the adjacent neighborhoods.
- e. Create a safe, convenient, and attractive environment for pedestrians, transit riders, and bicyclists, and which includes parking and convenient access for vehicles.

12. High Density Corridor-3 (HDC-3).

This district is intended to:

- a. Provide for a compatible mix of medium to high-intensity commercial, offices, and moderate to high-density multifamily residential uses.
- b. Ensure that access to transit is a part of all new projects.
- c. Establish a street edge that is as continuous as possible with buildings which are close to the street and which have multiple floors, distinctive windows facing the street, and entrances that are visible from the street.
- d. Create a safe, convenient, and attractive environment for pedestrians, transit riders, and bicyclists, and which includes parking and access for vehicles.

13. High Density Corridor-4 (HDC-4).

This district is intended to:

- a. Provide for a compatible mix of high-intensity commercial, offices, and high-density multifamily residential uses.
- b. Transform these areas to commercial and residential activity centers, over time.
- c. Ensure that access to transit is a part of new projects.
- d. Establish a street edge that is as continuous as possible with buildings which are close to the street and which have multiple floors, distinctive windows facing the street, and entrances that are visible from the street.
- e. Create a safe, convenient, and attractive environment for pedestrians, transit riders, and bicyclists, and which includes parking and access for vehicles.

14. Urban Waterfront - Housing District.

This district is intended to:

- a. Provide for a neighborhood of residential housing with the option of limited retail/commercial/office or other uses able to locate in a street edge storefront configuration.
- b. Help meet downtown housing and sustainability density goals, through the use of land for housing in a location, and at a density, that makes the use of a car a choice and not a necessity.
- c. Contribute to downtown vitality.
- d. Result in well designed buildings on continuous urban street edges.

- e. Increase resident surveillance and all day use of public spaces to increase safety and decrease vandalism or other security problems.
- f. Help the City achieve land use, transportation, environment, and housing goals.

EXHIBIT E

18.06.040 TABLES: Permitted and Conditional Uses
TABLE 6.01

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
District-Wide Regulations	18.06.060(R)				18.06.060(F)(2)	18.06.060(HH)	18.06.060(F)(2)						18.130.020	
1. EATING & DRINKING ESTABLISHMENTS														
Drinking Establishments			P		P	P	P		C 18.06.060(P)		P	P	P	
Drinking Establishments - Existing		P 18.06.060(GG)				P								
Restaurants, with drive-in or drive-through			P <u>18.06.060(F)(3)</u>										P <u>18.06.060(F)(3)</u>	
Restaurants, with drive-in or			P				P 18.06.060					C	P	

EXHIBIT E

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
drive-through, existing							(U)							
Restaurants, without drive-in or drive-through	P 18.06.060(U)(3)	C	P	P 18.06.060(U)(2)	P	P	P 18.06.060(U)(1)	P	P	P	P	P	P	
District-Wide Regulations	18.06.060(R)				18.06.060(F)(2)	18.06.060(HH)	18.06.060(F)(2)							
2. INDUSTRIAL USES														
Industry, Heavy														
Industry, Light			C		P/C 18.06.060(N)									
On-Site Treatment & Storage Facilities for Hazardous					P 18.06.060(Q)									

EXHIBIT E

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
Waste														
Piers, Wharves, Landings					P									
Printing, Industrial			C		P/C 18.06.060 (N)									
Publishing		C	C		P		P		C	C				
Warehousing			P		P/C 18.06.060 (AA)		P							
Welding & Fabrication			C		P/C 18.06.060 (N)		P							
Wholesale Sales		C 18.06.060(BB)(3)	P		P/C 18.06.060 (BB)			P		P	18.06.060(BB)(2)			
Wholesale Products Incidental to			P		P	P						P	P	

EXHIBIT E

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
Retail Business														
District-Wide Regulations	18.06.060(R)				18.06.060(F)(2)	18.06.060(HH)	18.06.060(F)(2)							
3. OFFICE USES (See also SERVICES, HEALTH)														
Banks		P	P		P/C 18.06.060(D)(2)	P 18.06.060(D)(2)	P/C 18.06.060(D)(2)	P	P	P	P	P 18.06.060(D)(1)	P <u>18.06.060(F)(3)</u>	
Business Offices		P	P		P	P	P	P	P	P	P	P	P	
Government Offices		P	P		P	P	P	P	P	P	P	P	P	
District-Wide Regulations	18.06.060(R)				18.06.060(F)(2)	18.06.060(HH)	18.06.060(F)(2)							
4. RECREATION AND CULTURE														

EXHIBIT E

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
Art Galleries	P	P	P		P	P	P		P	P	P	P	P	
Auditoriums and Places of Assembly			P		P	P	P					P	P	
Boat Clubs					P	P								
Boating Storage Facilities					P			P						
Commercial Recreation		C	P		P	P	P	P		C	C	P	P	
Health Fitness Centers and Dance Studios	P	P 18.06.060(L)	P	P	P	P	P	P	P	P 18.06.060(L)	P 18.06.060(L)	P	P	
Libraries	C	C	C	C	P	P	P		P	C	P	P	P	18.04.060(V)
Marinas/Boat Launching Facilities					P 18.06.060(CC)	P								
Museums		C	P		P	P	P		P	C	C	P	P	18.04.060(L)

EXHIBIT E

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
														V)
Parks, Neighborhood	P	P	P	P	P	P	P		P	P	P	P	P	18.04.060(T)
Parks & Playgrounds, Other	P	P	P	P	P	P	P		P	P	P	P	P	18.04.060(T)
Theaters (Drive-in)			C											
Theaters (No drive-ins)			P		P	P	P				C	P	P	
District-Wide Regulations	18.06.060(R)				18.06.060(F)(2)	18.06.060(HH)	18.06.060(F)(2)							
5. RESIDENTIAL														
Apartments		P	P	P 18.06.060(T)	P	P	P		P	P	P	P	P	
Apartments	P	P	P	P	P	P	P		P	P	P	P	P	

EXHIBIT E

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
above ground floor in mixed use development				18.06.060(T)										
Boarding Houses		P	P	P 18.06.060(T)	P	P	P		P	P	P	P	P	
Co-Housing		P	P			P	P			P	P		P	
Duplexes	P	P	P	P 18.06.060(T)			P		P	P	P		P	
Fraternities, Dormitories		C	P	P 18.06.060(T)	P	P	P		P	C	P	P	P	
Group Homes (6 or less)	P	P	P 18.06.060(K)	P 18.06.060(T)	P	P	P 18.06.060(K)		P	P	P	P 18.06.060(K)	P 18.06.060(K)	18.04.060(K)
Group Homes (7 or more)	C	C	C 18.06.060	C 18.06.060	C	C	C 18.06.060		C	C	C	C 18.06.060	P 18.06.060	18.04.060(K)

EXHIBIT E

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
			0(K)	0(T)			(K)					(K)	60(K)	
Mobile or Manufactured Homes Park - Existing		C	C	C 18.06.06 0(T)						C			C	18.04.060(P)
Quarters for Night Watch person/Caretaker					P	P								
Retirement Homes		P	P	P 18.06.06 0(T)	P	P	P		P	P	P	P	P	
Single-Family Residences	P	P	P	P 18.06.06 0(T)			P		P	P	P	P	P	
Single Room Occupancy Units			C		P	P	P		P				C	
Townhouses	P	P	P	P 18.06.06		P	P		P	P	P	P	P	

EXHIBIT E

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
				0(T)										
Triplexes, Fourplexes, and Cottage Housing		P											P	
District-Wide Regulations	18.06.060(R)				18.06.060(F)(2)	18.06.060(HH)	18.06.060(F)(2)							
6. RETAIL SALES														
Apparel and Accessory Stores			P		P	P	P					P	P	
Boat Sales and Rentals			P		P	P	P	P					P	
Building Materials, Garden and Farm Supplies	P		P		P	P	P					P	P	
Commercial Greenhouses,	C	C 18.04.060(G)	C	C					C		P	P		18.04.060(G)

EXHIBIT E

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
Nurseries, Bulb Farms		G)												
Electric Vehicle Infrastructure	P	P	P	P	P 18.06.060 (W)	P 18.06.060 (W)	P 18.06.060 (W)	P	P	P	P	P	P	
Food Stores	P	P 18.06.060 (H)	P		P	P	P		P	P 18.08.060 (H)	P	P	P	
Furniture, Home Furnishings, and Appliances			P		P	P	P				P	P	P	
Gasoline Dispensing Facilities accessory to a permitted use	P 18.06.060 (W)(4)		P		P 18.06.060 (W)		P 18.06.060 (W)(2)	P				P 18.06.060 (W)	P 18.06.060 (W)	
Gasoline Dispensing Facility	P 18.06.060 (W)		P		P 18.06.060 (W)		P 18.06.060 (W)				P	P 18.06.060 (W)	P	

EXHIBIT E

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
accessory to a permitted use - Existing														
General Merchandise Stores	P	P 18.06.060(J)	P		P	P	P			P 18.06.060(J)	P	P	P	
Mobile, Manufactured, and Modular Housing Sales			P											
Motor Vehicle Sales			P				P	P					P	
Motor Vehicle Supply Stores			P		P	P	P	P			P	P	P	
Office Supplies and Equipment		P 18.06.060(DD)	P		P	P	P		P	P 18.06.060(DD)	P	P	P	18.06.060(CC)
Pharmacies and Medical Supply	P	P 18.06.060(J)	P	P	P	P	P		P	P 18.06.060(J)	P	P	P	18.06.060(DD)

EXHIBIT E

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
Stores		EE)								(EE)				
Specialty Stores	P 18.06.060(Y)(3)	P 18.06.060(Y)(4)	P	C 18.06.060(Y)(2)	P	P	P			P 18.06.060(Y)(4)	P	P 18.06.060(Y)(1)	P	
District-Wide Regulations	18.06.060(R)				18.06.060(F)(2)	18.06.060(HH)	18.06.060(F)(2)							
7. SERVICES, HEALTH														
Hospitals				P			P		P					
Nursing, Congregate Care, and Convalescence Homes	C	CP	C	P			C		C	C	C	P	P	18.04.060(S)
Offices, Medical		P	P	P	P	P	P	P	P	P	P	P	P	
Veterinary Offices/Clinics		P	P	P			P			P	P	P	P	
District-Wide	18.06.06				18.06.060	18.06.060	18.06.060							

EXHIBIT E

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
Regulations	0(R)				(F)(2)	(HH)	(F)(2)							
8. SERVICES, LODGING														
Bed & Breakfast Houses (1 guest room)	P	P 18.06.060(E)	P 18.06.060(E)	P 18.06.060(E)	P	P	P			P	P	P	P	18.04.060(L)(3)(c)
Bed & Breakfast Houses (2 to 5 guest rooms)	C	P 18.06.060(E)	P 18.06.060(E)	P 18.06.060(E)	P	P	P		C	P	P	P	P	18.04.060(L)(3)(c)
Hotels/Motels			P	C	P		P		P				P	
Lodging Houses		P	P	P	P		P		P	P	P	P	P	
Recreational Vehicle Parks			P										P	
District-Wide Regulations	18.06.060(R)				18.06.060(F)(2)	18.06.060(HH)	18.06.060(F)(2)							
9. SERVICES, PERSONAL														
Adult Day Care	P	P	P	P	P	P	P		P	P	P	P	P	18.04.060(L)(3)(c)

EXHIBIT E

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
Home														L)(3)(b)
Child Day Care Centers	C	P	P	P	P	P	P		P	P	C	P	P	18.04.060(D)
Crisis Intervention	C	P	C	P			P		C	P	C	C	C	18.04.060(I)
Family Child Care Homes	P	P	P	P	P	P	P		P	P	P	P	P	18.04.060(L)
Funeral Parlors and Mortuaries		C	P				P			C		P	P	
Laundries and Laundry Pick-up Agencies	P	P	P	P	P	P	P			P	P	P	P	18.06.060(O)
Personal Services	P	P	P	P	P	P	P	P	P	P	P	P	P	
District-Wide Regulations	18.06.060(R)				18.06.060(F)(2)	18.06.060(HH)	18.06.060(F)(2)							
10. SERVICES, MISCELLANEO														

EXHIBIT E

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
US														
Auto Rental Agencies			P		P	P	P	P			C	P	P	
Equipment Rental Services, Commercial			P		P		P				P	P	P	
Equipment Rental Services, Commercial - Existing		P 18.06.060(FF)												
Ministorage			P				P							
Printing, Commercial	P	P	P		P	P	P		P	P	P	P	P	
Public Facilities (see also Public Facilities, Essential on next page)	C	C	C	C	P	C	P	P	P	C	C	C	C	18.04.060(V)
Radio/T.V.		P	P		P	P	P		P	P	P	P	P	

EXHIBIT E

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
Studios														
Recycling Facilities	P	P	P	P	P		P		P	P	P	P	P	18.06.060(V)
School - Colleges and Business, Vocational or Trade Schools		C	P		P	P	P		P	C	C	C	P	18.06.060(X)
Service and Repair Shops			P				P	P				P	P	
Service Stations/Car Washes			P				P 18.06.060 (W)	P				P 18.06.060 (W)	P 18.06.060 (W)	
Service Stations/Car Washes - Existing			P		P 18.06.060 (W)		P 18.06.060 (W)				P	P 18.06.060 (W)	P 18.06.060 (W)	
Servicing of Personal	P	P	P		P	P	P			P	P	P	P	

EXHIBIT E

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
Apparel and Equipment														
Truck, Trailer, and Recreational Vehicle Rentals			P					P						
Workshops for Disabled People	C	C	C	C	P	C	P		C	C	C	C	C	18.04.060(R)
District-Wide Regulations	18.06.060(R)				18.06.060(F)(2)	18.06.060(HH)	18.06.060(F)(2)							
11. PUBLIC FACILITIES, ESSENTIAL														
Airports			C										C	18.06.060(G)
Inpatient Facilities		C	C	C 18.06.060(T)	C		C		C	C	C	P	P	18.06.060(G) 18.04.060(K)

EXHIBIT E

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
Jails			C		C		C		C				C	18.06.060(G)
Mental Health Facilities			C	C 18.06.060(T)	C		C						C	18.06.060(G) 18.04.060(K)
Other Correctional Facilities		C	C	C 18.06.060(T)	C	C	C		C	C	C	C	C	18.06.060(G)
Other facilities as designated by the Washington State Office of Financial Management, except prisons and solid waste handling facilities		C	C		C		C			C	C	C	C	18.06.060(G)

EXHIBIT E

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
Radio/TV and Other Communication Towers and Antennas	C	C	C	C	C	C	C	C	C	C	C	C	C	18.06.060(G) 18.44.100
Sewage Treatment Facilities	C	C	C	C	P		P		C	C	C	C	C	18.06.060(G) 18.04.060(X)
State Education Facilities		C	C		C		C		C	C	C	C	C	18.06.060(G) 18.06.060(X)
State or Regional Transportation Facilities	C	C	C	C	C	C	C		C	C	C	C	C	18.06.060(G)
District-Wide Regulations	18.06.060(R)				18.06.060(F)(2)	18.06.060(HH)	18.06.060(F)(2)							

EXHIBIT E

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
12. TEMPORARY USES														
Entertainment Events			P		P	P	P						P	
Off Site Contractor Offices	P	P	P	P	P	P	P	P	P	P	P	P	P	18.04.060(DD)
Emergency Housing	P	P	P	P	P			P	P	P	P	P	P	18.04.060(DD)
Fireworks, as determined by Fire Dept.			P		P	P	P				P	P	P	9.48.160
Mobile Sidewalk Vendors		P	P	P	P	P	P			P	P	P	P	
Parking Lot Sales			P		P	P	P	P			P	P	P	
Residences Rented for	P	P	P	P	P	P	P		P	P	P	P	P	18.04.060(DD)

EXHIBIT E

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
Social Event (6 or less in 1 year)														
Residences Rented for Social Event (7 or more in 1 year)	C	C	C	C	C	C	C		C	C	C	C	C	
Temporary Surface Parking Lot		P	P		P	P	P		P					
District-Wide Regulations	18.06.060(R)				18.06.060(F)(2)	18.06.060(HH)	18.06.060(F)(2)							
13. OTHER USES														
Accessory Structures/Uses														
Adult Oriented Businesses			P										P	18.06.060(B)

EXHIBIT E

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
Agriculture	P	P	P	P					P	P	P	P	P	
Animals	P	P	P	P	P	P	P		P	P	P	P	P	18.06.060(C)
Cemeteries	C	C	C	C					C	C	C		C	
Conference Center			P		P	P	P						P	
Fraternal Organizations		P	P		P	P	P		P/C 18.06.060(I)	P	P	P	P	
Gambling Establishments			C											
Garage/Yard/Rummage and Other Outdoor Sales	P	P	P	P	P	P	P		P	P	P	P	P	5.24
Home Occupations	P	P	P	P	P	P	P		P	P	P	P	P	18.04.060(L)
Parking Facility,		P	P		P	P	P			P	P	P	P	18.04.060(I)

EXHIBIT E

PERMITTED AND CONDITIONAL USES														
COMMERCIAL DISTRICT	NR	PO/RM	GC	MS	UW	UW-H	DB	AS	CSH	HDC-1	HDC-2	HDC-3	HDC-4	APPLICABLE REGULATIONS
Commercial							18.06.060 (S)					18.06.060 (S)		V)
Places of Worship	C	C	P	C	P	P	P		C	C	C	P	P	18.04.060(U)
Racing Pigeons	C	C	C	C					C	C	C	C	C	18.04.060(Y)
Satellite Earth Stations	P	P	P	P	P	P	P	P	P	P	P	P	P	18.44.100
Schools	C	C	P	C	C	C	C		C	C	C	P	P	18.04.060(DD)
Utility Facility	P/C	P/C	P/C	P/C	P/C	P/C	P/C	P/C	P/C	P/C	P/C	P/C	P/C	18.04.060(X)
Wireless Communications Facilities	P/C	P/C	P/C	P/C	P/C	P/C	P/C	P/C	P/C	P/C	P/C	P/C	P/C	18.44

LEGEND			
P = Permitted Use	PO/RM = Professional	GC = General Commercial	HDC-1=High Density Corridor-1

EXHIBIT E

LEGEND			
MS = Medical Services	Office/Residential Multifamily	UW = Urban Waterfront	HDC-2=High Density Corridor-2
DB = Downtown Business	AS=Auto Services	UW-H = Urban Waterfront-Housing	HDC-3=High Density Corridor-3
C = Conditional Use	NR = Neighborhood Retail	CSH = Commercial Services-High Density	HDC-4=High Density Corridor-4

18.06.040 Permitted, conditional and prohibited uses**A. PERMITTED AND CONDITIONAL USES.**

Table 6.01, Permitted and Conditional Uses, identifies land uses in the commercial districts which are permitted outright (P) or subject to a Conditional Use Permit (C). The applicable requirements for these uses and activities are identified by a number referencing the list of use regulations under Section 18.06.060, Use Standards. Numbers listed under the heading Applicable Regulations apply to the corresponding land use in all of the commercial districts. Regulations that pertain only to a specific use in a specific district are identified by a number in the space corresponding to that use and district. (Also see Section 18.06.080, Development Standards--General, and 18.06.100, Development Standards--Specific.)

B. PROHIBITED AND UNSPECIFIED USES.

Land uses which are not listed as permitted or conditional uses are prohibited unless authorized by the Director of Community Planning and Development (or the Hearing Examiner on appeal) consistent with Section 18.02.080, Interpretations. However, in no event shall secure community transition facilities be permitted.

18.06.060 Commercial districts' use standards**A. Accessory Uses and Structures.**

Only those uses that meet the definition of an Accessory Use as defined under this chapter will be permitted in the zones outlined in the tables of this chapter.

B. Adult Oriented Businesses.

1. Location. Adult oriented businesses may be permitted, but only if the following separation and distance conditions are met:

a. No adult oriented businesses shall be located closer than one thousand (1,000) feet to another such business whether such other business is located within or outside the city limits. Said distance shall be measured by following a straight line from the nearest point of public entry into the structure which will house the proposed adult facility to the nearest point of public entry into the structure housing another adult facility.

b. No adult oriented businesses may be located closer than two hundred fifty (250) feet from the nearest point of the boundary of a General Commercial (GC, or High Density Corridor-4 (HDC-4) district; PROVIDED, this restriction shall not apply to a proposed business with respect to a particular zone boundary when the proposed site of the business is separated from said boundary by an arterial street of at least four (4) travel lanes in width.

c. No adult oriented businesses shall be located closer than three hundred thirty (330) feet of any of the following uses whether such use is located within or outside the city limits:

i. Any residential use;

ii. Family child care home;

iii. Child day care center;

iv. Preschool facility; and

v. Nursery school;

vi. The point of ingress to or egress from any public trail identified in the city's Comprehensive Plan, Urban Trails, except when such point is separated from the proposed business by a four-lane or wider street arterial.

d. No adult oriented businesses shall be located closer than one thousand three hundred twenty (1,320) feet to any of the following uses whether such use is located within or outside the City limits:

- i. Public park;
- ii. Public or private primary or secondary schools, colleges and universities; and
- iii. Places of worship (e.g., church, temple or synagogue or other facility primarily devoted to the teaching or practice of religious beliefs);
- iv. Public library

e. Such distance shall be measured by following a straight line distance between the point of public entry into the structure housing the adult facility and:

- i. The nearest point on a property line of a public park; or
- ii. The nearest point of public entry to any residential use, public library, child day care home, child day care center, preschool, nursery school, public or private primary or secondary school, college, university, church, temple, or synagogue, or other facility primarily devoted to the teaching or practice of religious beliefs, or the nearest point on the perimeter of the area actually used in conjunction with any such use, whichever is closer.

For purposes of this ordinance, "actually used in conjunction with" means areas used for the primary and related structures, yards, parking lots, designated play areas and other areas used to determine site coverage under this code.

f. Waiver of Distance Requirements. The following procedures and criteria shall be adhered to with regard to a request for waiver of distance requirements:

- i. Distance waiver required. Any party proposing to locate an adult facility within less than the required distances from uses or zones as specified in this ordinance may do so only after obtaining a waiver therefor from the Hearing Examiner through a conditional use permit.
- ii. Waiver notice requirements. In addition to the notice requirements for conditional use permits, first class mailing notice shall be made to all parties within either distance set forth in subsections 18.06.060(B)(1)(c) and (d), depending upon the use in question. The applicant shall provide the names and addresses of all property owners and businesses within said distances from the proposed use.
- iii. Criteria for decision. The final decision on the request for waiver of distance shall be made by the Hearing Examiner, based on consideration of the following:
 - (a) The extent to which physical features would result in an effective separation in terms of visibility and access.
 - (b) Compatibility with adjacent and surrounding land uses.
 - (c) The availability or lack of alternative locations for the proposed use.
 - (d) Ability to avoid the adult facility by alternative vehicular and pedestrian routes.

2. Intervening Uses. Uses and zones specified in Subsection 18.06.060(B)(1)(c) and (d) shall not be allowed to locate within the specified distances of an adult oriented business. Any party proposing to locate such a use or zone within the specified distances of an adult facility is considered an intervening

use and may do so only after obtaining a distance waiver pursuant to the provisions of Subsection 18.06.060(B)(1)(f) of this code regarding waiver of distance requirements; provided, that notice requirements shall conform with the provisions of Section 18.78.020 of the Olympia Municipal Code; and provided further, that the owner seeking to expand a sensitive use specified in Subsections 18.06.060(B)(1)(c) or (d) into a separation area provided herein need not procure a waiver of distance requirement under (B)(1)(f) herein if such expansion is to be done on the same parcel on which the sensitive use is located and no new lots are thereby created.

3. Adult Oriented Businesses - Forbidden in Other Zones. The allowance of adult oriented businesses shall be limited to the General Commercial (GC, or High Density Corridor-4 (HDC-4) zones and such uses are forbidden in all other zones within the City of Olympia.

C. Animals. All Commercial Districts:

1. Quantity. No more than three (3) pets, such as dogs, cats, hens, and untraditional pets (e.g., potbelly pigs and rabbits), four (4) months of age or older, shall be permitted per dwelling unit. (Traditional pets are defined as a species of animals which can be house-broken, or walked on a leash, or are frequently, but not necessarily, housed within a residence and are neither obnoxious nor a public safety or health threat.)

2. Birds. Song birds or other traditional pet birds (e.g., parrots) are permitted. Fowl, such as roosters, ducks and geese, are prohibited. [NOTE: The keeping of racing and performing pigeons is permitted as a conditional use.]

3. Other Animals. Swine, other than potbelly pigs, and goats are prohibited. The keeping of other animals and pets, which are not specifically prohibited in this section is permitted, provided that:

a. There shall be no more than one (1) animal per acre, in addition to those animals/pets permitted in Subsection C.1 above; and

b. Such animals shall be confined within a suitably fenced area which shall be located no closer than fifty (50) feet from any property line; and

c. The keeping of such animals does not constitute a nuisance or hazard to the peace, health or welfare of the community in general and neighbors in particular.

D. Banks.

1. High Density Corridor-3 (HDC-3) Requirements. Banks which offer only drive-through service (i.e., which serve customers exclusively in or on their vehicles) are not permitted.

2. Urban Waterfront (UW) and Downtown Business (DB) District Requirements. Drive-through banks may be permitted with a conditional use permit if the proposed project meets the Street Edge Development Standards of the Pedestrian Streets Overlay District, Chapter 18.16 OMC. The proposed project may be exempted from the Pedestrian-Oriented Street Wall Requirement under the "Pedestrian Streets Overlay District Requirements" if it is found that:

a. The proposed design meets the intent of the Pedestrian Streets Overlay District, "Pedestrian Streets Overlay District Requirements"; and

b. The building site presents unusual conditions which require an alternative design to accomplish the intent of the Pedestrian Streets Overlay District, "Pedestrian Streets Overlay District Requirements."

E. Bed and Breakfast Houses. Professional Office/Residential Multifamily (PO/RM), High Density Corridor-1 (HDC-1), General Commercial (GC), High Density Corridor-4 (HDC-4), and Medical Services (MS) districts

requirements: All Bed and Breakfast Houses are subject to the Bed and Breakfast House requirements in residential districts, Section 18.04.060(L)(3)(c).

F. Drive-Through and Drive-In Uses.

1. High Density Corridor-3 (HDC-3) Requirements. Businesses which serve customers exclusively in their vehicles are prohibited. This includes uses such as drive-through laundry pick-up agencies, drive-through-only banks, and drive-through photo processing services. This does not include car washes. Restaurants are not permitted to have drive-up or drive-through facilities.
2. Downtown Business and Urban Waterfront (UW) Requirements. Drive-through and drive-in uses are prohibited as a primary or accessory use (exception: drive-through banks are a conditional use). Existing drive-in and drive-through restaurants permitted before January 1, 1994, are conforming uses. Such uses shall be treated the same as other allowed uses, consistent with applicable regulations or conditional use requirements. Other uses made nonconforming by this zoning ordinance are subject to the requirements of Chapter 18.37, Nonconforming Buildings and Uses.

3 Pedestrian Streets and Drive-Through or Drive-In Uses. Drive-through and drive-in uses are allowed on parcels that abut pedestrian oriented streets, as follows:

- a. A Streets: Drive-through or drive-in uses are permitted on parcels abutting Pedestrian Oriented A Streets when there is another building(s) or a designated pedestrian plaza or other gathering space located between the drive-through or drive-in building and the street. In the event a pedestrian plaza or gathering space is located between the building and an "A" Street, provisions to prevent vehicles from entering the plaza or gathering space shall be provided (e.g. curb and a landscaped area, bollards, low masonry wall).
- b. B Streets: Drive-through lanes are prohibited between the pedestrian oriented street and the building. Drive-through lanes may be located to the side or rear of the building when designed for the safety of pedestrians or bicyclists on the sidewalk or other internal designated routes for pedestrians and/or bicyclists.

G. Public Facilities, Essential.

1. Regulations applicable to all commercial zoning districts. Essential public facilities are subject to the procedures and conditions listed in Section 18.04.060(W), as well as any other applicable provisions of this Title.
2. Community Retail District (CMR) and Professional Office/Residential Multifamily District (PO/RM) Regulations. "Other facilities as designated by the Washington State Office of Financial Management, except prisons and solid waste handling facilities" (see Table 6.01 above) shall be limited to office uses.
3. General Commercial District (GC), High Density Corridor-4 (HDC-4), Urban Waterfront (UW), and Downtown Business District (DB) Regulations. "Other facilities as designated by the Washington State Office of Financial Management, except prisons and solid waste handling facilities" (see Table 6.01 above) shall be limited to those office uses, industrial uses, recreation and culture uses otherwise allowed in these districts. In addition to the requirements for Essential Public Facilities, these uses shall meet all other applicable regulations of this Chapter and Title.

H. Food Stores. Professional Office/Residential Multifamily District (PO/RM), and High Density Corridor-1 (HDC-1) Requirements: Food stores are allowed up to a maximum size of five thousand (5,000) square feet of gross floor area. EXCEPTION: In the PO/RM area west of Yauger Road adjacent to Harrison/Mud Bay Road, maximum gross floor area shall be ten thousand (10,000) square feet.

I. Fraternal Organizations. Fraternal organizations are permitted within the CSH zone district under the condition that if alcohol is served, the use shall be subject to all requirements governing nightclubs, taverns and lounges (see Section 18.06.060(P) Nightclubs and Taverns, below).

J. General Merchandise Stores. Professional Office/Residential Multifamily District (PO/RM), and High Density Corridor-1 (HDC-1) Requirements: General Merchandise stores shall have a maximum size of five thousand (5,000) square feet of gross floor area. EXCEPTION: In the PO/RM area west of Yauger Road adjacent to Harrison/Mud Bay Road, maximum gross floor area shall be ten thousand (10,000) square feet.

K. Group Homes.

1. General requirements for group homes are identified in subsection 18.04.060(K).
2. Downtown Business District (DB) Requirements. There is no minimum lot size for group homes with up to twenty (20) unrelated residents, exclusive of on-site operators.
3. General Commercial District (GC), High Density Corridor-3 (HDC-3), High Density Corridor-4 (HDC-4), and Medical Services District (MS) Requirements. The Hearing Examiner may relax the minimum lot size standard in paragraph 18.04.060(K)(3) where the characteristics of the home so warrant.

L. Health Fitness Centers and Dance Studios. Professional Office/Residential Multifamily (PO/RM), High Density Corridor-1 (HDC-1), and High Density Corridor-2 (HDC-2) District Requirements: These uses shall have a maximum size of five thousand (5,000) square feet of gross floor area. EXCEPTION: In the PO/RM area west of Yauger Road adjacent to Harrison/Mud Bay Road, maximum gross floor area shall be ten thousand (10,000) square feet.

M. Industry, Heavy. CW - 1 Zone District: Manufacturing, compounding, processing, treatment or assembly of products--except those which cause excessive danger or offense--is permitted within this district. Heavy industrial uses are not permitted south of Corky Street.

N. Industry, Light.

1. Urban Waterfront (UW) Requirements. Light industry is a permitted use east of Washington Street. It is a conditional use west of Washington Street.

O. Laundry and Laundry Pick-Up Agency. High Density Corridor-3 (HDC-3) Requirements: No drive-through facilities are allowed for drop-off or pick-up of laundry.

P. Nightclubs and Taverns. Commercial Services - High Density (CS-H) District Requirements: Night clubs, taverns and lounges are a conditional use, subject to the following conditions:

1. Setback. No building shall be located closer than sixty (60) feet from a property line abutting a residential use.
2. Noise Insulation. The building shall be of sound-reducing construction that will assure compliance with the sound emission requirements of Section 18.40.080, Property Protection Standards.
3. Loudspeakers. There shall be no outside loudspeakers.

Q. On-Site Treatment and Storage Facilities for Hazardous Waste. Urban Waterfront (UW) District Requirements: These facilities are allowed only as an accessory use, subject to siting criteria pursuant to Chapter 70.105 RCW.

R. Operating Hours - Neighborhood Retail (NR) District. Operating hours for businesses in the NR District shall be limited to the hours between 6:00 a.m. and 11:00 p.m. A later opening time and/or an earlier closing time may be required if necessary to assure compatibility with the adjacent residential neighborhood.

S. Parking Facilities and Garages.

1. High Density Corridor-3 (HDC-3) Requirements. Parking lots established as separate, primary uses are a conditional use. The proposed parking lot shall exclusively serve specifically identified uses in or adjacent to the district to accommodate shared employee or customer parking or off-site employee parking. The uses served by the lot may change over time.

2. Downtown Business District (DB) Requirements. Public plazas, temporary surface parking lots (See OMC 18.04.060(EE)(1)(h)), or structured parking is permitted. Parking lots not associated with a permitted or conditional use are prohibited. All existing parking lots permitted before January 1, 1994, are conforming uses. Such lots shall be treated the same as other allowed uses, consistent with applicable regulations. Other uses made nonconforming by this zoning ordinance are subject to the requirements of Chapter 18.37, Nonconforming Buildings and Uses.

T. Residential Restrictions in the Medical Services District (MS). Residential uses may not be constructed within six hundred (600) feet of Lilly Road except above the ground floor in mixed use buildings.

U. Restaurants.

1. Downtown Business (DB) District Requirements. New drive-in and drive-through restaurants are not permitted. All existing drive-in and drive-through restaurants permitted before January 1, 1994, are conforming uses. Such uses shall be treated the same as other allowed uses, consistent with applicable regulations. Other uses made nonconforming by this zoning ordinance are subject to the requirements of Chapter 18.37, Nonconforming Buildings and Uses.

2. Medical Services (MS) District Requirements. Restaurants may be allowed as a conditional use where it can be demonstrated that the medical community or the consumers of medical services are clearly and primarily benefitted by the convenience of the facilities.

3. Restaurants in the Neighborhood Retail District shall be limited to one thousand (1000) square feet of gross floor area and shall not require nor include a Type I Hood as defined in the Uniform Mechanical Code.

V. Recycling Facilities. Only Type I Recycling Facilities are allowed in the following districts: Neighborhood Retail (NR), Community Retail (CMR), Professional Office/Residential Multifamily (PO/RM), Medical Services (MS), Urban Waterfront (UW), High Density Corridor-1 (HDC-1), High Density Corridor-2 (HDC-2), High Density Corridor-3 (HDC-3), High Density Corridor-4 (HDC-4), and Commercial Services-High Density (CS-H).

W. Electric Vehicle Infrastructure, Service Stations and Car Washes.

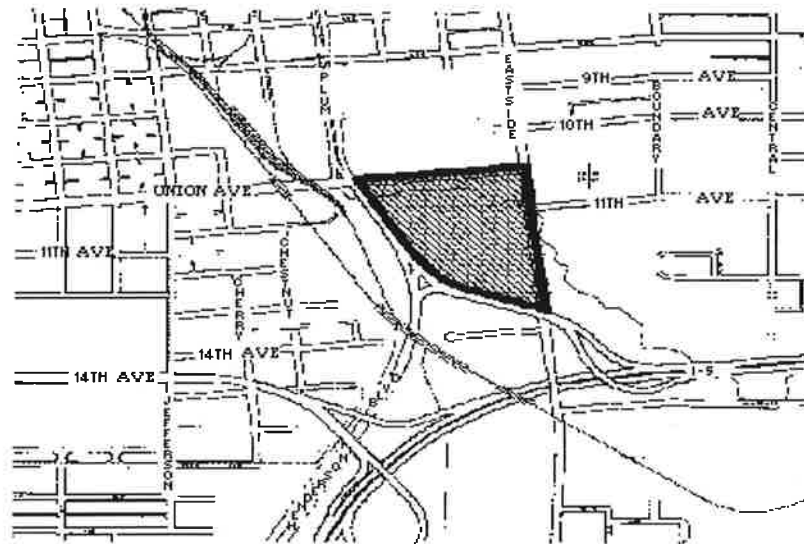
1. High Density Corridor-3 (HDC-3) District Requirements. Car washes are permitted only in conjunction with a service station.

2. High Density Corridor-3 (HDC-3) and High Density Corridor-4 (HDC-4) District Requirement. Service stations and car washes are not permitted to locate on corner properties at intersections. Services stations and car washes vested before January 20, 2001 are conforming uses.

3. Downtown Business District (DB) Requirements.

a. Service stations, car washes and accessory uses are not permitted, except on those properties located south of Union Street and east of Plum/Henderson (see Figure 6-1) or where otherwise listed as allowed under this section. This also applies to gasoline dispensing facilities accessory to a permitted use.

- b. All existing service stations permitted before January 1, 1994, are conforming uses. Such uses shall be treated the same as other allowed uses, consistent with applicable regulations.
- c. Detached drive-thru restaurants of less than 200 square feet without Type I hoods are permitted as an accessory use to service stations in this District.
- d. Electric Vehicle Infrastructure is permitted as an accessory use in the Downtown Business District if it meets the criteria set forth in OMC 18.04.060GG.



SERVICE STATIONS ALLOWED

FIGURE 6-1

Other uses made nonconforming by this zoning ordinance are subject to the requirements of Chapter 18.37, Nonconforming Buildings and Uses.

4. Urban Waterfront (UW) Requirements.

- a. New service stations and car washes are not permitted. This also applies to gasoline dispensing facilities accessory to a permitted use. EXCEPTION: gasoline dispensing facilities accessory to a marina or boat club are permitted.
- b. All existing service stations and car washes permitted before January 1, 1994, are conforming uses. Such uses shall be treated the same as other allowed uses, consistent with applicable regulations. Other uses made nonconforming by this zoning ordinance are subject to the requirements of Chapter 18.37, Nonconforming Buildings and Uses.
- c. Electric Vehicle Infrastructure is permitted as an accessory use in the Urban Waterfront (UW) District if it meets the criteria set forth in OMC 18.04.060GG.

5. Neighborhood Retail District (NR) and Community Retail (CMR) District Requirements. State or regional transportation facilities shall be linear facilities only, such as roads or railroads.

- a. Underground petroleum storage tanks are prohibited within the Allison Springs aquifer recharge area.

b. No more than four (4) gas dispensing devices serving a maximum of eight (8) vehicles at one time are permitted at convenience stores/gas stations in the CMR District.

c. No more than two (2) gas dispensing devices serving a maximum of four (4) vehicles at one time are permitted at convenience stores in the NR District.

6. Urban Waterfront Housing (UW-H) Requirements.

a. Electric Vehicle Infrastructure is permitted as an accessory use in the Urban Waterfront Housing (UW-H) District if it meets the criteria set forth in OMC 18.04.060GG.

X. School - Colleges and Commercial, Business or Trade Schools. The following requirements apply to all colleges and commercial, business or trade schools requiring a Conditional Use Permit hereafter erected, established or relocated.

1. Lot Size. In addition to complying with the minimum lot size requirements of the zoning district in which located, the minimum lot area of a school in excess of four (4) students shall be based upon a determination made by the Hearing Examiner.

2. Setbacks. Setbacks and screening shall be sufficient to protect neighboring uses.

3. Traffic. The Hearing Examiner shall set such conditions as may be necessary to limit traffic impacts to levels that will be compatible with the neighborhood. If the traffic to be generated cannot be adequately mitigated without adverse impacts, the permit shall be denied.

Y. Specialty Stores.

1. High Density Corridor-3 (HDC-3) District Requirements. No drive-through facilities are allowed for retail uses, such as a pick-up window for photo processing.

2. Medical Services (MS) District Requirements. Retail developments such as florists, gift shops and the like may be allowed as a conditional use where it can be demonstrated that the medical community or the consumers of medical services are clearly and primarily benefitted by the convenience of such retail facilities.

3. Neighborhood Retail (NR) District Requirements. Specialty stores are limited to those selling such items as gifts, antiques, variety goods, light hardware, hobby supplies, garden supplies, reading materials and other small items used primarily in a private home.

4. Professional Office/Residential Multifamily District (PO/RM), and High Density Corridor-1 (HDC-1) Requirements. Specialty stores shall have a maximum gross floor area of five thousand (5,000) square feet. EXCEPTION: In the PO/RM area west of Yauger Road adjacent to Harrison/Mud Bay Road, maximum gross floor area shall be ten thousand (10,000) square feet.

Z. Temporary Uses.

1. Intent. Certain uses, when active for a limited period of time and when properly regulated, can be compatible, or otherwise limited in impact to neighboring properties and the general community. In accord with this intent, no temporary use shall be allowed unless a temporary use permit is approved by the City as prescribed by this section. Each separately proposed activity or use shall require a separate permit and pay the fee required by OMC 4.40.010(A).

2. General Standards. Temporary uses are subject to the following regulations:

- a. Temporary uses not listed in the use table of this chapter may be authorized by the applicable approval authority, provided such temporary uses are similar to and no more intensive than other temporary uses permitted in the district in which the subject property is located.
 - b. The applicable approval authority may apply additional conditions to any temporary use permit in order to:
 - i. Ensure compliance with this chapter;
 - ii. Ensure that such use is not detrimental to neighboring properties and the community as a whole; and
 - iii. Ensure compliance with the Building Code.
 - c. Within three (3) days after termination of the temporary use permit, such use shall be abated and all structures, signs and evidence of such use removed. The Director may require a financial surety be posted by the applicant upon application to defray the costs of cleanup and repair of the property should the permittee fail to do so. The property owner is responsible for such abatement action and costs should the permittee fail to properly clean and repair the property.
 - d. Temporary use permits not exercised within thirty (30) days of issuance shall be null and void.
 - e. Unless otherwise stated in this section temporary use permits are valid from the date of issuance for ninety (90) consecutive days per calendar year.
 - f. Unless otherwise stated in this section no more than two (2) temporary use permits will be issued for any specific site per calendar year.
 - g. Nothing in this section shall exempt the applicant from obtaining all necessary applicable permits from all other agencies having jurisdiction.
 - h. Hours of operation, including the use of generators and lot lighting, excluding security lighting, shall be limited to 7:00 a.m. to 10:00 p.m. unless otherwise specified in writing by the Planning Director or his designee. Security lighting shall be shielded to prevent light spillage onto adjacent properties.
3. Specific Temporary Use Standards. The following temporary uses are permitted in commercial districts and the Evergreen Park PUD, subject to the following regulations:
- a. Entertainment Events to include: circuses, carnivals and similar transient amusement enterprises, limited to operation of not more than twice each year, and not more than ten (10) consecutive days per event per site in any one (1) calendar year.
 - b. Off-site contractor's Offices (including trailers and mobile homes) and storage yards associated with an active construction project, not to exceed one (1) year in duration.
 - c. Mobile Vendors.
 - i. Temporary use permits for mobile vendors are valid for one (1) year from the date of issuance.
 - ii. Approval from the property owner, or underlying property owner if located in a right-of-way, is required.
 - iii. Mobile vendors located within the sidewalk right-of-way must comply with the following rules:

- (a). Only one mobile sidewalk vendor shall be permitted per block face.
 - (b). Public sidewalks used by mobile vendors shall have a minimum width of eight (8) feet.
 - (c). In no instance shall the clear walking area around a sidewalk vendor be less than forty-eight (48) inches. The clear walking area around a sidewalk vendor must be at least six (6) feet if within the downtown "Pedestrian Walking Lane" area delineated in OMC 9.16.180(B), Figure 1.
 - (d). The maximum length of space occupied by a mobile sidewalk vendor and equipment is eight (8) feet.
 - (e). Mobile sidewalk vendor stands must be readily movable at all times.
 - (f). Mobile sidewalk vendors shall locate their stands at the back of the sidewalk away from curb.
 - (g). All locations shall be approved on a first-come, first-serve basis.
 - (h). Mobile sidewalk vendors shall sign a Hold Harmless Agreement with the City of Olympia.
- d. Parking lot and other outdoor sales of merchandise and/or services unrelated to the primary use of the property must comply with the following:
- i. Merchandise displays may only occupy parking stalls which are in excess of city parking requirements.
 - ii. There shall be no obstruction of emergency exits, Fire Lanes or other Emergency apparatus.
 - iii. Sales areas shall be maintained in an attractive and trash-free manner.
 - iv. Sales areas shall not substantially alter the existing circulation pattern of the site.
- e. Temporary, commercial wireless communications facilities, for the purposes of providing coverage of a special event such as news coverage or sporting event. Such facilities must comply with all federal and state requirements. Temporary wireless communications facilities may be exempt from the provisions of Chapter 18.44 up to one week after the duration of the special event.
- f. Temporary surface parking lots on previously developed property are allowed subject to approval by the Director, and are limited to a one time permit valid for two years. A one year extension may be granted by the Director if a complete Land Use Application has been submitted for review. All applications must provide a complete Site Plan and comply with the following:
- i. A twenty by twenty (20'x20') foot paved surface at all approved points of ingress/egress.
 - ii. A dust-free surface.
 - iii. An erosion control and stormwater containment plan.

- iv. Clear designation of parking spaces and drive aisles consistent with OMC 18.38.220 with striping and/or parking blocks. To prevent obstruction of public rights-of-way wheel blocks must be provided at the perimeter of the site.
- v. No new or additional points of access.
- vi. Provision for an enforceable compliance and closure agreement.
- vii. May not be established on Pedestrian "A" streets per OMC 18.16.080(H).

4. Violations. At any time a temporary use is operated in violation of required conditions of this section or of the permit (Subsection 18.06.060(Z)(2)(b)) or otherwise found to constitute a nuisance, the City may take appropriate enforcement action including the process set forth at OMC 18.73.010.

AA. Warehousing.

1. Urban Waterfront (UW) District Requirements:

- a. East of Adams Street and north of Olympia Avenue.
 - i. Warehousing is a permitted use.
- b. Elsewhere in the District:
 - i. Warehouses are prohibited except when part of a larger project and included within the proposed building.
 - ii. All existing warehouses permitted before January 1, 1994, are conforming uses. Such warehouses shall be treated the same as other allowed uses, consistent with applicable regulations. Other uses made nonconforming by this zoning ordinance are subject to the requirements of Chapter 18.37, Nonconforming Buildings and Uses.

BB. Wholesale Sales. The following Conditional Use Permit restrictions apply to wholesale uses:

1. Urban Waterfront (UW) District Requirements. Wholesale sales are a permitted use in those portions of the UW District which are not within the Pedestrian Streets Overlay District (see Chapter 18.16). In those portions of the UW District which are within the Pedestrian Streets Overlay District, wholesale sales may be allowed as a conditional use if the proposed project meets the Street Edge Development Standards of the Pedestrian Streets Overlay District. The proposed project may be exempted from the Pedestrian-Oriented Street Wall Requirement if:

- a. The proposed design meets the intent of the Chapter; and
- b. The building site presents unusual conditions which require an alternative design to accomplish the intent.

2. Commercial Services - High Density (CS-H) District Requirements. Permitted uses include those which offer specialized products at wholesale to other uses permitted in this district, including, but not limited to, office machine sales and repair services, and office supply sales.

CC. Marinas. Urban Waterfront (UW) District Requirements: As an accessory use, marinas may provide sites for recreational vehicles for users of the marina, at a rate of up to 1.5 sites per 100 mooring slips. These sites shall be provided with hook-ups for water, sewer, and electricity. Users of these sites shall be limited to two (2) weeks occupancy per year.

DD. Office Supplies and Equipment. High Density Corridor-1 (HDC-1), and Professional Office/Residential Multifamily District (PO/RM) Requirements. Office supplies and equipment stores shall have a maximum gross floor area of five thousand (5,000) square feet. EXCEPTION: In the PO/RM area west of Yauger Road adjacent to Harrison/Mud Bay Road, maximum gross floor area shall be ten thousand (10,000) square feet.

EE. Pharmacies and Medical Supply Stores. High Density Corridor-1 (HDC-1), and Professional Office/Residential Multifamily District (PO/RM) Requirements. Pharmacies and medical supply stores shall have a maximum gross floor area of five thousand (5,000) square feet. EXCEPTION: In the PO/RM area west of Yauger Road adjacent to Harrison/Mud Bay Road, maximum gross floor area shall be ten thousand (10,000) square feet.

FF. Equipment Rental Services. Existing equipment rental services legally established as of July 2001 are conforming uses in the PO/RM District and shall be treated the same as other allowed uses, consistent with applicable regulations. In the PO/RM area west of Yauger Road adjacent to Harrison Mud Bay Road, maximum gross floor area shall be ten thousand (10,000) square feet.

GG. Drinking Establishments. Existing drinking establishments legally established (as of July 2001) are conforming uses in the PO/RM District and shall be treated the same as other allowed uses, consistent with applicable regulations. In the PO/RM area west of Yauger Road adjacent to Harrison/Mud Bay Road, maximum gross floor area shall be ten thousand (10,000) square feet.

HH. Urban Waterfront Housing. Up to a maximum of 1 FAR (floor area ratio) can be made up of allowed uses other than residential. (Refer to Pedestrian Street Overlay District 1816.060(B) for amount and location of commercial uses in a predominantly residential project on "A" streets.)

II. Veterinary Clinic and Office, Veterinary Clinic. Downtown Business (DB), Medical Services (MS), High Density Corridor-1 (HDC-1), and Professional Office/Residential Multifamily District (PO/RM) Requirements. Animals shall be kept indoors except for very brief periods of time as necessary for the animal's health.

18.06.080 Commercial districts' development standards (General)

A. General Standards. The standards contained in Table 6.02 (Commercial Districts' Development Standards) relating to lot area, building setbacks, development coverage, building coverage, and building height apply to commercial districts as indicated. They may be a minimum requirement (e.g. minimum lot size), or a maximum allowance (e.g., maximum height). Many of the standards in Table 6.02 are summaries of more detailed information found in Section 18.06.100, Specific Development Standards.

EXHIBIT E

18.06.080 TABLES: Commercial Districts' Development Standards

TABLE 6.02

COMMERCIAL DISTRICTS' DEVELOPMENT STANDARDS

STANDARD	NR	PO/RM	GC	HDC-1	HDC-2	HDC-3	HDC-4 and HDC-4 Capital Mall	ADDITIONAL REGULATIONS
MINIMUM LOT SIZE	7,200 Sq. Ft.	No minimum, except 1,600 = cottage 3,000 = zero lot 1,600 sq. ft. minimum 2,400 sq. ft. average = townhouse 6,000 sq. ft. = duplex 7,200 sq. ft. = multifamily 4,000 = other	No minimum, except 1,600 sq. ft. minimum 2,400 sq. ft. average = townhouse	No minimum, except 1,600 = cottage 3,000 = zero lot 1,600 sq. ft. minimum 2,400 sq. ft. average = townhouse 6,000 sq. ft. = duplex 7,200 sq. ft. = multifamily 4,000 = other	No minimum, except 1,600 = cottage 3,000 = zero lot 1,600 sq. ft. minimum 2,400 sq. ft. average = townhouse 6,000 sq. ft. = duplex 7,200 sq. ft. = multifamily 4,000 = other	No minimum, except 1,600 sq. ft. minimum 2,400 sq. ft. average = townhouse	No minimum, except 1,600 sq. ft. minimum 2,400 sq. ft. average = townhouse	See also 18.06.100(D) for regulations on existing undersized lots of record.
FRONT YARD SETBACK	See City-Wide Design Guideline: "Building Design - Orientation & Form of	10' maximum, if located in a High Density Corridor; 10' minimum otherwise.	5' minimum for residential otherwise none.	0-10' See 18.06A.180	0-10' See 18.06A.180	0-10' See 18.06A.180	0-10' See 18.06A.180	1. 50' minimum from property line for agriculture buildings (or structures) which house animals other than pets.

EXHIBIT E

TABLE 6.02

COMMERCIAL DISTRICTS' DEVELOPMENT STANDARDS

STANDARD	NR	PO/RM	GC	HDC-1	HDC-2	HDC-3	HDC-4 and HDC-4 Capital Mall	ADDITIONAL REGULATIONS
	Commercial & Public Buildings," 18.20.090.							2. Must comply with clear sight triangle requirements, Section 18.40.060(C). 3. Must comply with site design standards, Chapter 18.06A.180.
REAR YARD SETBACK	15' minimum.	10' minimum; Except: 1. Next to an R 4, R 4-8, or R 6-12 district = 15' minimum + 5' for each bldg. floor above 2 stories. 2. Next to MR	10' minimum; Except: 1. Next to single-family use or an R 4, R 4-8, or R 6-12 district = 15' minimum + 5' for each bldg. floor above 2 stories.	10' minimum; Except: 1. Next to an R4, R4-8, or R6-12 district = 15' minimum + 5' for each bldg. floor above 2 stories; 10 ft. where an alley separates	10' minimum; Except: 1. Next to an R4, R4-8, or R6-12 district = 15' minimum + 5' for each bldg. floor above 2 stories; 10 ft. where an alley separates	10' minimum; Except: 1. Next to single-family use or an R4, R4-8, or R6-12 district = 15' minimum + 5' for each bldg. floor above 2 stories. 2. Next to MR7-	10' minimum; Except: 1. Next to single-family use or an RLI, R4, R4-8, or R6-12 district - 15' minimum + 5' for each bldg. floor above 2 stories. 2. Next to MR7-	1. 50' minimum from property line for agriculture buildings (or structures) which house animals other than pets. 2. Must comply with site design standards,

EXHIBIT E

TABLE 6.02

COMMERCIAL DISTRICTS' DEVELOPMENT STANDARDS

STANDARD	NR	PO/RM	GC	HDC-1	HDC-2	HDC-3	HDC-4 and HDC-4 Capital Mall	ADDITIONAL REGULATIONS
		7-13, MR 10-18, RM-18, RM-24 or RMH district = 10' minimum + 5' for each bldg. floor above 2 stories.	2. Next to MR 7-13, MR 10-18, RM-18, RM-24 or RMH district (refer to 1 above if adjacent use is single-family) = 10' minimum + 5' for each bldg. floor above 2 stories.	HDC-1 from the above residential district. 2. Next to MR7-13, MR 10-18, RM-18, RM-24 or RMH district = 10' minimum + 5' for each bldg. floor above 2 stories.	HDC-2 from the above residential district. 2. Next to MR7-13, MR 10-18, RM-18, RM-24, or RMH district = 10' minimum + 5' for each bldg. floor above 2 stories.	13, MR10-18, RM-18, RM-24 or RMH district (refer to 1 above if adjacent use is single-family) = 10' minimum + 5' for each bldg. floor above 2 stories.	13, MR10-18, RM-18, RM-24 or RMH district (refer to 1 above if adjacent use is single-family) = 10' minimum + 5' for each bldg. floor above 2 stories.	Chapter 18.06A.180.
SIDE YARD SETBACK	15' minimum.	No minimum on interior, 10' minimum on flanking street; Except: 1. Next to R 4, R 4-8, or R 6-12 district = 15' minimum + 5' for each	No Minimum; Except: 1. Next to R 4, R 4-8, or R 6-12 district = 15' minimum + 5' for each building floor above 2 stories. 2. Next to MR 7-	No minimum on interior, 10' minimum on flanking street; Except: 1. Next to R4, R4-8, or R6-12 district = 15' minimum + 5' for each building floor	No minimum on interior, 10' minimum on flanking street; Except: 1. Next to R4, R4-8, or R6-12 district = 15' minimum + 5' for each building floor	No Minimum; Except: 1. Next to R4, R4-8, or R6-12 district = 15' minimum + 5' for each building floor above 2 stories. 2. Next to MR7-13, MR10-18, RM-	No Minimum; Except: 1. Next to <u>RLI</u> , R4, R4-8, or R6-12 district = 15' minimum + 5' for each building floor above 2 stories. 2. Next to MR7-13, MR10-18, RM-	1. 50' minimum from property line for agriculture buildings (or structures) which house animals other than pets. 2. Must comply with clear sight triangle

EXHIBIT E

TABLE 6.02

COMMERCIAL DISTRICTS' DEVELOPMENT STANDARDS

STANDARD	NR	PO/RM	GC	HDC-1	HDC-2	HDC-3	HDC-4 and HDC-4 Capital Mall	ADDITIONAL REGULATIONS
		building floor above 2 stories. 2. Next to MR 7-13, MR 10-18, RM-18, RM-24 or RMH district = 10' minimum + 5' for each bldg. floor above 2 stories. 3. Residential excluding mixed use structures: 5' except 6' on one side of zero lot.	13, MR 10-18, RM-18, RM-24 or RMH district = 10' minimum + 5' for each bldg. floor above 2 stories. 3. Residential excluding mixed use structures: 5' except 6' on one side of zero lot.	above 2 stories. 2. Next to MR7-13, MR10-18, RM-18, RM-24 or RMH district = 10' minimum + 5' for each bldg. floor above 2 stories. 3. Residential excluding mixed use structures: 5' except 6' on one side of zero lot.	above 2 stories. 2. Next to MR7-13, MR10-18, RM-18, RM-24 or RMH district = 10' minimum + 5' for each building floor above 2 stories. 3. Residential excluding mixed use structures: 5' except 6' on one side of zero lot.	18, RM-24 or RMH district = 10' minimum + 5' for each bldg. floor above 2 stories. 3. Residential excluding mixed use structures; 5' except 6' on one side of zero lot.	18, RM-24 or RMH district = 10' minimum + 5' for each bldg. floor above 2 stories. 3. Residential excluding mixed use structures; 5' except 6' on one side of zero lot.	requirements, Section 18.40.060(C). 3. Residential sideyards can be reduced consistent with 18.04.080(H)(5). 4. Must comply with site design standards, Chapter 18.06A.180.
MAXIMUM BUILDING HEIGHT	Up to 35', whichever is less.	Up to 35', if any portion of the building is within 100' of R	Up to 35', if any portion of the building is within 100' of R 4, R 4-	The portion of a building within 100' of land zoned for maximum	The portion of a building within 100' of land zoned for maximum	The portion of a building within 100' of land zoned for maximum	The portion of a building within 100' of land zoned for maximum	1. Not to exceed height limit set by State Capitol Group Height

EXHIBIT E

TABLE 6.02

COMMERCIAL DISTRICTS' DEVELOPMENT STANDARDS

STANDARD	NR	PO/RM	GC	HDC-1	HDC-2	HDC-3	HDC-4 and HDC-4 Capital Mall	ADDITIONAL REGULATIONS
		4, R 4-8, or R 6-12 district; Up to 60' otherwise.	8, or R 6-12 district; Up to 60' otherwise; or up to 70', if at least 50% of the required parking is under the building; or up to 75', if at least one story is residential.	density of less than 14 units per acre is limited to 35'. The portion of a building within 50' of land zoned for a maximum density of 14 units per acre or more is limited to the lesser of 60' or the height allowed in the abutting district. Up to 60' otherwise. Provided that one additional story may be built for residential development only.	density of less than 14 units per acre is limited to 35'. The portion of a building within 50' of land zoned for a maximum density of 14 units per acre or more is limited to the lesser of 60' or the height allowed in the abutting district. Up to 60' otherwise. Provided that one additional story may be built for residential development only.	density of less than 14 units per acre is limited to 35'. The portion of a building within 50' of land zoned for a maximum density of 14 units per acre or more is limited to the lesser of 60' or the height allowed in the abutting district. Up to 60' otherwise; or up to 70', if at least 50% of the required parking is under the building; or up to	density of less than 14 units per acre is limited to 35'. The portion of a building within 50' of land zoned for a maximum density of 14 units per acre or more is limited to the lesser of 60' or the height allowed in the abutting district. Up to 60' otherwise; or up to 70', if at least 50% of the required parking is under the building; or up to	District, 18.10.060, for properties near the State Capitol Campus. 2. Must comply with site design standards, Chapter 18.06A.180. 3. HDC-1 and HDC-2 additional story must comply with OMC 18.06.100.A.6.

EXHIBIT E

TABLE 6.02

COMMERCIAL DISTRICTS' DEVELOPMENT STANDARDS

STANDARD	NR	PO/RM	GC	HDC-1	HDC-2	HDC-3	HDC-4 and HDC-4 Capital Mall	ADDITIONAL REGULATIONS
						75', if at least one story is residential.	75', if at least one story is residential. See 18.06A.251(4) Significant Building Entry tower exemption (allows an additional 30' for a tower element at Capital Mall). Up to 75' for HDC-4 zoned properties where the proposed project provides for the development of replacement dwelling units in a development agreement and the project site is	

EXHIBIT E

TABLE 6.02

COMMERCIAL DISTRICTS' DEVELOPMENT STANDARDS

STANDARD	NR	PO/RM	GC	HDC-1	HDC-2	HDC-3	HDC-4 and HDC-4 Capital Mall	ADDITIONAL REGULATIONS
							all or part of an area of 40 acres or more that was in contiguous common ownership in 2009.	
MAXIMUM BUILDING COVERAGE	45%	70%, except 55% for residential only structures	70%; or 85% if at least 50% of the required parking is under the building.	70% for all structures	70% for all structures	70% for all structures, 85% if at least 50% of the required parking is under the building.	70% for all structures. 85% of the site if at least 50% of the required parking is under the building. On redeveloped sites, 85% if at least 50% of new required parking is under the building or in a structured parking	Must comply with site design standards, Chapter 18.06A.180. <u>For projects in the GC and HDC-4 zones west of Yauger Way, limitations of building size per 18.06.100(C) and 18.130.020 apply.</u>

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TABLE 6.02

COMMERCIAL DISTRICTS' DEVELOPMENT STANDARDS

STANDARD	NR	PO/RM	GC	HDC-1	HDC-2	HDC-3	HDC-4 and HDC-4 Capital Mall	ADDITIONAL REGULATIONS
							form. 85% for HDC-4 zoned properties where the proposed project provides for the development of replacement dwelling units in a development agreement and the project site is all or part of an area of 40 acres or more that was in contiguous common ownership in 2009.	
MAXIMUM DEVELOPMENT	60%	85%, except 75% for	85%	85% for all structures	85% for all structures	85% for all structures	85% for all structures	Must comply with site design

EXHIBIT E

TABLE 6.02

COMMERCIAL DISTRICTS' DEVELOPMENT STANDARDS

STANDARD	NR	PO/RM	GC	HDC-1	HDC-2	HDC-3	HDC-4 and HDC-4 Capital Mall	ADDITIONAL REGULATIONS
COVERAGE		residential only structures						standards, Chapter 18.06A.180.
ADDITIONAL DISTRICT-WIDE DEVELOPMENT STANDARDS	Maximum building size (gross sq. ft.): 3,000 for single use; 6,000 for mixed use.	Building floors above 3 stories which abut a street or residential district must be stepped back a minimum of 8 feet (see 18.06.100(ĐB) and Figure 6-3).	Building floors above 3 stories which abut a street or residential district must be stepped back a minimum of 8 feet (see 18.06.100(ĐB)).	Building floors above 3 stories which abut a street or residential district must be stepped back a minimum of 8 feet (see 18.06.100(ĐB)).	Building floors above 3 stories which abut a street or residential district must be stepped back a minimum of 8 feet (see 18.06.100(ĐB)).	Building Floors above 3 stories which abut a street or residential district must be stepped back a minimum of 8 feet (see 18.06.100(ĐB)).	Building floors above 3 stories which abut a street or residential district must be stepped back a minimum of 8 feet (see 18.06.100(ĐB)).	For properties in the vicinity of the <u>Downtown</u> or <u>Kaiser Road and Harrison Ave NE</u> , also see Pedestrian Streets Overlay District, Chapter 18.16. For retail uses over 25,000 square feet in gross floor area, see Section 18.06.100(G) Large Scale Retail Uses. EXCEPTION:

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TABLE 6.02

COMMERCIAL DISTRICTS' DEVELOPMENT STANDARDS

STANDARD	NR	PO/RM	GC	HDC-1	HDC-2	HDC-3	HDC-4 and HDC-4 Capital Mall	ADDITIONAL REGULATIONS
								Section 18.06.100(G) shall not apply to motor vehicle sales.

LEGEND		
NR = Neighborhood Retail GC = General Commercial	PO/RM = Professional Office/Residential Multifamily	HDC-1=High Density Corridor-1 HDC-2=High Density Corridor-2 HDC-3=High Density Corridor-3 HDC-4=High Density Corridor-4

EXHIBIT E

TABLE 6.02

COMMERCIAL DEVELOPMENT STANDARDS

STANDARD	MS	UW	UW-H	DB	CS-H	AS	ADDITIONAL REGULATIONS
MINIMUM LOT AREA	7,200 Sq. Ft.	No minimum.	No minimum.	No minimum.	7,200 Sq. Ft. if bldg. height is 35' or less. 12,500 Sq. Ft. if bldg. height is over 35'.	No minimum.	
FRONT YARD SETBACK	10' maximum.	No minimum; however, see Chapter 18.100 for design guidelines for pedestrian access and view corridors.	No minimum.	No minimum.	No minimum.	30' minimum for buildings; 15' for other structures except signs	1. 50' minimum from property line for agriculture buildings (or structures) which house animals other than pets. 2. Must comply with clear sight triangle requirements, Section 18.40.060(C). 3. See Design Guidelines, Chapter 18.100.

EXHIBIT E

TABLE 6.02

COMMERCIAL DEVELOPMENT STANDARDS

STANDARD	MS	UW	UW-H	DB	CS-H	AS	ADDITIONAL REGULATIONS
REAR YARD SETBACK	15' minimum; If next to a residential zone, 15' minimum plus 5' for every story over 3 stories.	No minimum; however, see Chapter 18.100 for design guidelines for pedestrian access and view corridors.	No minimum.	No minimum.	5' minimum if building has 1 or 2 stories. 10' minimum if building has 3 or more stories.	15' minimum.	50' minimum from property line for agriculture buildings (or structures) which house animals other than pets.
SIDE YARD SETBACK	10' minimum; 15' minimum plus 5' for every story over 3 stories if next to a residential zone.	No minimum; however, see Chapter 18.100 for design guidelines for pedestrian access and view corridors.	No minimum.	No minimum.	5' minimum if building has 1 or 2 stories. 10' minimum if building has 3 or more stories; AND the sum of the 2 side yards shall be no less than 1/2 the building height.	5' minimum 30' minimum for buildings and 15' minimum for other structures from flanking streets.	1. 50' minimum from property line for agriculture buildings (or structures) which house animals other than pets. 2. Must comply with clear sight triangle requirements, Section 18.40.060(C). 3. See Design Guidelines, Chapter 18.100.

EXHIBIT E

TABLE 6.02

COMMERCIAL DEVELOPMENT STANDARDS

STANDARD	MS	UW	UW-H	DB	CS-H	AS	ADDITIONAL REGULATIONS
MAXIMUM BUILDING HEIGHT	75'; except hospitals, which may exceed that height.	See Figure 6-2, Urban Waterfront District Height Limits Exceptions: 1) In the portion of the area Downtown with a height limit of 65', two additional residential stories may be built. See 18.06.100(A)(2)(b). 2) In the portion of the area on West Bay Drive with a height limit of 42' to 65', the taller height limit is conditioned upon the provision of certain waterfront amenities. See 18.06.100(A)(2)(c).	Refer to Figure 6-2 and 6-2B for specific height and building configurations required on specific blocks.	75'; PROVIDED, however, that two additional stories may be built, if they are residential. There are also restrictions around Sylvester Park. For details, see 18.06.100(C)(6) Height, Downtown Business District.	75' Exception: Up to 100' may be allowed with conditional approval by the City Council, upon recommendation of the Hearing Examiner. For details, see 18.06.100(C)(5), Height, Commercial Services-High Density.	40' accessory building limited to 20'.	Not to exceed height limit set by State Capitol Group Height District, 18.10.060, for properties near the State Capitol Campus.
MAXIMUM BUILDING COVERAGE	50%	60% for properties between the shoreline and the nearest upland street.	100%	No requirement.	No requirement.	85%	

EXHIBIT E

TABLE 6.02

COMMERCIAL DEVELOPMENT STANDARDS

STANDARD	MS	UW	UW-H	DB	CS-H	AS	ADDITIONAL REGULATIONS
		100% for properties not between the shoreline and the nearest upland street. See also Chapter 18.100 for design guidelines for pedestrian access and view corridors.					
MAXIMUM DEVELOPMENT COVERAGE	65%	100% development coverage.	100%	100%	100%	85%	
ADDITIONAL DISTRICT-WIDE DEVELOPMENT STANDARDS	Building floors above 3 stories which abut a street or residential district must be stepped back a minimum of 8 feet (see 18.06.100(F). Residential uses (Section 5 of	Street ends abutting the water shall be preserved to provide views of and public access to the water, pursuant to Section 12.16.050(D) OMC. See also Chapter 18.100 for Downtown design guidelines for Pedestrian Access and View Corridors and Waterfront Public Access;	Street ends abutting the water shall be preserved to provide views of and public access to the water, pursuant to OMC Section 12.16.050(D).		Residential uses must comply with High Rise Multi-family (RM-H) development standards.	6' of sight-screening buffer shall be provided along north, east, and west district boundaries. See Olympia Park Replat covenants for access, and	For properties in the vicinity of the Downtown, also see Pedestrian Streets Overlay District, Chapter 18.16. For retail uses over 25,000 square feet in gross floor area, see Section 18.06.100 (G) Large

EXHIBIT E

TABLE 6.02

COMMERCIAL DEVELOPMENT STANDARDS

STANDARD	MS	UW	UW-H	DB	CS-H	AS	ADDITIONAL REGULATIONS
	<p>Table 6.01) may not be constructed within 600 feet of Lilly Road except in upper stories of mixed use building; all other development standards are the same as for commercial uses.</p>	<p>Chapter 18.100 for Port Peninsula design guidelines for Pedestrian Connections and View Corridors; Section 18.06.100(A)(2)(c) for West Bay Drive building height and view blockage limits; and Chapter 18.100 for West Bay Drive view corridors. See also Chapter 18.100 for Downtown design guidelines for Pedestrian Access and View Corridors and Waterfront Public Access; Chapter 18.100 for Port Peninsula design guidelines for Pedestrian Connections and View Corridors; Section 18.06.100(A)(2)(c) for West Bay Drive building height</p>				<p>other standards applicable to replat lots.</p>	<p>Scale Retail Uses. EXCEPTION: Section 18.06.100 (G) shall not apply to motor vehicle sales.</p>

TABLE 6.02

COMMERCIAL DEVELOPMENT STANDARDS

STANDARD	MS	UW	UW-H	DB	CS-H	AS	ADDITIONAL REGULATIONS
		and view blockage limits; and Chapter 18.100 for West Bay Drive view corridors.					

LEGEND		
MS = Medical Services	CS-H = Commercial Services - High Density	UW = Urban Waterfront UW-H = Urban Waterfront-Housing AS=Auto Services
DB = Downtown Business		

18.06.100 Commercial districts' development standards--Specific

A. Height.

1. Roof structures for the housing of elevators, stairways, tanks, ventilating fans and similar equipment required to operate and maintain the building, fire or parapet walls, skylights, towers, flagpoles, chimneys, smoke stacks, wireless masts, T.V. antennas, steeples and similar structures may be erected above the height limits prescribed in this Title, provided that no roof structure, feature or any other device above the prescribed height limit shall be allowed or used for the purpose of providing additional floor space. This height exception does not apply to the additional story provision for residential development described in OMC 18.06.100.A.6. Provided, further, that no roof structure or architectural feature shall be erected more than eighteen (18) feet above the height limit of the district, whether such structure is attached to it or free-standing.

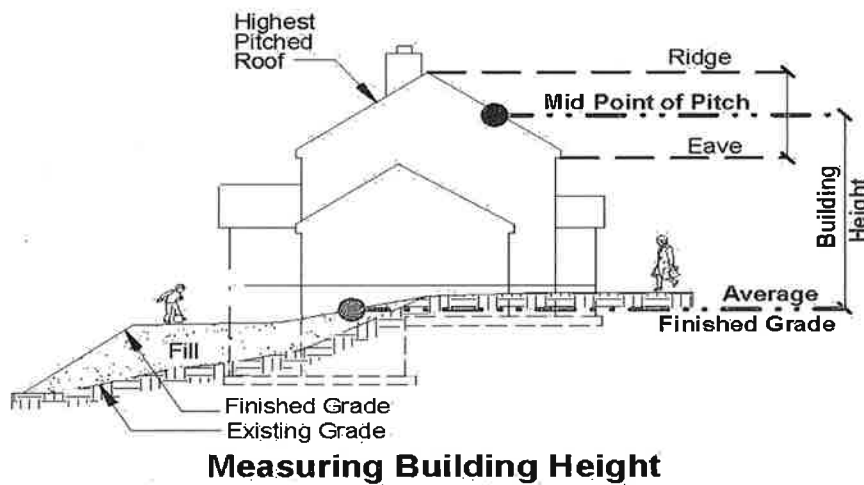


FIGURE 6-1A

2. Urban Waterfront (UW) District.

a. Allowed building heights in the Urban Waterfront (UW) District are specified in Figure 6-2.

b. Bonus for residential development.

i. In the area labeled sixty-five (65) feet on Figure 6-2, up to two additional stories may be built, if the project is located in the downtown, if the added stories are stepped back from the street wall at least eight (8) feet, and if floor area equal to the amount from the added stories is provided for residences:

- (a) In the same building--i.e., it is a residential or a mixed use building; or
- (b) With commercial and residential uses in separate buildings on the same site; or
- (c) With commercial and residential uses on separate sites within the Urban Waterfront (UW) district.

ii. Occupancy. Housing provided under this bonus provision as part of a mixed use project must receive an occupancy permit at the same time as, or in advance of, issuance of an occupancy permit for non-residential portions of the project.

iii. Conversion. Housing provided under this bonus provision shall not be converted to commercial use.

iv. Source of housing units. Housing provided under this bonus provision may be:

- (a) New construction,
- (b) Adaptive reuse of a formerly non-residential structure, or
- (c) Rehabilitation of existing housing.

c. West Bay Drive building height and view blockage limits.

i. In order to retain public and private view access to Budd Inlet from hillside sites above West Bay Drive, the maximum building height in the West Bay Drive portion of the Urban Waterfront (UW) District labeled " 42'-65' " on Figure 6-2 shall be up to a maximum of 42 feet, except as provided in subsections (iii) and (iv) below.

ii. In order to retain public view access of Budd Inlet from street level in the West Bay Drive portion of the Urban Waterfront (UW) District labeled " 42'-65' " on Figure 6-2, view blockage shall be limited as follows:

- (a) Views of the water will be defined as area without obstruction by buildings or major structures measured between 45 and 90 degrees to West Bay Drive, as illustrated in Figure 6-2A.
- (b) Said view blockage shall be limited to 45 percent of the views of the water from West Bay Drive by buildings or major structures located between West Bay Drive and the mean high water line.
- (c) Exceptions are provided in subsections (iii) and (iv) below.

iii. Development shall be subject to the alternate standards for building height and view blockage, if alternate waterfront view access is provided through public amenities as follows:

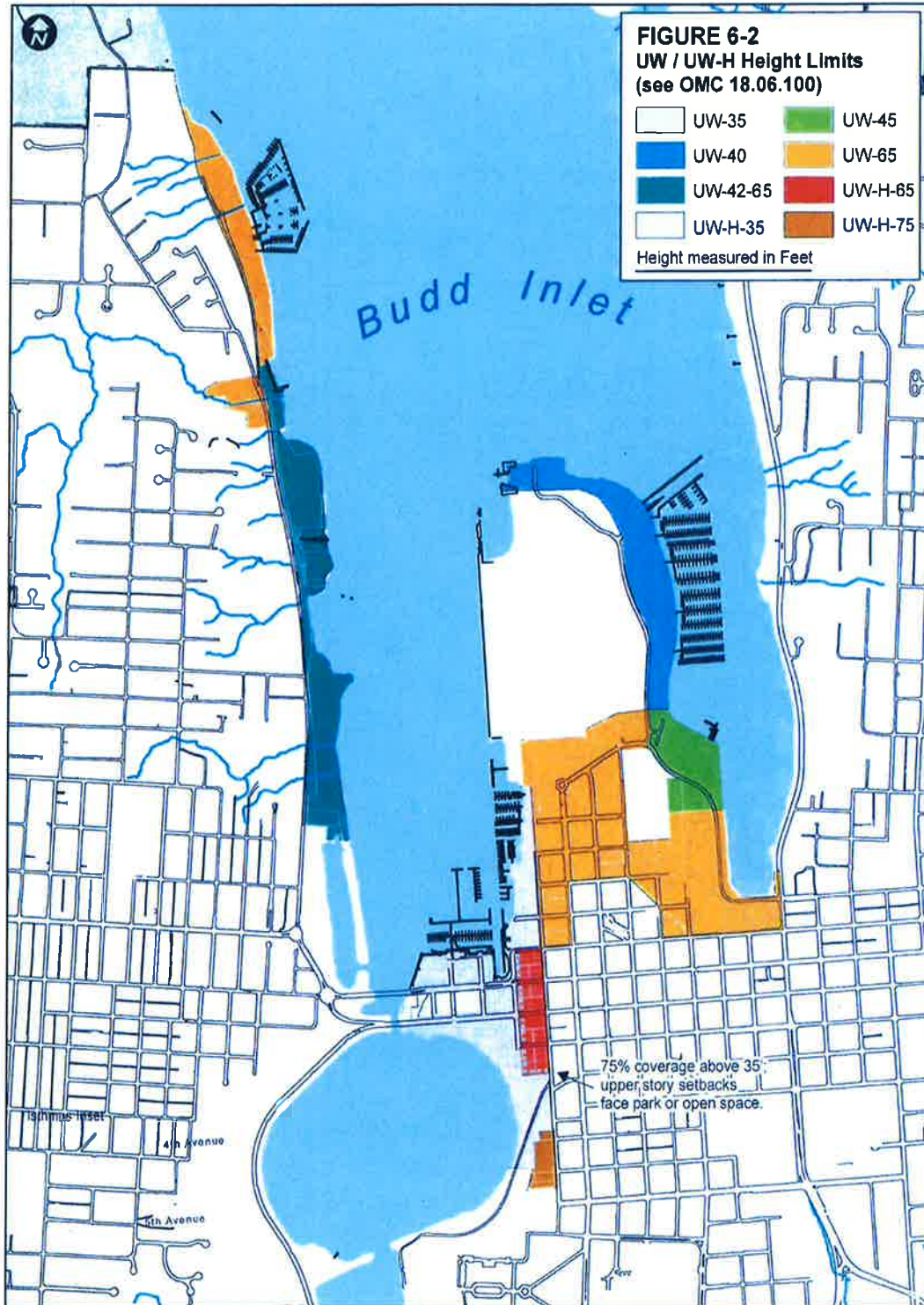
Amenity Provided	Limits on Horizontal View Blockage and Height
Waterfront Trail	70% up to 42 ft., OR 45% up to 65 ft.
Expanded Waterfront Trail Corridor Facility (or small waterfront park area).	50% up to 42 ft., OR 45% up to 50 ft.
Both	70% up to 65 ft.
Any development over 42 feet shall be required to include a minimum of 20% of the usable building area for residential purposes.	

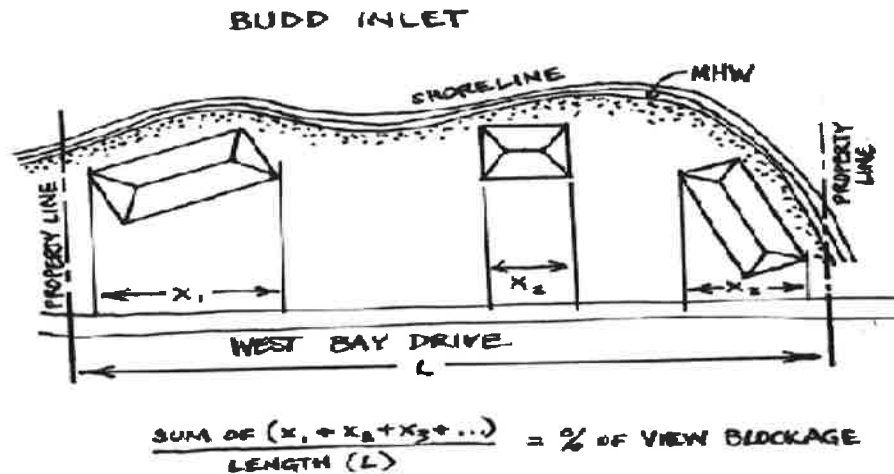
iv. Criteria for approval of alternate waterfront view access.

- (a) Waterfront Trail.

- (1) Trail right-of-way consistent with City trail standards shall be dedicated to the City.
 - (2) The trail shall be designed consistent with City standards and requirements, or as otherwise approved by the Olympia Parks, Arts and Recreation Department. Because the trail passes by different land uses, it may take a different character in different locations, for reasons of safety, privacy, or environmental protection.
 - (3) The developer shall design, build, and dedicate the facility to the City.
 - (4) An analysis of recreation needs shall be provided by the Olympia Parks, Arts and Recreation Department. An analysis of environmental impacts, hazardous waste risks, and engineering issues sufficient to determine the design and location for the trail facility shall be approved by the Olympia Parks, Arts and Recreation Department but provided by the developer. All analysis shall be complete prior to approval.
- (b) Expanded Waterfront Trail Corridor Facility or Small Waterfront Park.
- (1) The developer shall build and dedicate the facility and its site to the City.
 - (2) The expanded waterfront trail corridor facility or small park area shall be designed consistent with City and other applicable government standards and requirements, or as otherwise approved by the Olympia Parks, Arts and Recreation Department. The expanded waterfront trail corridor facility or small park may vary in size from City park standards and could include additional right-of-way for the expanded trail, landscaping, habitat enhancement, benches, lighting, parking, restrooms, garbage receptacles, telephones, interpretive signs and other park facilities.
 - (3) An analysis of environmental impacts, hazardous waste risks, trail improvements, and engineering issues sufficient to design the expanded waterfront trail corridor facility or small park area shall be approved by Olympia Parks, Arts and Recreation Department but provided by the developer. All analysis shall be complete prior to approval.
 - (4) The expanded waterfront trail corridor facility or small park shall have a publicly accessible connection to West Bay Drive, designed, constructed, and dedicated for public use by the developer.
- v. The view blockage rules shall be applied on a project-wide basis and not for each lot or parcel in a project, thus allowing projects providing more views on some lots to have more view blockage on other lots as long as the overall project meets the view blockage requirements.

Figure 6-2 Urban Waterfront and Urban Waterfront Height Limits



**FIGURE 6-2A****Calculating View Blockage in a portion of the Urban Waterfront District along West Bay Drive.**

3. Commercial Services-High Density. The maximum building height allowed is one hundred (100) feet. Provided, however, that no building or structure may exceed seventy-five (75) feet in height without conditional review and approval by the Hearing Examiner. Approval of structures exceeding seventy-five (75) feet in height shall meet the following criteria:

a. The building design shall be compatible with or enhance the physical characteristics of the site, the appearance of buildings adjacent to the site and the character of the district.

b. The site plan shall facilitate efficient and convenient circulation, shall include landscaping that creates a pleasing appearance from both within and off the site and shall be an asset to the community at large.

c. Enhancement of public view access or direct public access to usable open space areas shall offset any potential upland view loss which may occur as a result of the proposal.

4. Downtown Business District.

a. Building height allowed outright in the DB zone is seventy-five (75) feet.

b. Bonus for residential development.

c. Enhancement of public view access or direct public access to usable open space areas shall offset any potential upland view loss which may occur as a result of the proposal.

i. Buildings may exceed the height allowed outright (75 feet) by up to two (2) stories, if the added stories are stepped back from the street wall at least eight (8) feet, and if floor area equal to the amount from the added stories is provided for residences:

(a) In the same building--i.e., it is a residential or a mixed use building; or

(b) With commercial and residential uses in separate buildings on the same site; or

(c) With commercial and residential uses on separate sites within the Downtown Business (DB) zone.

- ii. Occupancy. Housing provided under this bonus provision as part of a mixed use project must receive an occupancy permit at the same time as, or in advance of, issuance of an occupancy permit for non-residential portions of the project.
 - iii. Conversion. Housing provided under this bonus provision shall not be converted to commercial use.
 - iv. Source of housing units. Housing provided under this bonus provision may be:
 - (a) New construction,
 - (b) Adaptive reuse of a formerly non-residential structure, or
 - (c) Rehabilitation of existing housing.
5. Urban Waterfront - Housing.
- a. Allowed building heights in the Urban Waterfront-Housing District are specified in Figure 6-2.
 - b. Required step backs and placement of step backs over 35 feet on specific blocks are specified in Figure 6-2.
6. High Density Corridor (HDC - 1 and HDC - 2).
- a. Building height allowed outright in the HDC-1 and HDC-2 zones as outlined in OMC 18.06.080, Table 6.02.
 - b. Additional story for residential development.
 - i. Additional story can only be allowed for those development that do not provide a mechanical "penthouse" room as allowed under the provisions of OMC 18.06.100.A. However, the additional story can be occupied with both residential development and mechanical equipment.
 - ii. Buildings may exceed the height allowed outright in OMC 18.06.080, Table 6.02, by one (1) story. The additional story cannot exceed fourteen (14) feet above the maximum allowable height requirement as specified in OMC 18.06.080, Table 6.02.
 - iii. The additional story must be stepped back at least eight (8) feet from any abutting street or any abutting residential zoning district. See OMC 18.06.100.B.2.
 - iv. Housing provided under this additional story as part of a mixed use project must receive an occupancy permit at the same time as, or in advance of, issuance of an occupancy permit for non-residential portions of the project.
 - v. Housing provided under this additional story provision shall not be converted to commercial use. Except that the residential units may conduct business activities under the provision for home occupations. See OMC 18.04.060.I.
 - vi. Housing provided under this bonus provision may be:
 - (a) New construction;
 - (b) Adaptive reuse of a formerly non-residential structure, or
 - (c) Rehabilitation of existing housing.

B. Upper Story StepBacks.

1. High Density Corridor-1 (HDC-1), Community Retail (CMR), High Density Corridor-2 (HDC-2), General Commercial (GC), High Density Corridor-4 (HDC-4), Medical Services (MS), and Professional Office/Residential Multifamily (PO/RM) District Requirements:

Building floors above three (3) stories which abut a street or residential district must be stepped back a minimum of eight (8) feet (see Figure 6-3).

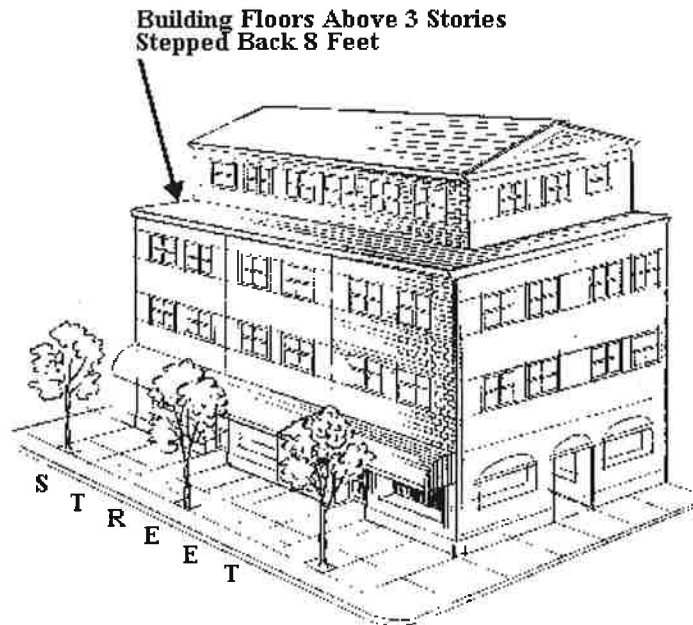


FIGURE 6-3

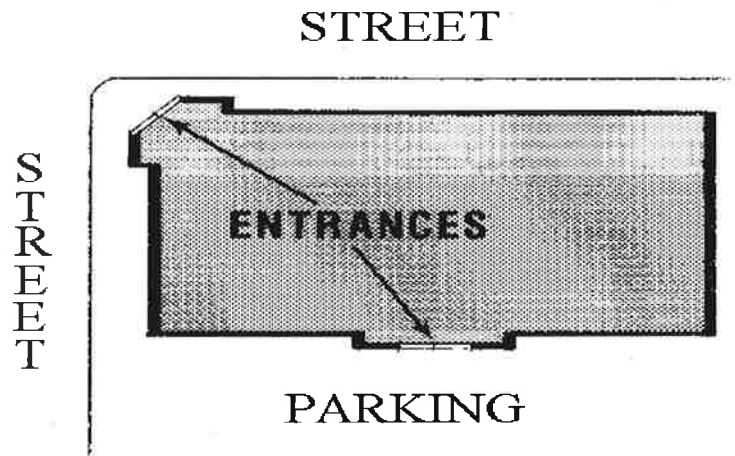
2. Additional Story Provision for HDC-1 and HDC-2. Projects within the HDC-1 and HDC-2 zoning districts which use the additional story provisions for residential development as outlined in OMC 18.06.100.A.6, must step the additional story back by a minimum of eight (8) feet. The step back is required for the additional story which abuts a street or residential district.

C. Large Scale Retail Uses. Retail uses over twenty-five thousand (25,000) square feet in gross floor area under common ownership or use shall meet the design requirements of this section. For purposes of this section, a retail use under common ownership or use shall mean a single establishment which shares checkstands, management, a controlling ownership interest, or storage areas, e.g., a plant nursery or a grocery store associated with a general merchandise store, such as a home improvement store.

In General Commercial and HDC-4 zones west of Yauger Way, single story or single use commercial retail space shall not occupy more than 60,000 square feet of enclosed building space on the ground floor, unless a development agreement is approved. These buildings shall be designed and oriented to provide for pedestrian and bicycle circulation throughout the site and to adjacent buildings and properties. A building larger than 60,000 square feet can be allowed when it is not directly adjacent to a street designated as an "A" street in the Pedestrian Street Overlay and if a development agreement is approved that at a minimum addresses:

- 1) Building orientation, massing, and use of high quality materials
- 2) Parking is located to the rear or side of the building, or is separated from the street by additional retail buildings
- 3) Pedestrian, bicycle, and vehicular circulation on site and connections to adjacent properties
- 4) Community assets, such as the multi-use trail identified in the Kaiser Harrison Opportunity Area Plan

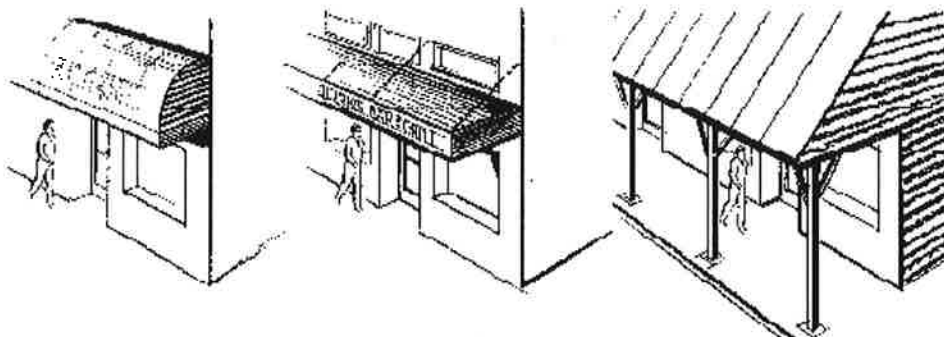
1. Customer entrances. Customer entrances shall be provided on each facade that faces an abutting street, customer parking, or a public park or plaza, up to a maximum requirement of three customer entrances per business occupancy. If there are two or more facades facing abutting streets, at least two such facades must provide a customer entrance. An entrance on a corner of the building may count as serving two facades. Such entrances shall provide both ingress and egress, and shall be double doors, not just single units. See Figure 6-4.



Customer Entrances must be provided on facades facing abutting streets and parking. Example shows corner entrance serving two street facades, plus entrance serving parking.

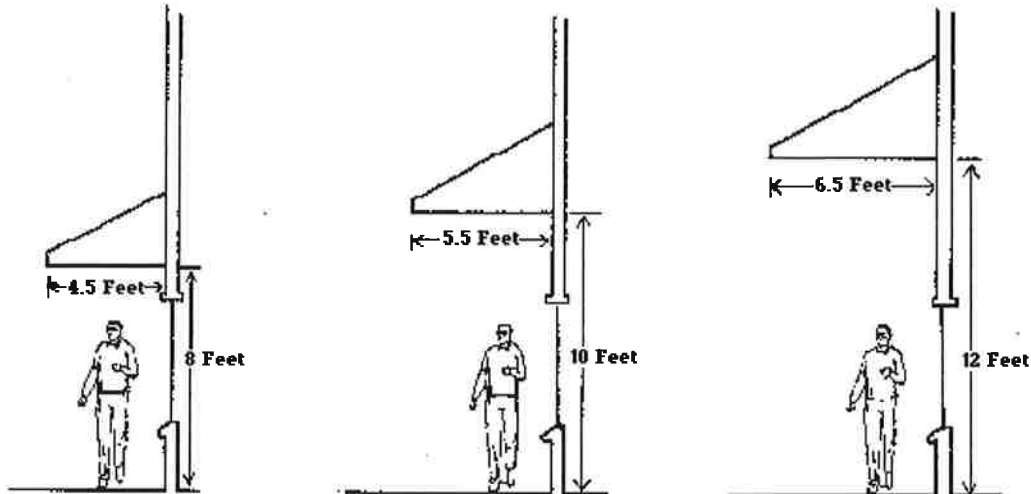
FIGURE 6-4

2. Rain protection. Awnings, canopies, marquees, arcades, building overhangs or similar forms of pedestrian weather protection, at least four and one half (4 1/2) feet wide, shall be provided over a pedestrian walkway along at least eighty (80) percent of any facade with a customer entrance. See Figure 6-5. Such weather protection shall be placed no less than eight (8) feet above the walkway. If placed more than eight (8) feet above the walkway, such weather protection shall be at least an additional six (6) inches in width for each additional foot of height, or portion thereof. See Figure 6-6. Development in the HDC-4 Capital Mall Area shall use design standards established for this area instead of the above rain protection regulation. See 18.06A.280 Building Design - B.4 Building elements.



Rain Protection (L to R): Awning, Marquee, Arcade

FIGURE 6-5



Width of Rain Protection is determined by height above walkway.

FIGURE 6-6

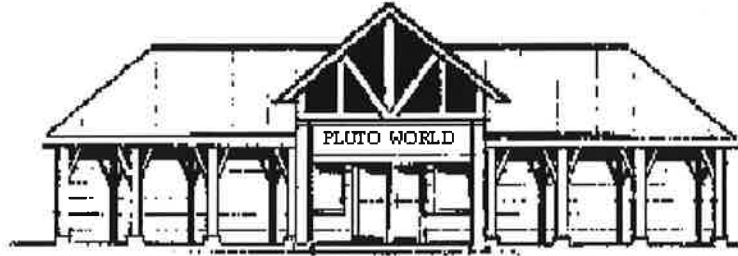
3. Wall articulation. Facades greater than fifty (50) feet in length shall incorporate wall plane projections or recesses having a depth of at least three percent (3%) of the length of the facade and extending in the aggregate at least twenty percent (20%) of the length of the facade. No uninterrupted length of any such facade shall exceed fifty (50) horizontal feet. EXCEPTION: This requirement shall not apply to walls which:

- a. have no customer entrance; and
- b. are only visible from service areas, and not from nearby residences or from the customer parking lot or an abutting street.

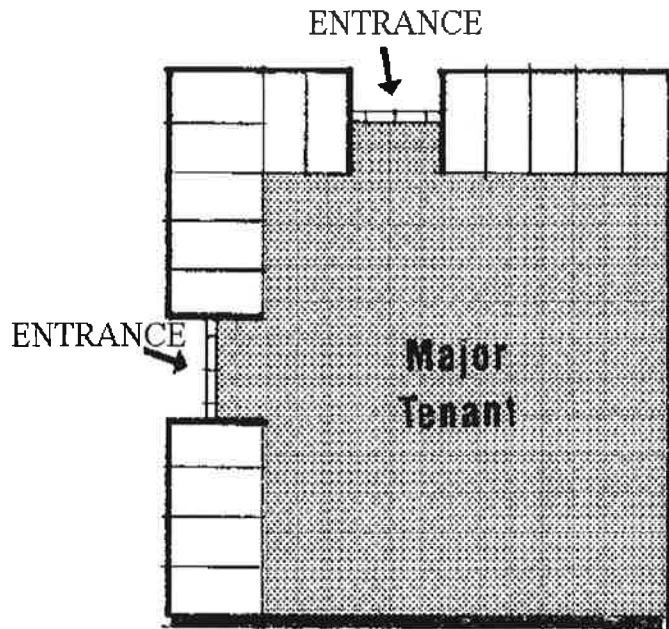
Development in the HDC-4 Capital Mall Area shall use design standards established for this area instead of the above wall articulation regulation. See 18.06A.280 through 18.06A.284, Building Design.

4. Frontage limit. The frontage per business occupancy shall be limited to one hundred (100) feet along any facade facing an abutting street, unless sixty percent (60%) or more of the facade between two (2) and eight (8) feet above the sidewalk is in transparent glazing; i.e., transparent windows, display windows, or transparent store doors (staff note: this would allow a major tenant to have lots of its own display windows, or to lease peripheral space to lots of small tenants, or to look like it was doing so, or to build added stories to get added floor area). See Figures 6-7 through 6-12. EXCEPTION: This requirement shall not apply to that portion of a facade where the average grade level of the sidewalk of the abutting street is 4 feet or more above or below the adjacent floor level of the building. See Figure 6-13.

Development in the HDC-4 Capital Mall Area shall use design standards established for this area instead of the above frontage limit. See 18.06A.280 through 18.06A.284, Building Design.



Example of building with 100' frontage, hence exempt from transparent glazing requirement.
FIGURE 6-7



Frontage limited by placing small shops on periphery of building, plan view.
FIGURE 6-8



Small shops on periphery of building, elevation view.
FIGURE 6-9



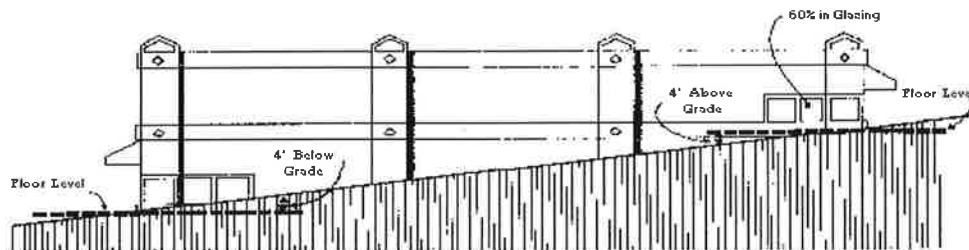
150 foot frontage with 60% of facade between 2' and 8' in transparent glazing.
FIGURE 6-10



25,000 square foot 1-story building with 150 feet of frontage
FIGURE 6-11



50,000 square foot building on 2 stories with 150 feet of frontage
FIGURE 6-12



Transparency requirement does not apply to the portion of a facade with a floor level over 4' above or below grade.

FIGURE 6-13

5. Very Large Scale Retail Facilities. Retail uses under common ownership or use, which exceed size thresholds set forth in subsection (a) below for the zone in which the retail uses are proposed, shall meet the additional development and design requirements specified in subsections (a)(ii)-(iv). Those which exceed size thresholds set forth in subsection (b) below for the zone in which the retail uses are proposed shall be subject to the requirements for Conditional Use approval provided in subsection (b)(ii).

a. Added development and design requirements for Very Large Scale Retail Facilities

i. Thresholds for requirements

District	Size (gross floor area)
GC	60,000 sq. ft.
HDC-2	40,000 sq. ft.
HDC-3	50,000 sq. ft.
HDC-4, except Capital Mall Area	60,000 sq. ft.
UW	40,000 sq. ft.
UW-H	25,000 sq. ft.
DB	25,000 sq. ft.
UC	50,000 sq. ft.

ii. Adaptability for Reuse/Compartmentalization. The building design shall include specific elements that facilitate the structure’s adaptation for multi-tenant reuse. Such elements may include but are not limited to compartmentalized construction, including plumbing, electrical service, heating, ventilation and air conditioning. The building design shall also allow for all of the following:

- (1) Subdivision of the interior of structure into separate tenancies. The design for interior subdivision shall accommodate multiple potential tenancies, each no larger than fifty percent (50%) of the size threshold for the district defined in subsection (i) above. Example A: An applicant designs a 120,000 sq. ft. Very Large Scale Retail Use in the GC district to accommodate reuse by four potential tenancies of 30,000 sq. ft. each. Example B: An applicant designs the same building to accommodate two potential tenancies of 30,000 sq. ft., and four potential tenancies of 15,000 sq. ft.
- (2) Facades that readily adapt to multiple entrances without compromising the structural integrity of the building, and adapt to entrances on at least two sides of the building; or, if the building is designed to have only one front facade, all potential tenancies shall be designed for access from the front facade.
- (3) Parking lot designs that are shared by establishments or are linked by safe and functional pedestrian connections.
- (4) Landscaping schemes that complement the multiple entrance design.
- (5) Design and placement of loading docks/loading bays to accommodate multiple potential tenancies.
- (6) Other elements of design which facilitate the multi-tenant reuse of the building and site.

iii. Parking Design.

- (1) Parking lots with over one acre in paving shall be designed for on-site infiltration of the stormwater generated on site. This may be accommodated by underground infiltration vaults, porous paving, or other techniques permitted by the City of Olympia Stormwater Drainage Manual, and subject to the approval of the Department of Public Works.

(2) Bicycle parking shall meet all requirements of the City's bicycle parking regulations, in particular Sections 18.38.100 Vehicular and Bicycle Parking Standards, and 18.38.220 Design Standards - General.

iv. Site Design.

(1) The site design shall include a plan for pedestrian circulation with logical connections between buildings, between buildings and adjacent streets, and from buildings to parking areas. (See also Sections 18.110.030, 18.120.110, and 18.150.030.)

(2) Pedestrian walkways within the development shall be differentiated from driving surfaces through a change in materials, and shall be designed to accommodate persons with disabilities, such as wheelchair users.

b. Conditional Use Approval

i. Thresholds for Conditional Use Approval

District	Size (gross floor area)
GC	125,000 sq. ft.
HDC-2	60,000 sq. ft.
HDC-3	75,000 sq. ft.
HDC-4, except Capital Mall Area	125,000 sq. ft.
UW	60,000 sq. ft.
UW-H	40,000 sq. ft.
DB	40,000 sq. ft.
UC	100,000 sq. ft.

ii. Conditions for Approval. The following requirements apply to all Very Large Scale Retail Facilities subject to conditional use approval.

(1) The Hearing Examiner shall determine that the proposed facilities meet the development and design requirements of subsection (a) above, and all other requirements of this Title.

(2) The Hearing Examiner shall determine that the proposed facilities will not be detrimental to the health, safety, or welfare of the general public, nor injurious to property, improvements or potential development in the vicinity, with respect to aspects including but not limited to the following:

(a) The nature of the proposed site, including its size and shape, and the proposed size, shape and arrangement of structures;

(b) The accessibility and traffic patterns for persons and vehicles, the type and volume of such traffic, and the adequacy of proposed off-street parking and loading;

(c) The safeguards afforded to prevent noxious or offensive emissions such as noise, glare, dust and odor; and

(d) The treatment given, as appropriate, to such aspects as landscaping, screening, open spaces, parking and loading areas, service areas, lighting and signs.

(e) The impact upon public facilities or public services.

6. Additional Regulations. Refer to the following Chapters for additional related regulations:

- a. Chapter 18.36, Landscaping and Screening
- b. Chapter 18.38, Parking and Loading
- c. Chapter 18.48, Conditional Uses
- d. Chapter 18.100, Design Review
- e. Chapter 18.110, Basic Commercial Design
- f. Chapter 18.120, Commercial Design Criteria Downtown
- g. Chapter 18.130, Commercial Design Criteria High Density Corridor (HDC)
- h. Chapter 18.150, Port Peninsula

18.06.120 Additional regulations

Refer to the following Chapters for additional related regulations.

Chapter 18.06A	Design Guidelines
Chapter 18.36	Landscaping and Screening
Chapter 18.38	Parking and Loading
Chapter 18.40	Property Development and Protection Standards
Chapter 18.42	Signs
Article III	Overlay Districts

Chapter 18.16 PEDESTRIAN STREET OVERLAY DISTRICT

18.16.000 Chapter Contents

Sections:

- 18.16.020 Intent.
- 18.16.040 Map.
- 18.16.060 General requirements.
- 18.16.080 Specific development standards.

18.16.020 Intent

This chapter will implement City and regional transportation plans by establishing a pedestrian street overlay district that results in a network of active, aesthetically pleasing, and interesting streets that link the downtown retail core, the Capitol Campus, the waterfront, and residential neighborhoods. In the Kaiser-Harrison area, it will provide a highly integrated and pedestrian friendly mix of office, retail, and residential use. In these is high quality, compact pedestrian-oriented environments people will be encouraged to work, shop, play, and live in an area that supports decreased dependence on motor vehicles, makes travel on foot pleasant and easy, and encourages around-the-clock activity to occur. The downtown form development should: 1) acknowledge the need to accommodate arrival by vehicle, 2) support increased arrival by transit, carpool, bike, and on foot, 3) create incentives to circulate within the areadowntown by non-auto modes (on foot, by shuttle or bus, or bicycle), 4) encourage employee Transportation Demand Management Program success. In addition, development in the area should:

- A. Increase architectural and historic continuity between the existing downtown retail core and the remainder of downtown, or provide for continuity between new and existing uses in other mixed use areas.
- B. Provide a comfortable sense of enclosure along the street by providing strong, continuous edges which clearly define public open spaces and rights-of-way.
- C. Contribute to the streetscape by incorporating human-scaled elements into building design.
- D. Provide direct visual contact between activities occurring inside buildings and the street environment.
- E. Incorporate artistic elements and public art into the streetscape and buildings.
- F. Result in urban building patterns and curtail the construction of suburban building patterns.

18.16.040 Map

- A. Relationship to Underlying Zoning. This overlay zone contains regulations that apply in addition to the regulations included within each downtown zone the underlying zoning district.
- B. Areas Subject to Pedestrian Streets Regulations.
 - 1. "A" Streets. All of the requirements of this Chapter apply to the "A" Streets identified in Figure 16-1.

2. "B" Streets. The following requirements of this Chapter apply to "B" Streets as identified in Figure 16-1:

- a. Blank Wall Limitations, Section 18.16.080(F);
- b. Primary Building Entrance, Section 18.16.080(G);
- c. Parking Structure Design, Section 18.16.080(J).

3. The Pedestrian Streets Overlay District Map and this text regulate the Pedestrian Streets Overlay District. The Pedestrian Streets Overlay District regulates portions of the DB, RMH, RMU, UR, UW-H, and UW districts in downtown Olympia, and in the HDC-4, GC, and PO/RM districts west of Yaeger Way. All properties which abut the Pedestrian Streets shown on Figure 16-1 are subject to the rules of this District.

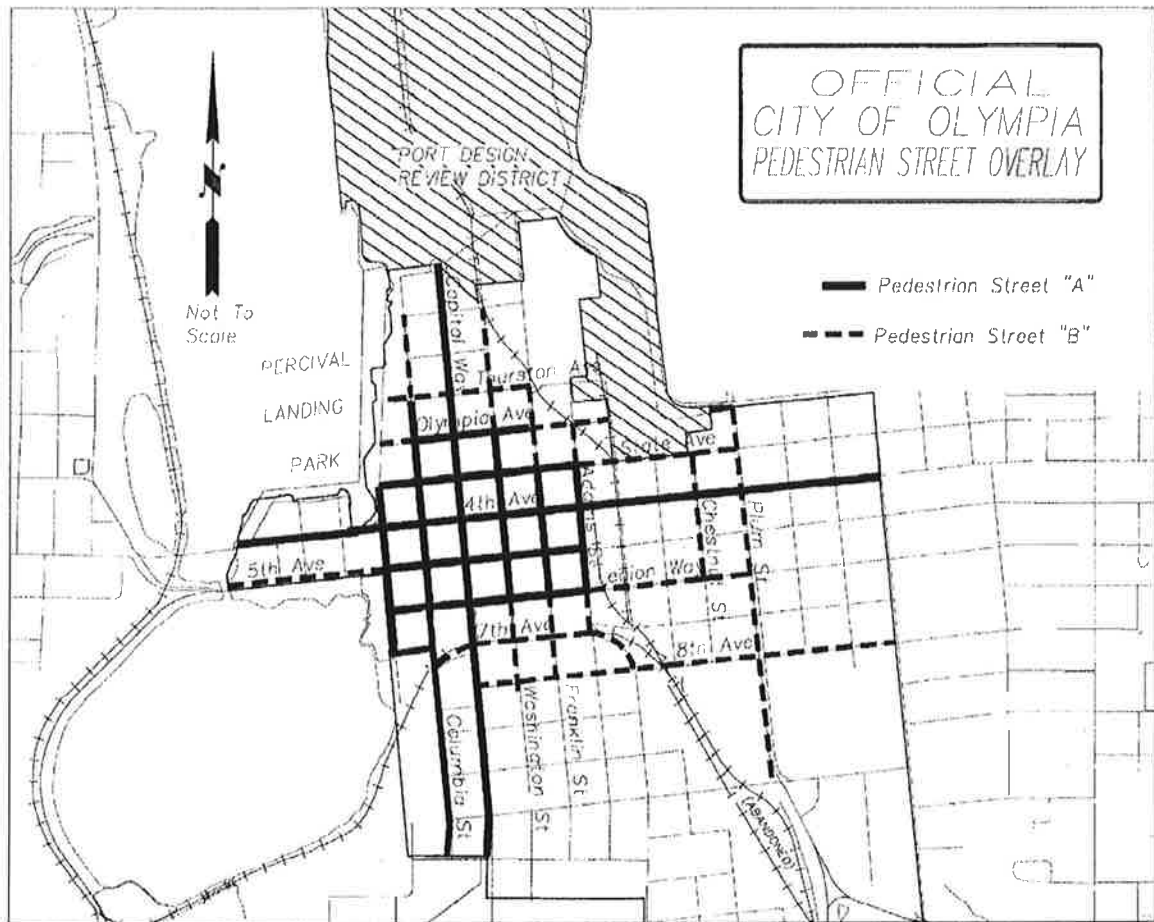


FIGURE 16-1

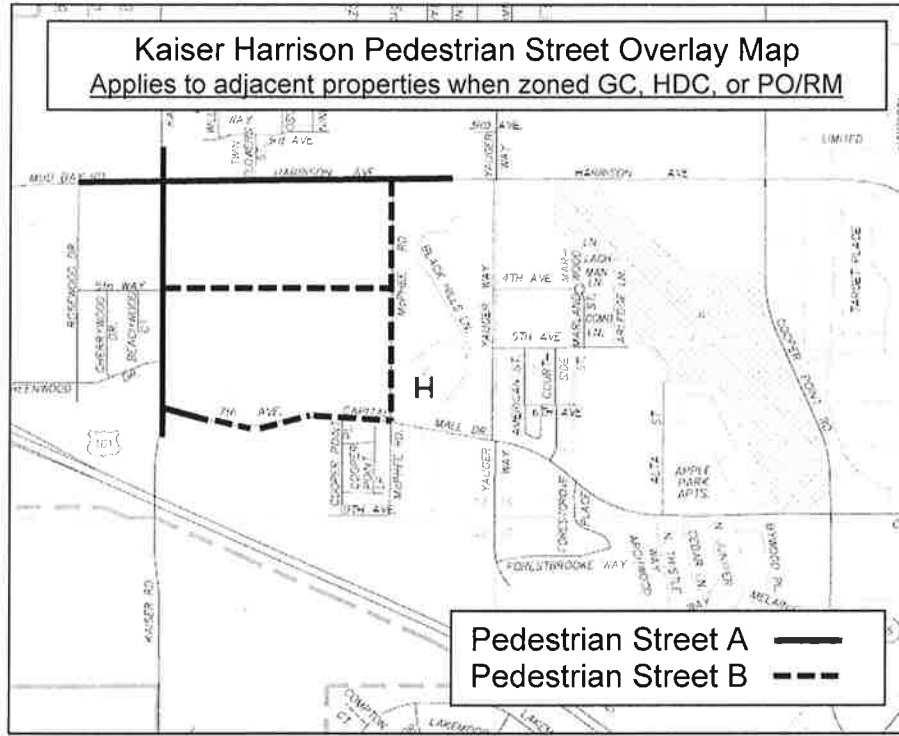


FIGURE 16-1b

18.16.060 General requirements

A. Types of Projects Regulated.

1. **New Development.** The regulations in this chapter apply to all properties which abut the Streets identified in Figure 16-1 and Figure 16-1b (both "A" and "B" Streets). The regulations of this Chapter apply to all new building construction.
2. **Existing Buildings.** The regulations in this Chapter also apply to all remodel projects if both of the following conditions apply: (1) the value of the remodel project is greater than fifty (50) percent of the assessed value of the pre-remodeled building excluding land value, and (2) structural facade renovation will be done. (Painting and routine maintenance are not considered structural facade renovation).

B. Exemptions

1. **Residential Exemption.** Residential projects must comply with the applicable design criteria of Chapters 18.100 through 18.180 Downtown Design Guidelines (18.06A) and Citywide Design Guidelines (18.20). All or a portion of a residential project shall be exempt from the requirements of this Chapter as follows:
 - a. In the event a residential project comprises the entire block face, 50% of the block face shall comply with this Chapter; provided, that the area of compliance must be evenly placed at each corner of the block face.
 - b. In the event a residential project is proposed for a portion of a block face which block face has been partially developed in compliance with this Chapter, a portion of the project shall comply with this Chapter according to this formula. The amount of project compliance shall equal the difference between 50% and the percentage of existing improvements on the block which complies with this Chapter. For example, 15% of the project must comply

with this Chapter where 35% of existing improvements on the block face meets the requirements of this Chapter. In the event the proposed project is located at a corner of a block face, that portion of the project which is required to comply with this Chapter shall be located at the corner.

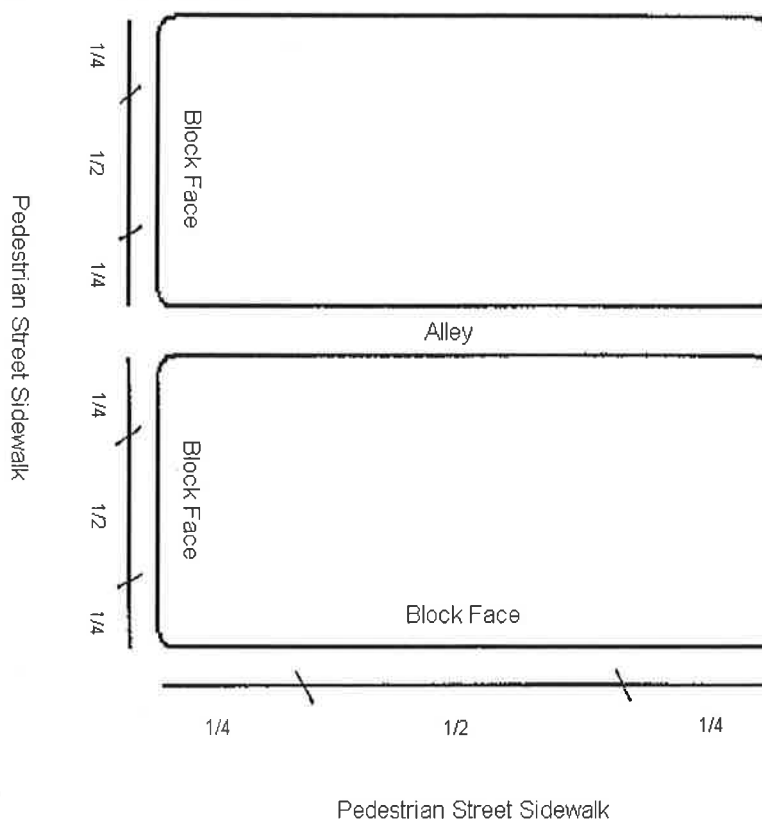


FIGURE 16-9

2. Historic Buildings. All buildings on the Heritage Register at the time of application are exempt from the requirements of this District.
3. Small Buildings. Buildings with less than five thousand (5,000) square feet in gross floor area are exempt from the requirements of this District.
4. Existing Parking Lots in Front of Buildings. When the requirements of this Chapter are triggered by remodels or renovations, existing parking lots remain conforming as an exception to the "Maximum Setbacks Allowed" of this Chapter.

C. See applicable Design Review Chapters, Chapters 18.100 through 18.180 Downtown Design Guidelines, Sections 18.105.020.

18.16.080 Specific development standards

The following requirements apply to "A and/or B" Streets.

- Setbacks (A)
- Pedestrian Plaza (A)
- Minimum Street Wall Height (A)
- Awnings, Marquees, and Canopies (A)
- Blank Wall Limitations (A & B)

Primary Building Entrance (A & B)
 Surface Parking (A)
 Street Frontage - Retail and Service Use (A)
 Parking Structure Design (A & B)

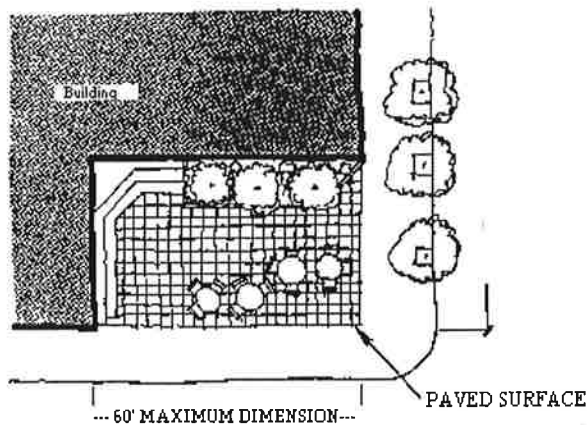
A. Maximum Setbacks ("A" Streets).

1. Intent. Enclose and define the street space. Place building, walls, and design height and massing that will:
 - a. Provide human scaled street enclosure and building edge continuity on key downtown pedestrian-oriented streets.
 - b. Contribute to a continuous building edge on lots adjacent to designated pedestrian streets.
 - c. Increase liveliness on the street by making physical and visual contact between interior building activities and the street.
2. Maximum Setback.
 - a. The maximum street wall setback is ten (10) feet.
 - b. Street wall setback areas (the area between the front property line and street wall):
 - i. May be used for landscaping and small commercial uses designed primarily to cater to pedestrians, including, but not limited to vendors, newsstands, flowers, and cafes.
 - ii. May not be used for fences, large trees and landscaping, or other features which form visual barriers or block views to street wall windows.
 - c. Exceptions to Maximum Setbacks.
 - i. The street wall may be set back to retain water views and to provide transition to residential neighborhoods, to allow privacy in residential development, to meet centerline setback requirements, for building entrances, for pedestrian plazas, and to allow existing setback buildings as conforming uses.
 - ii. Building Entrances Allowance. Large entryways which are integral to a building design may be set back more than ten (10) feet.
 - iii. Pedestrian Plazas.

B. Pedestrian Plazas ("A" Streets).

1. Intent. When proposed, plazas will provide small spaces which will attract concentrations of people to gather along the most intensively developed streets ~~in downtown~~. Buildings will be grouped around small pedestrian plazas; will be carefully located and integrated into the streetscape or building; will make the most of light and sun exposure; and, when proposed, will integrate landscaping and/or public art into the design.
2. Pedestrian Plaza Requirements. Pedestrian plaza designs shall meet the following development standards:
 - a. Size and dimension. The maximum pedestrian plaza dimension shall be sixty (60) feet across (see Figure 16-2).
 - b. Access. The surface of all pedestrian plazas shall be visually and physically accessible from the public rights-of-way. Allowances may be made on sites with steep topography.
 - c. Surface. Paved walking surfaces shall be provided.

- d. Landscaping and/or Public Art.
 - i. Required landscaping and/or public art. At least ten (10) percent of the plaza area shall be landscaped with living plants or use public art incorporated into the plaza to provide texture, breakup unadorned expanses of hard surfaces, and add human scale interest to the space.
 - ii. Landscaping. The landscaping shall be planted and maintained according to the City of Olympia development standards (Section 18.38).
 - iii. Location of landscaping and/or public art. Landscaping and/or public art shall not block pedestrian views into the plaza.
- e. Seating.
 - i. Required seating. Seating shall be provided in all pedestrian plazas.
 - ii. Allowed seating walls and steps. Tops of walls and steps may be considered seating.
- f. Exposure to sunlight. Southern locations are encouraged to allow direct sunlight to enter the space and strike the plaza floor.
 - i. Required sunlight. Pedestrian plazas shall be designed to allow measurable direct sunlight to enter the plaza and shall not be blocked by landscaping or art.



- PLAZA MUST BE ENCLOSED ON AT LEAST TWO SIDES
- 30% OF ENCLOSURE WALLS MUST BE OCCUPIED BY PEDESTRIAN-ORIENTED USE
- 10% OF PLAZA AREA MUST BE LANDSCAPED AND/OR USE PUBLIC ART INCORPORATED INTO THE PLAZA TO PROVIDE TEXTURE, BREAK UP UNADORNED EXPANSES OF HARD SURFACES, AND ADD HUMAN SCALE INTEREST TO THE SPACE

Required Pedestrian Plaza Conditions
FIGURE 16-2

- g. Plaza edges.
 - i. Plaza enclosure. All pedestrian plazas shall be enclosed on at least two sides by a structure or by landscaping and/or art which creates a wall-like effect.
 - ii. Prohibited edge conditions.
 - (a) Unscreened parking lots, chain link fences, barbed wire, and other inhibiting conditions are prohibited adjacent to pedestrian plazas.
 - (b) Blank walls in pedestrian plazas are subject to the blank wall limitation standards.
- h. Uses in pedestrian plazas.
 - i. Permitted uses. The following uses are permitted: playground equipment, fountains, waterfalls, pools, sculptures, works of art, arbors, trellises, benches, trees, planting beds, trash receptacles, drinking fountains, bicycle racks, open air cafes, kiosks, vending carts, outdoor furniture, lighting, flagpoles, public telephones, temporary exhibits, canopies, awnings, and similar uses which encourage pedestrian use of these spaces.
 - ii. Allowed motor vehicle use. Motor vehicle use of pedestrian plazas for passenger drop off and pick up at a building entrance may be allowed. All other loading or motor vehicle access is prohibited.
- i. Exceptions. The Director of Community Planning & Development or his or her delegate may grant exceptions to the Pedestrian Plaza Standards if the proposed design meets the intent of this Section and Subsection.

C. Minimum Street Wall Height Requirement ("A" Streets).

- 1. Intent. To ensure spatial enclosure on the street and achieve dense land use in the downtown.
- 2. The minimum street wall height is sixteen (16) feet above the average sidewalk elevation.

D. Awnings, Marquees or Canopies, and Arcades ("A" Streets). Awnings, marquees, canopies, and other projections are allowed to project into the ROW. See Downtown Building Design Guidelines - Projections into the ROW. These provisions apply to all Pedestrian-Oriented Streets in the City, even when not located in the downtown.

- 1. Intent. Provide pedestrians rain protection, contribute to overall integration of individual buildings within the streetscape, and help define the pedestrian zone. When awnings are used, they should cover the pedestrian clear zone.
- 2. Awnings, Marquees or Canopies, and Arcade Requirements.
 - a. Coverage allowed in public rights-of-way. Awnings, canopies, or marquees may project into the public rights-of-way (see Downtown Building Design Guidelines - Projections Into the ROW). Arcades must be on private property.
 - b. Types of material allowed. Awnings, marquees, and canopies must be fabric on a retractable frame, metal, glass, or plexiglass. On historic buildings, wood may be used.
 - c. Area and coverage requirements. Awnings, marquees, and arcades shall be provided along the street wall, or that portion of the street wall that abuts or is parallel to the sidewalk. The maximum depth (projection from street wall) for awnings and marquees or canopies is regulated in the applicable Section of the Uniform Building Code. Awnings,

marquees, or canopies should extend to the maximum depth allowed by the UBC, with allowance for street tree and street light clearance. (See Figure 16-4 of this Chapter.)

d. Height requirements. Except valances, the lower edge of all awnings, marquees, canopies, and arcades must be between the heights of eight (8) and twelve (12) feet above finished grade. Every attempt should be made to make awnings of like heights on a given block.

e. Historic buildings. Historic buildings may be reviewed for exceptions to these standards. Exceptions shall be jointly reviewed by the Design Review Board and Heritage Commission, called the Joint Review Committee or the Heritage Commission and Design Review staff. (See Chapter 18.12 OMC)

3. Prohibited Awnings. Back-lit awnings or canopies are prohibited.

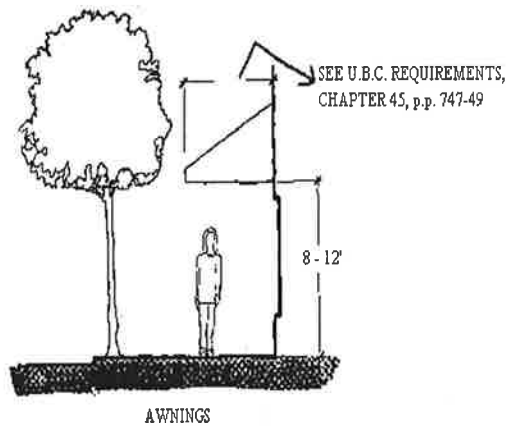
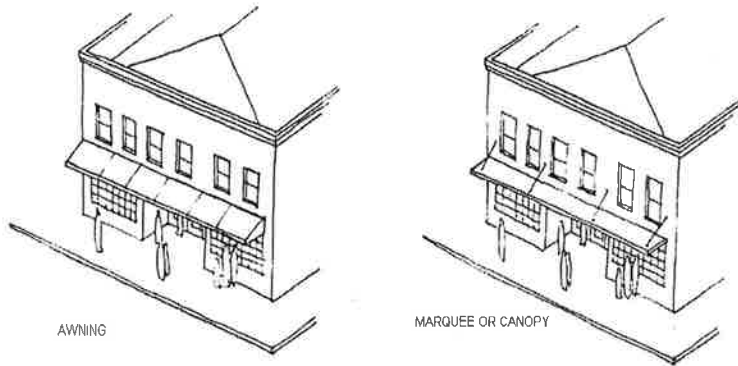


FIGURE 16-4



ARCADE



AWNING

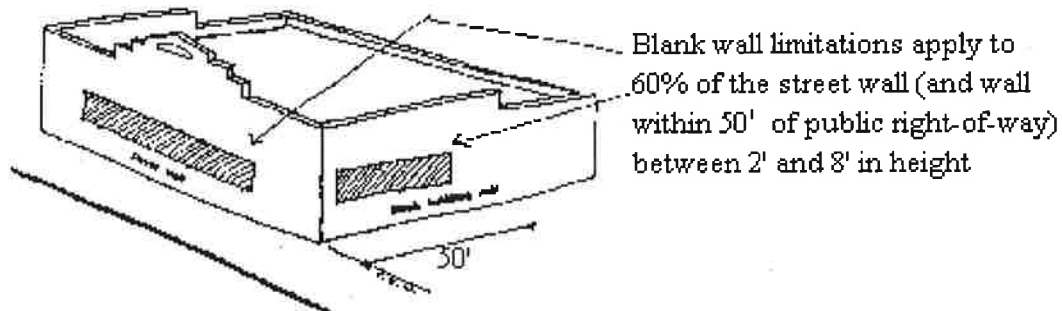
MARQUEE OR CANOPY

FIGURE 16-5

F. Blank Wall Limitation ("A" and "B" Streets).

1. Intent. Reduce blank wall impacts on the pedestrian and business district environment. Provide varied, pedestrian-friendly building facades and sidewalk activities. Avoid the creation of blank walls and dull facades that deaden the surrounding space and create an uninviting street environment.

2. Development Standards. Blank walls are not allowed adjacent to or within fifty (50) feet of a pedestrian street right-of-way or a public park. At least sixty percent (60%) of these walls between two (2) feet and eight (8) feet in height must be pedestrian friendly. Pedestrian-friendly facades shall have one or more of the following characteristics:



BLANK WALL LIMITATIONS: AREAS REGULATED

FIGURE 16-6

- a. Transparent windows. Transparent window area or display windows which provide visibility into building interiors. The following transparency standards apply.
 - i. Glass must be clear or lightly tinted in windows, doors, and displays. Because it does not provide visibility between the street and building interiors, reflective or opaque or painted glass is prohibited.
 - ii. Doors and entry windows must be transparent to meet this requirement. If they are not, they shall be considered blank walls.
 - b. Art or architectural treatment. Sculpture, mosaic, glass block, opaque art glass as relief artwork, or similar features of visual interest. Structural architectural elements may be acceptable as an administrative waiver if the design meets the intent of this Section.
 - c. Vertical trellis. A permanent vertical trellis in front of the wall with climbing plants or plant materials.
 - d. Pedestrian plazas. Pedestrian plazas may meet this requirement if the design complies with Section 18.16.080(A), Maximum Setbacks Allowed.
 - e. Display windows. Display windows may be used to meet this requirement.
3. Retaining Walls. Retaining walls on Pedestrian Streets shall be considered blank walls and are subject to the regulations in this Section.
4. Exceptions. Where this Section is in conflict with the Uniform Fire Code, the Uniform Fire Code shall govern.
5. Approval. All proposed blank wall treatments are subject to City approval. The applicant must submit architectural plans and elevations showing proposed treatments for approval.
- G. Primary Building Entrance ("A" and "B" Streets).
- 1. Intent. Allow people to arrive on foot, by transit, or by other means (in addition to by car), and to increase pedestrian and street activity. Create a prominent entry that conveys a clear sense of arrival and that uses high quality products that contribute to the richness and detail of the facade.
 - 2. Primary Building Entrance Requirements.
 - a. The primary entrance to all buildings must face the street.
 - b. All primary building entrances must be clearly visible from the sidewalk.
 - c. Direct access shall be provided either:
 - i. From the sidewalk if the building facade is adjacent to the sidewalk; or
 - ii. From a pedestrian plaza if the building facade is not directly adjacent to the sidewalk.
 - d. For properties west of Yauger Way, the Primary Building Entrance Requirements may be altered upon approval of a development agreement if it is determined that an entry from a location other than the wall facing a "B" street provides equal or better building orientation and human scaled design elements to integrate into the mixed use neighborhood.
- H. Surface Parking Lots ("A" Streets).

1. Intent. Prohibit the disruption of the continuous building edge along the street.
 2. Surface Parking Lot Requirements. Surface parking lots are prohibited on lots abutting pedestrian streets, except when they are located behind buildings.
 3. Exception. Properties west of Water Street and east of Budd Inlet, and properties west of Yauger Way ~~in this District~~ may have surface parking directly abutting twenty (20) percent of the street frontage.
- I. Street Frontage - Active Street Edge Requirements ("A" Streets).
1. Intent. Provide interest and activity that contribute to the pedestrian streets, reinforce existing service business uses (retail, office, commercial), and establish new activity along the main pedestrian links between downtown and the surrounding neighborhoods and around other pedestrian oriented streets designated west of Yauger Way (see Figure 16-1b).
 2. Development Standards. Seventy-five (75) percent of the street frontage will look like a storefront by including one or more of the following: direct at-grade sidewalk entries, high ceilings, recessed doors, storefront windows, awnings, canopies, large room spaces, and similar features in the building. Building entries and lobbies along the street wall are exempt from the street level use requirement calculations;
 3. Exemption.
 - a. Structured parking or that portion of a project used as structured parking are not subject to this requirement. See Section J below for structured parking requirements.
 - b. See 18.16.060(1)(B)(1) for residential experience.
- J. Parking Structure Design ("A" and "B" Streets).
1. Intent. Design structured parking in a way that will fit the real and intended human scale of downtown and on other designated pedestrian oriented streets. Acknowledge that while structured parking provides convenient, less intrusive, less land consumptive parking than surface parking, the buildings themselves can be intrusive, since they are often large, monolithic structures with few refinements and little interest or activity.
 2. Street Level Treatment. Retain the ~~downtown~~ pedestrian oriented scale and character on structured parking and ground level covered parking facing a pedestrian street by breaking up horizontal openings and incorporating at least one of the following techniques:
 - a. Provide at least one (1) of the five (5) pedestrian-friendly facade techniques in Section 18.16.080(F), Blank Wall Limitations. Transparent windows shall be permitted only where pedestrian-oriented uses or businesses are located in a parking structure.
 - b. Provide enclosed occupiable space for "pedestrian-oriented uses or businesses" at least ten (10) feet in depth along all of the building frontage facing the Pedestrian Street, excluding vehicle and pedestrian entries. If there are multiple pedestrian street frontages, this option should be used on the dominant pedestrian street frontage.
 - c. Incorporate public art into the design or use decorative metal grille work or similar architectural detailing which provides texture and covers the parking structure opening. EXCEPTION: Parallel vertical bars (resembling a jail cell pattern or HVAC intake grills) are prohibited, and do not meet the intent of this requirement.
 3. Parking structures over one-half block in size must provide enclosed occupiable space for "pedestrian-oriented uses or businesses" at least ten (10) feet in depth along the dominant

pedestrian street frontage of the building facing the pedestrian street, excluding vehicle and pedestrian entries.

4. Vehicle Entry. The width of garage doors shall be limited to the width of the driveway plus five (5) feet. Vehicular entries shall be recessed at least six (6) feet from the primary facade plane in order to minimize their prominence in the facade.

5. Upper Level Treatment. Assure that upper levels of parking structures remain compatible with downtown-pedestrian oriented scale and character. Break up horizontal openings by:

- a. Providing relief, detail, and variation on the facade by employing well-proportioned openings that are designed to create shade and shadow detail.
- b. Public art, decorative metal grille work or similar art or architectural detailing which provides texture and covers the opening in the facade.
 - i. Parallel vertical bars (resembling a jail cell pattern or HVAC intake grilles) are prohibited, and do not meet the intent of this requirement.

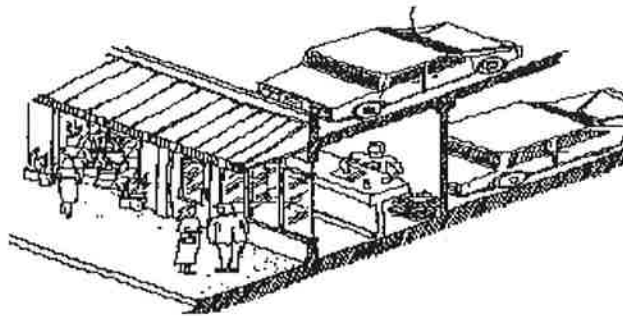
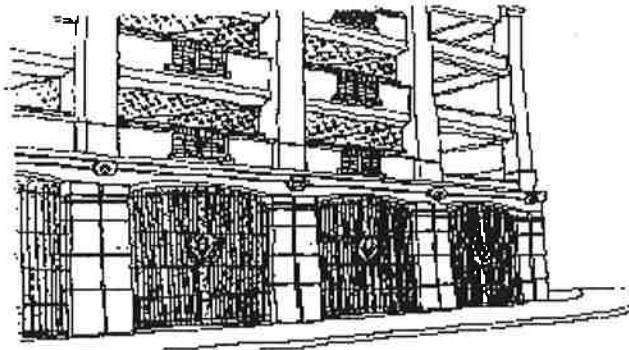


FIGURE 16-7



Parking garage facade treated with decorative grill work.

FIGURE 16-8

Chapter 18.100
DESIGN REVIEW

18.100.000 Chapter Contents

Sections:

- 18.100.020 Statement of Policy.
- 18.100.040 Purpose.
- 18.100.060 Projects Subject to Design Review.
- 18.100.080 Design Review Districts and Corridors.
- 18.100.090 Design Review Process.
- 18.100.100 How to Use Design Criteria.
- 18.100.110 Submission Requirements.

18.100.020 Statement of policy

The City Council finds that new development can have a substantial impact on the character of the area in which it is located. Some harmful effects of one land use upon another can be prevented through zoning, subdivision regulations and building codes. Other aspects of development are more subtle and less amenable to exacting rules promulgated without regard to specific development proposals. Among these are: the general form of the land before and after development, the spatial relationships of the structures and open spaces on adjacent land uses, and the appearance of buildings, signs and open spaces. Such matters require the timely exercise of judgment in the public interest by people qualified to evaluate the design of a new development.

18.100.040 Purpose

The purposes of design review are:

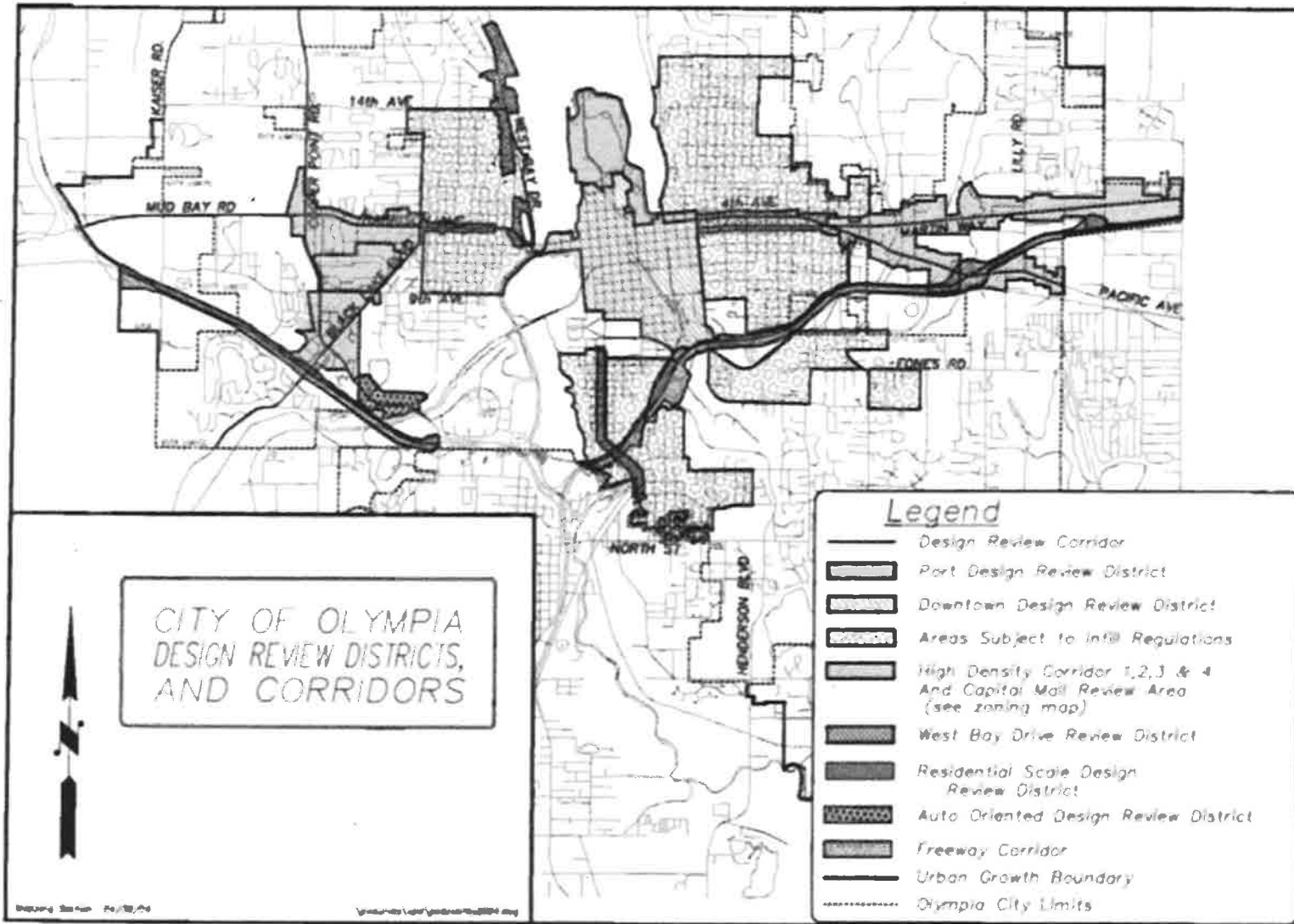
- A. To promote those qualities in the natural environment which bring value to the community;
- B. To foster the attractiveness and functional utility of the community as a place to live and work;
- C. To preserve the special character and quality of Olympia by maintaining the integrity of those areas which have a discernible character or are of special historic significance;
- D. To raise the level of community expectations for the quality of the built environment;
- E. To encourage originality and creativity in site planning and architecture;
- F. To communicate these purposes to the applicant and to assist the applicant in achieving these purposes;
- G. To preserve and enhance property value;
- H. To ensure that new developments maintain or improve neighborhood character and livability; and
- I. To consider the applicants' needs and goals and the broader public impact of any proposal.

18.100.060 Projects Subject to Design Review

- A. The following projects are subject to design review:
 1. Projects within designated design review districts and corridors, as shown on the Official Design Review Map (See OMC Section 18.100.080);
 2. Commercial projects adjacent to residential zones;
 3. Commercial or residential projects for Heritage Register properties or those within an historic district;
 4. Projects with a building area greater than 5000 square feet that require a Conditional Use Permit in a residential zone;
 5. Multifamily projects;

6. Single family housing, including designated manufactured homes, on lots less than 5000 square feet or on substandard lots;
7. Dwellings proposed on lots within the area depicted on Figure 4-2a, "Areas Subject to Infill Regulations";
8. Master Planned Developments;
9. Manufactured housing parks;
10. Duplexes, triplexes, fourplexes, townhouses, accessory dwelling units, and cottage housing;
11. All projects within scenic vistas as identified on the official maps of the City (See OMC Section 18.100.110); and,
12. Signs within designated design review districts and corridors or associated with a project that is subject to design review.

B. Exceptions: Evergreen Park PUD; interior work; and projects which do not affect the character, use, or development of the surrounding properties, or the architectural character of the structure, such as fences, parking lots with less than twenty (20) spaces or minor repair or renovation of, or minor additions to, existing structures.

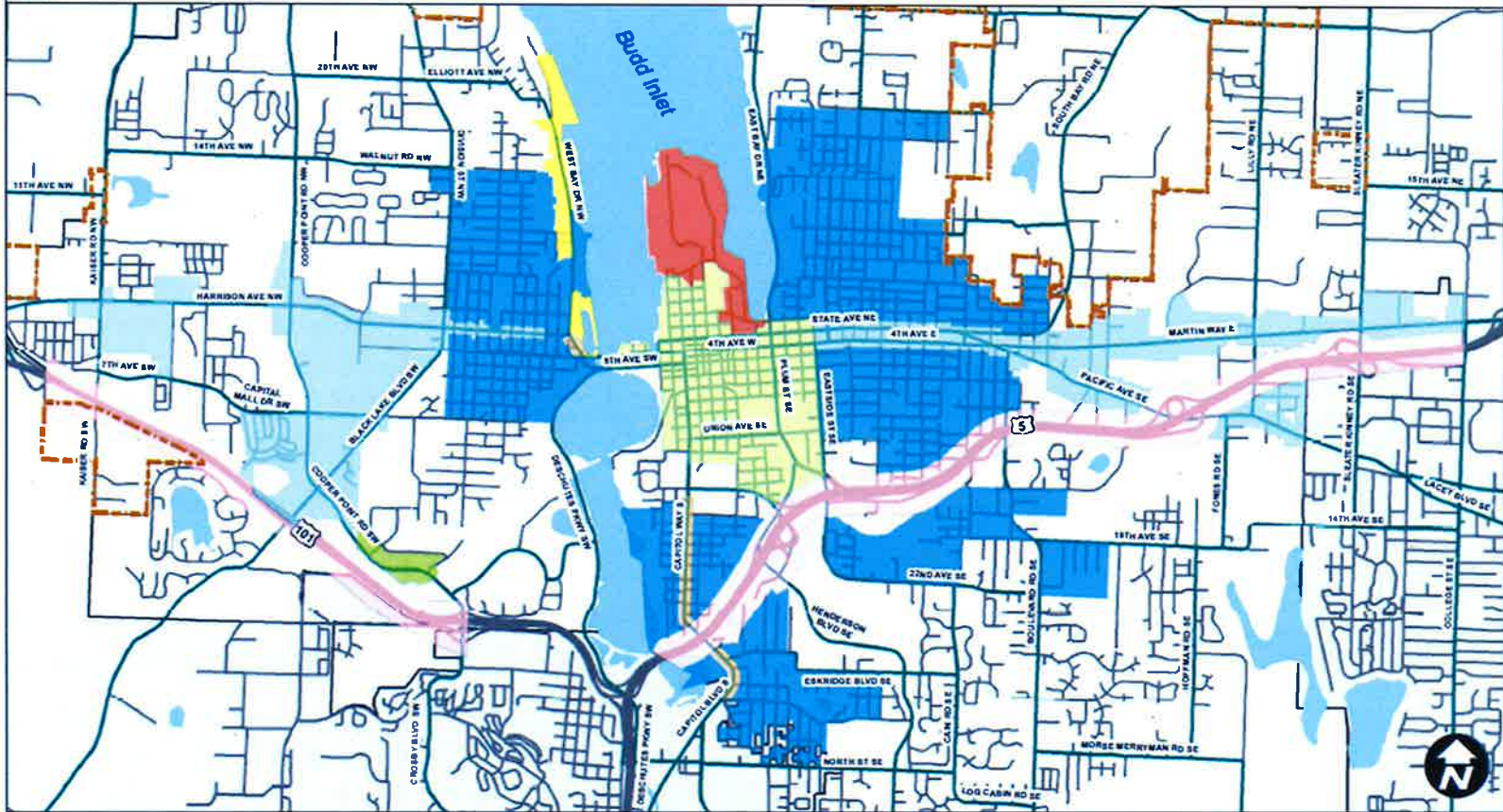




Design Review Districts and Corridors

Proposed Addition - Kaiser-Harrison Opportunity Area - **DRAFT**

Date Published: 5/25/2016



Design Review Districts and Corridors

- | | | |
|-------------------------|----------------------------|--------------------|
| Auto Oriented District | High Density Corridor | Infill Regulations |
| Downtown District | Freeway Corridor | Urban Growth Area |
| Port Design District | Design Review Corridor | City Limits |
| West Bay Drive District | Residential Scale Corridor | |

0 0.5 1 Miles 1 inch = 3,756 feet

The City of Olympia and its personnel cannot assure the accuracy, completeness, reliability, or suitability of this information for any particular purpose. The parcel, right-of-ways, utilities and structures depicted herein are based on record information and aerial photos only. It is recommended the recipient and/or user field verify all information prior to use. The use of this data for purposes other than those for which they were created may yield inaccurate or misleading results. The recipient may not assert any proprietary rights to this information. The City of Olympia and its personnel neither accept or assume liability or responsibility, whatsoever, for any activity involving this information with respect to lost profits, lost savings or any other consequential damages.

This map is intended for 8.5x11" landscape printing.

For more information, please contact: Joyce Phillips, Senior Planner | jphillip@ci.olympia.wa.us | (360)570-3722

File name and path: C:\work\44\25\Map_AutoOrientedArea\Harrison Opportunity Area\2016\525\Map_AutoOrientedArea.mxd

EXHIBIT G

18.100.080 Design Review Districts and Corridors

The following districts and corridors are hereby established as Design Review Districts and Corridors: Downtown, Port Peninsula, High Density Corridor 1, 2, 3, and 4 (HDC), High Density Corridor-4 Capital Mall Area, Freeway Corridor, West Bay Drive, Residential Scale, Auto Oriented, and Infill Districts. Said districts and corridors are defined on the "Official Design Review Districts and Corridors Map," on file with the City Clerk and generally described in the illustration below. In the event of a conflict between this illustration and the official map, the official map shall govern.

18.100.090 Design Review Process

All projects subject to design review shall be reviewed either by the Design Review Board (DRB), the Joint Review Committee (JRC) or by staff as noted below. The DRB, JRC, or staff shall provide a recommendation to the review authority. The review authority shall give substantial weight to the recommendation of the DRB or JRC. (A project reviewed by the JRC shall not also be reviewed by the Heritage Review Committee.)

A. Projects subject to review by the Design Review Board or Joint Design Review Committee, as described in OMC 18.76.180:

1. Any proposed development project located within the following design districts:
 - a. Downtown District; and,
 - b. Residential Scale Design District.
2. Any proposed building development over 5,000 square feet in gross floor area, located within the following design districts and corridors:
 - a. High Density Corridors;
 - b. West Bay Drive District;
 - c. Auto Mall District;
 - d. Port Peninsula District;
 - e. Freeway Corridor; and,
 - f. Design Review Corridors.
3. Any multifamily building with five (5) units or more, any townhouse building with five (5) units or more, and any multifamily development with twenty (20) units or more.
4. All Master Planned Developments.
5. Projects with a building area greater than 5,000 square feet that require a Conditional Use Permit in a residential zone.

B. Projects subject to review by Staff:

1. Single family dwellings on lots within the area depicted on Figure 4-2a.
2. Minor additions or alterations to residential or commercial projects on a Heritage Register or within a Historic District.
3. Any proposed development of 5,000 square feet or less in gross floor area, and signs in the following design districts and corridors:
 - a. High Density Corridors;
 - b. West Bay Drive District;
 - c. Auto Mall District;
 - d. Port Peninsula District;
 - e. Freeway Corridor; and,

f. Design Review Corridors.

4. Any other project subject to design review not described in "A" above.

18.100.100 How to Use Design Criteria

- A. Requirements and Guidelines. Each section of the design review chapters consists of a requirement and possibly several guidelines. Compliance with each requirement is necessary; the guidelines provide methods to achieve compliance with the requirement. Applicable guidelines must be incorporated into the project design except in cases where proposed design solutions are considered by the Board and/or staff to be equal to or better than the guidelines, and meet the intent of the requirement. If the project does not conform to the design criteria, the Board and/or staff will indicate the requirements that have not been met.
- B. Illustrations. Illustrations contained within specific criteria sections are intended to be in harmony with the text of the same section. In the event of conflict between text and an illustration, the text shall govern.
- C. Design Review Criteria Chapters. Design criteria are separated into chapters, depending on whether the development is commercial or residential, and the location or type of that development, as outlined below.

18.105 Historic Structures and Buildings within the Historic Districts. This Chapter applies to structures listed on the Olympia Heritage Register, Washington Heritage Register, and the National Register of Historic Places, and all structures within a Historic District.

18.110 Basic Commercial Design Criteria. This chapter applies to all commercial projects throughout the City that require design review, in addition to the district-specific requirements found in the following chapters, as applicable. It also applies to projects with a building area greater than 5,000 square feet in gross floor area that require a Conditional Use Permit in a residential zone, to commercial projects adjacent to residential buildings, to commercial or residential projects. The design districts are shown on the Official Design Review Districts and Corridors.

18.120 Commercial Design Criteria - Downtown District. This chapter applies to all commercial projects that require design review that are located in the Downtown Design Review District. In addition, commercial projects in the Downtown Design District may also be subject to the requirements of Chapter 18.16, Pedestrian Streets.

18.130 Commercial Design Criteria - High Density Corridors. This chapter applies to all projects that require design review that are located in the HDC districts.

18.135 Commercial Design Criteria - Residential Scale District. This chapter applies to all projects that require design review that are located in the Residential Scale District.

18.140 Commercial Design Criteria - Auto Oriented District. This chapter applies to all projects that require design review that are located in the Auto Oriented District.

18.145 Commercial Design Criteria - Freeway Corridor. This chapter applies to all projects that require design review that are located in the Freeway Corridor District.

18.150 Commercial Design Criteria - Port Peninsula. This chapter applies to all projects that require design review that are located in the Port Peninsula design review district. This chapter contains the only design criteria that apply to the Port Peninsula.

18.155 Commercial Design Criteria - West Bay Drive District. This chapter applies to all projects that require design review that are located in the West Bay Drive District.

18.170 Residential Design Criteria - Multifamily. Design criteria contained in this chapter (Sections 18.170.030 - 18.170.160) apply to all multifamily residential buildings with five or more units and any multifamily development with twenty (20) units or more throughout the city. Projects of this type and size are reviewed by the Design Review Board.

18.175 Residential Design Criteria - Infill and other residential. Sections 18.175.020 through 18.175.060 of this chapter apply to single-family dwellings, including designated manufactured housing, proposed on lots within the area depicted on Figure 4-2a, on lots less than 5000 square feet, or on substandard lots, duplexes, triplexes, fourplexes, and townhouse buildings of four (4) units or less throughout the city. Sections 18.175.080 and 18.175.090 apply to accessory dwelling units throughout the city. Section 18.175.100 applies to cottage development.

18.180 Residential Design Criteria - Manufactured home parks. This chapter applies only to manufactured home parks.

18.100.110 Submission Requirements

- A. Conceptual Design Review. Applications for review of the Conceptual Design shall be filed with the Department as part of a land use review application on forms provided by the Department.
- B. Detailed Design Review. Applications for review of the Detailed Design shall be submitted to the Department prior to or at the same time as a building permit application on forms provided by the Department.
- C. Combined Conceptual & Detailed Design Review. For those projects which have had a presubmission conference, do not have any apparent environmental issues, (such as those listed in the Critical Areas Ordinance, Title 14.10, or transportation issues), and which do not require a public hearing, upon request by the applicant and with the concurrence of staff, the Design Review Board may conduct both Conceptual and Detailed Design Review of a project at the time of Land Use Review.
- D. Time Limitations. Except in cases where the applicant agrees to an extension of time, the Design Review Board shall have a public meeting on an application for a major project within thirty (30) days after an application is deemed complete pursuant to OMC 18.72.

Chapter 18.130
COMMERCIAL DESIGN CRITERIA
HIGH DENSITY CORRIDOR (HDC)

18.130.000 Chapter Contents

Sections:

- 18.130.020 Building orientation.
- 18.130.030 Building design.
- 18.130.040 Surface parking.
- 18.130.050 Historic building types - HDC 1 & 2.
- 18.130.060 HDC 4 Capital Mall - Incremental expansion.

18.130.020 Building Orientation

A. REQUIREMENTS:

1. Primary building entries, storefront windows, and building facades shall face the street, create a continuous row of storefronts along the street frontage, and provide direct access from the street to the building with close pedestrian access to the nearest bus stop. In the case of multifamily housing or townhouse projects, stoops and porches or distinctive entryways can substitute for commercial storefront window openings.
2. In the case of buildings at corners of street intersections, this requirement applies to both street frontages. An entrance at the corner would meet this requirement.
3. In General Commercial and HDC-4 zones west of Yauger Way, single story or single use commercial retail space shall not occupy more than 60,000 square feet of enclosed building space on the ground floor, unless a development agreement is approved. These buildings shall be designed and oriented to provide for pedestrian and bicycle circulation throughout the site and to adjacent buildings and properties. A building larger than 60,000 square feet can be allowed when it is not directly adjacent to a street designated as an "A" street in the Pedestrian Street Overlay and if a development agreement is approved that at a minimum addresses:
 - 1) Building orientation, massing, and use of high quality materials
 - 2) Parking is located to the rear or side of the building, or is separated from the street by additional retail buildings
 - 3) Pedestrian, bicycle, and vehicular circulation on site and connections to adjacent properties
 - 4) Community assets, such as the multi-use trail identified in the Kaiser Harrison Opportunity Area Plan
4. For HDC zones west of Yauger Way, where a local transit route does not exist, meet with the local public transit provide to ascertain the viability of developing a bus stop for the site. Provide improvements for a future bus shelter if the future route and bus stop locations are generally known and applicable to the project site. The improvements shall be constructed to City and transit provider standards and sized to allow for the future installation of a transit shelter and related appurtenances, such as transit signage and refuse collection.
5. HDC-4 exceptions: The following exceptions apply if excellent pedestrian connections to the HDC street are provided:

- a. Building entries need not face the HDC street edge but may be located on the corner, off a publicly accessible pedestrian plaza or on the side of the building. All other applicable design guidelines apply.
- b. Hotels and multifamily developments may be set back greater than 10 feet provided that the setback area does not include parking.
- c. Setbacks may be increased where street improvements do not exist or where they do not provide the separation from vehicles as described in the Development Guidelines and Public Works Standards (Street Standards) to include the additional distance required by those standards for sidewalk and planter strips. The setback area may not include parking.

B. GUIDELINES:

1. Locate service and delivery entries in the rear whenever possible.
2. On transit routes, site design should accommodate transit bus stop zones as may be needed, including:
 - a. in-lane bus stop(s) or possible stop pullout(s),
 - b. ADA accessible pedestrian pathways,
 - c. shelter and/or provision for cover to help reduce pedestrian exposure to the elements, and
 - d. other amenities such as such as lighting, benches, or information kiosks.
3. Where a transit route does not exist, meet with the local public transit provider to ascertain the viability of developing a bus stop for the site.

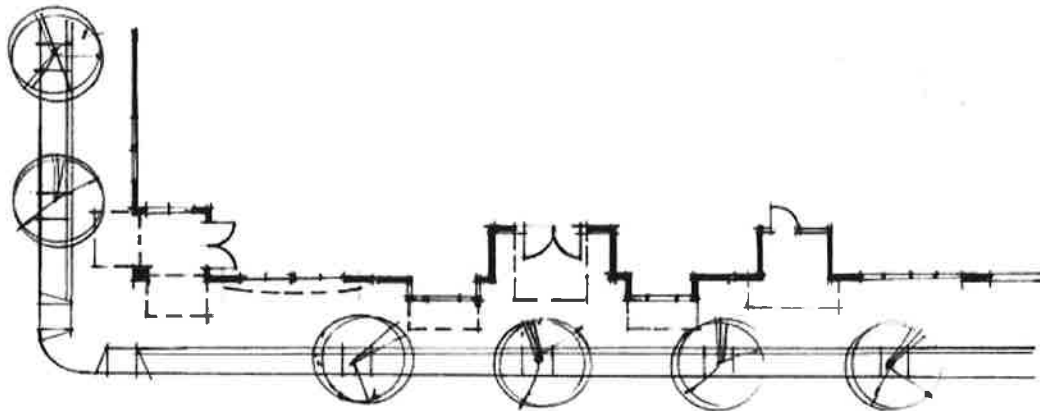
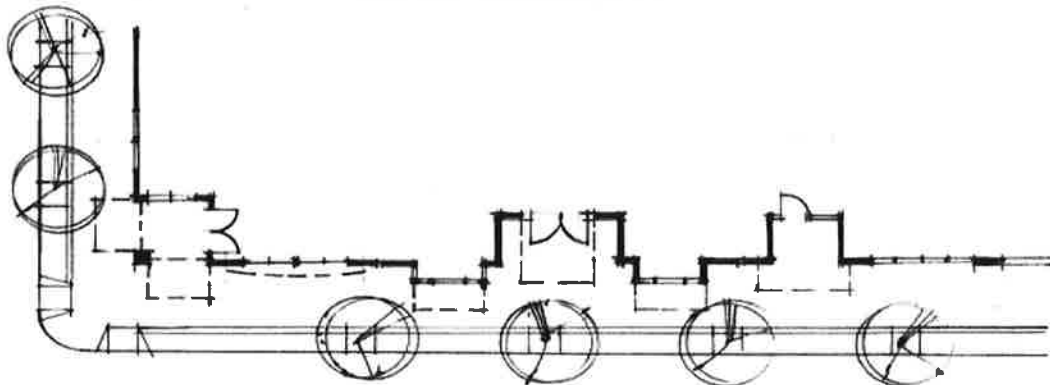


FIGURE 18.130.020-A



FIGURE 18.130.020-B**18.130.030 Building Design**

- A. **REQUIREMENT:** Where visible to pedestrians, walls over thirty (30) feet in length shall be modulated and building elements shall be articulated. Orient buildings and locate windows to provide privacy, to the extent practical, both within the project and to the adjacent residential neighborhood.
- B. **GUIDELINES:**
1. Provide vertical and horizontal modulation in the facade;
 2. Provide features that identify a clear base to the building;
 3. Provide recessed entries;
 4. Provide awnings, canopies, marquees, building overhangs, or similar forms of pedestrian weather protection. Such elements should be at least four and one half (4-1/2) feet wide and cover at least eighty (80) percent of the length of buildings, including entries, which abut a pedestrian walkway, subject to Section 18.06.080, Commercial Development Standards.
 5. Use fenestration;
 6. Vary roof lines; use dormers;
 7. Provide porches or balconies.

**FIGURE 18.130.030-A****FIGURE 18.130.030-B**

18.130.040 Surface Parking

- A. REQUIREMENT: Locate parking behind or on the side of buildings. On corner lots, parking shall not be located on the corner. Minimize curb cuts and provide landscaping, berms and/or low walls for screening.

(See also Section 18.38.200, Parking Facility Location.)

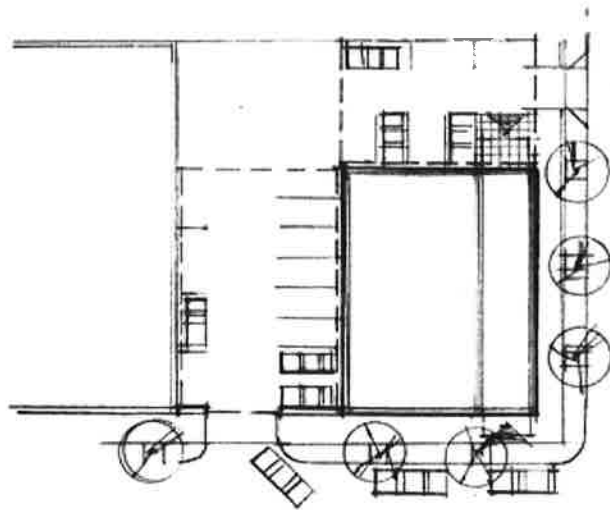
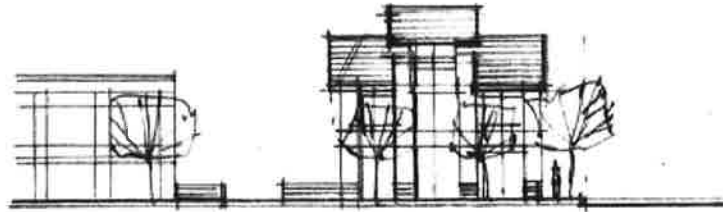


FIGURE 18.130.040-A

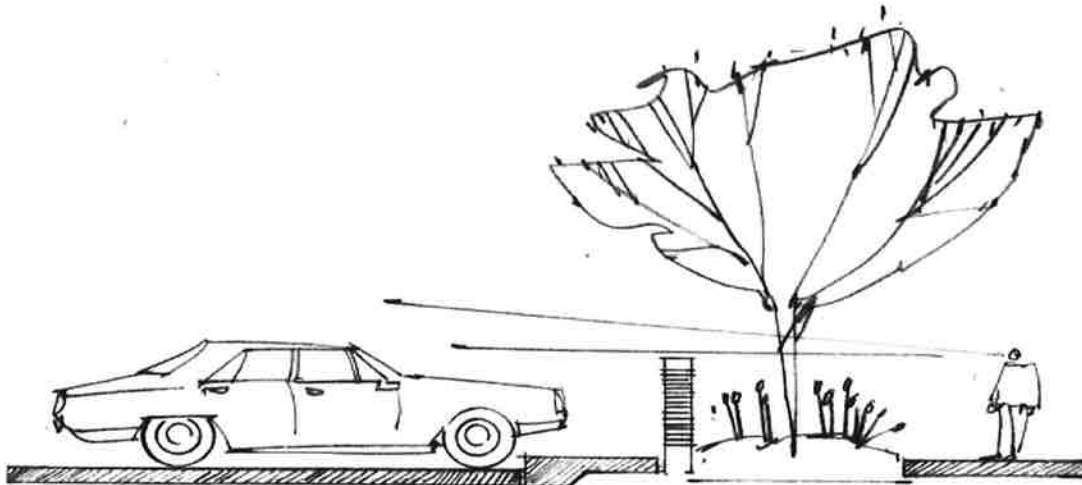


FIGURE 18.130.040-B

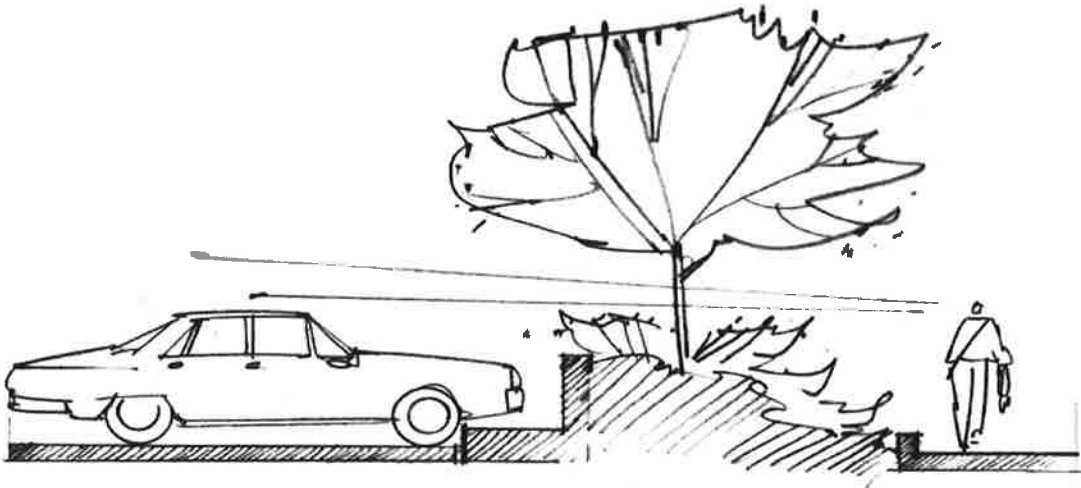


FIGURE 18.130.040-C

18.130.050 Historic Building Types - HDC 1 and HDC 2

- A. **REQUIREMENT:** In HDC 1 and 2 districts, buildings shall include similar details to one of the historic building types as found on the corridor and in the adjacent neighborhoods. Orient buildings and locate windows to provide privacy, to the extent practical, both within the project and to the adjacent residential neighborhood. In the case of multifamily housing or townhouse projects, stoops and porches or distinctive entryways can substitute for commercial storefront window openings.
- B. **GUIDELINES:**
1. Craftsman design may include:
 - a. Wide pitched roofs with broad overhangs;
 - b. Visible structural detail such as rafter tails and knee brackets;
 - c. Heavy porch columns;
 - d. Deep covered porches;
 - e. Broad, horizontal lines.
 2. Vernacular design may include:
 - a. Gable roof;
 - b. Horizontal clapboard exterior material;
 - c. Vertical windows;
 - d. Minimal detailing.
 3. Tudor design may include:
 - a. Steeply pitched gabled or hipped roofs and cross-gables;
 - b. Stone, stucco or brick (sometimes with decorative patterns);
 - c. Arched doorways;
 - d. Tall, vertical proportions.

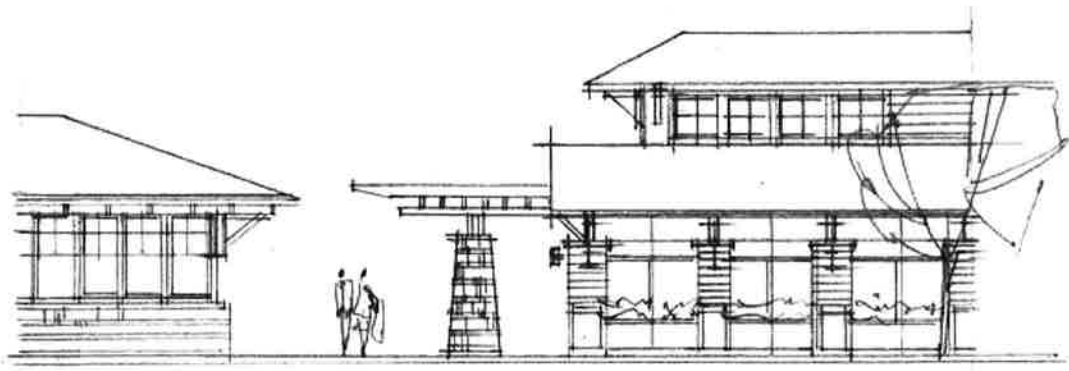


FIGURE 18.130.050-A (Craftsman)



FIGURE 18.130.050-B (Vernacular)

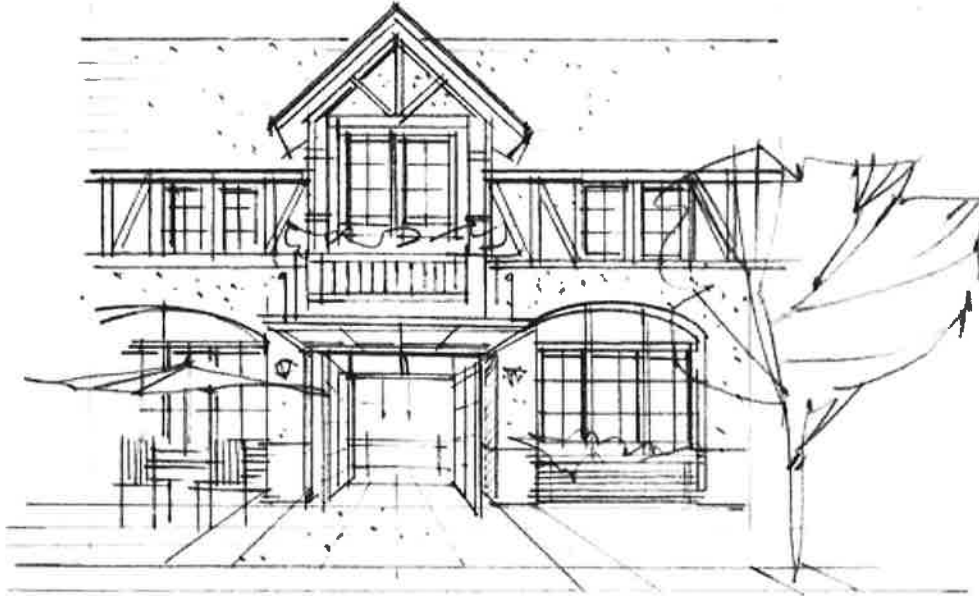


FIGURE 18.130.050-C (Tudor)

18.130.060 HDC 4-Capital Mall –Incremental Expansion

- A. Pedestrian Streets - 4th Avenue West and Kenyon Street. (Pedestrian Street Requirement does not apply to existing mall vehicular circulation.)
1. **Building Frontages.** Buildings are required to front the sidewalk and be a minimum of 20 feet in height. The building facades shall convey an urban character and include significant retail office storefronts at the ground level. (Building frontage requirement does not apply to below grade structures.) No less than 60% of the street-level building facade, between 2 feet and 8 feet above grade, shall be transparent storefront. Mixed-Use development, including residential and office uses, is encouraged on upper levels. Variations from the setback are allowed in order to encourage unique storefront building design features, and to create plazas. Canopies, arcades and/or colonnades shall be provided as overhead weather protection.
 2. **Interim Surface Parking Allowance.** Interim surface parking is allowed along 4th Avenue W and Kenyon Street as long as minimum 20 foot sidewalk / landscape area (see required sidewalks below) is provided. The area within 60 feet of the sidewalk/landscape link should be planned for future infill development. The planning of this area shall be reviewed by the Design Review Board for consistency with Pedestrian Street requirements, other design standards and street slope conditions along 4th Avenue W. and Kenyon Street.
 3. **Required Sidewalks.** 15 foot sidewalk with street trees when building faces street; (Staff note: Current major commercial collector street standards include a 10 foot sidewalk. Urban designers suggest that a wider sidewalk is necessary where a retail/commercial storefront is combined with a canopy and street tree.) 20 foot linkage when parking lot faces the street. The linkage will include a 10 foot sidewalk and a 10 foot landscaped planter. See street cross section illustrations. (This allows for development of full street improvements when buildings are developed that face the street edge.)
- B. **Building Focal Points.** A focal point, including tower elements and entryway features shall enhance street intersections, plaza areas and mall entry points. (Renovation of existing mall entries shall not

be required when minor renovations - such as a new coat of paint or new signage - or interior remodeling by tenant or anchor store are constructed within existing building envelope.) Renovated exterior entry and new mall entries shall include significant entry features, retail facades, display windows and streetscape elements to enhance the pedestrian experience, improve safety, and encourage walking between uses in the area.

- C. Transit Facility. A Transit stop or facility shall be integrated into the Capital Mall Area. It shall be located to provide a direct link to the shopping mall while supporting transit plans for the area. Transit stop or facility location can change as mall area expansion occurs over time. Transit routing through the site to the facility shall be designed to reduce conflicts with customer vehicles and parking. Transit stops will: a) be a short walking distance to the significant building entrance, b) be located at a safe and attractive location, c) be weather protected, d) include amenities such as benches and illumination, e) have adequate space to accommodate transit arrivals and departures and, f) be efficiently integrated into a transit route.
- D. Significant Building Entry. At the north side of the mall a significant entry shall be provided. (Development of this site feature is required when expansion on the north side of the Capital Mall Area meets or exceeds a cumulative total of 100,000 SF of new floor area). It is recommended that the Significant Building Entry be oriented toward Kenyon Street. The design of this entry shall include a tower element that is visible from intersection of Black Lake Blvd. and Capital Mall Drive. This element is allowed to extend up to 30 feet beyond the height limit. The feature tower shall be subject to Design Review Board approval based on design appropriateness and compatibility with overall shopping center renovation design.
- E. Urban Plaza. An urban plaza shall be located in the general area shown on the attached diagram. (Development of this site feature is required when expansion on the north side of the Capital Mall Area meets or exceeds a cumulative total of 100,000 SF of new floor area.) The plaza shall be integrated with a Significant Building Entry.
- F. Pedestrian Linkages. Increments of new development, including parking lot expansion, shall include landscaped illuminated walkways or paths that link the new mall development to surrounding commercial development, allowing people to walk or bike through parking areas safely and conveniently. (Pedestrian linkages shall be added and integrated into any parking lot expansion consistent with UDC Chapter 18.36 Landscaping and Screening regulations.) The intent is that pedestrian linkages and parking lot improvements be made incrementally as building improvements are made--not to require the entire mall parking lot to be improved at once but in proportion to the amount of mall area being developed.
- G. Site Access Points. These would create an urban block pattern along 4th Avenue by locating intersecting streets or pedestrian paths between buildings. (The Site Access Point may be connected to parking or internal circulation in the mall, but not impose block pattern to the mall as long as adequate vehicle and illuminated pedestrian connections are maintained to the surrounding High Density Corridor street network.) These shall be developed incrementally when buildings are added in the Capital Mall area and when 4th Avenue street improvements are made.

Capital Mall Redevelopment

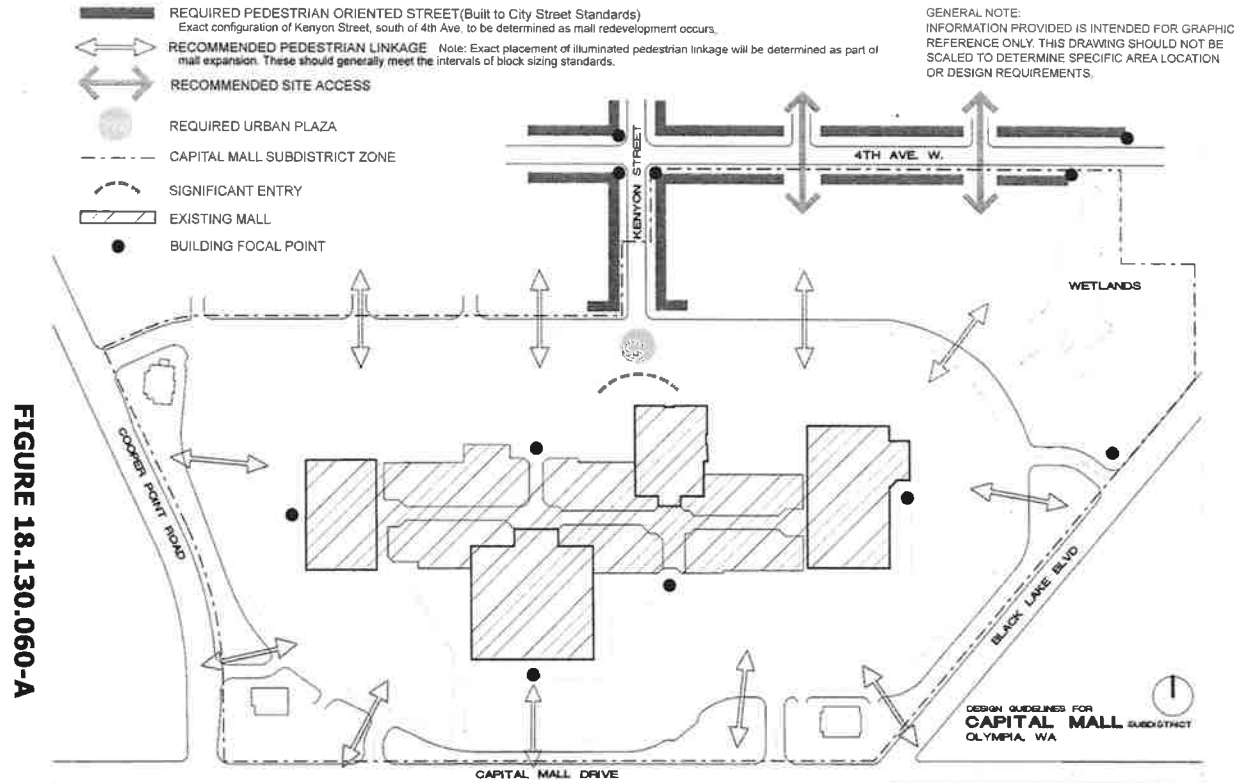


FIGURE 18.130.060-A

Note 1: The location of a transit stop/facility is required and will be determined at each development phase. See 18.06.251-A.3

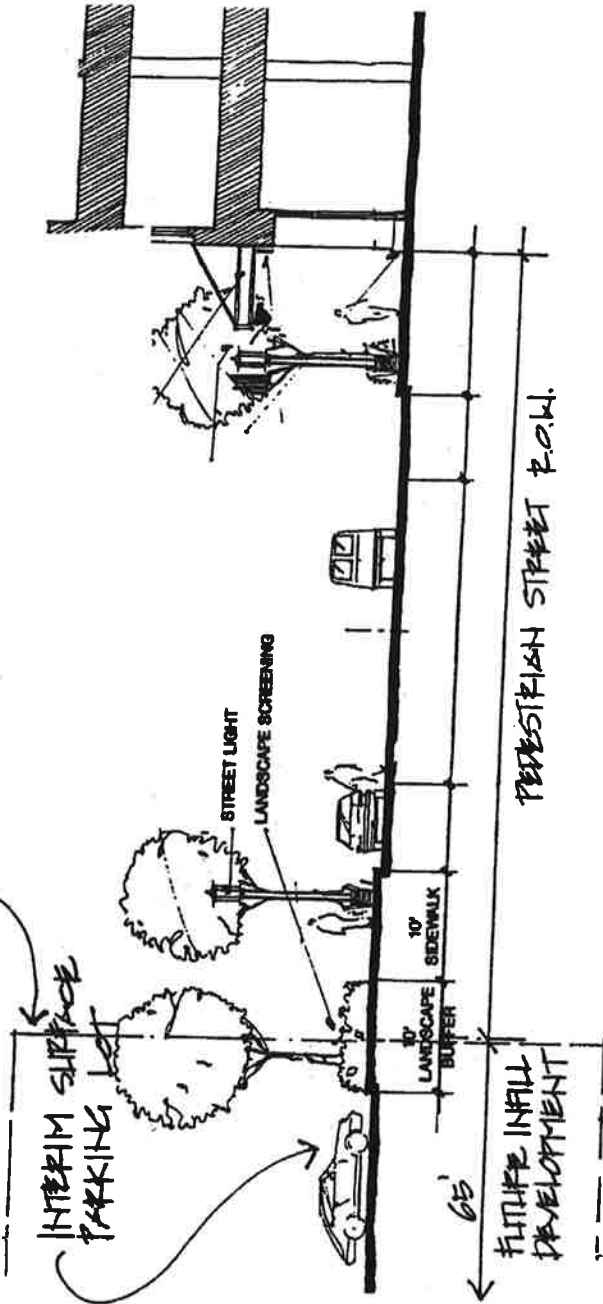
Note 2: The private road network shown maybe altered but must offer comparable road network redundancy in order to supplement the public street network.

CAPITAL MALL AREA

Possible Interim Development of 4th Avenue

NOTE: STREETSCAPE & STREET FRONT ELEVATIONS MAY VARY DUE TO STREET SLOPE CONDITIONS.

FUTURE BUILDING



PEDESTRIAN STREET W/
INTERIM SURFACE PARKING

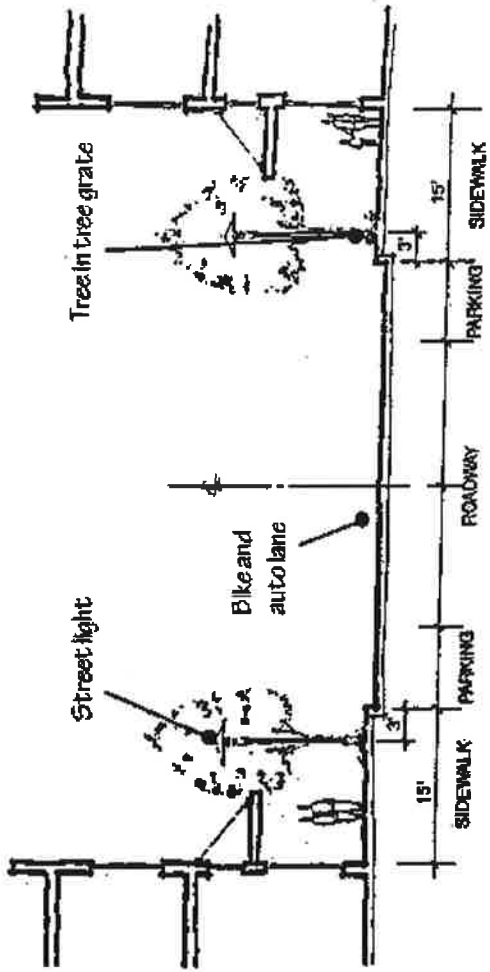
BELOW-GRADE BUILDING

Details of the street cross section will be determined at the time of development.

FIGURE 18.130.060-B

CAPITAL MALL AREA

Expected Later
Development
of 4th Avenue



PEDESTRIAN STREET

Details of the street
cross-section will be
determined at the time of
development.

FIGURE 18.130.060-C